



CITY COUNCIL MEETING AGENDA

Monday, January 10, 2022, 6:00 p.m.

Meeting held electronically and open to public attendance

Council Chamber, City Hall

We recognize and respect that New Westminster is on the unceded and unsurrendered land of the Halkomelem speaking peoples. We acknowledge that colonialism has made invisible their histories and connections to the land. As a City, we are learning and building relationships with the people whose lands we are on.

LIVE WEBCAST: Please note City Council Meetings, Public Hearings, Council Workshops and some Special City Council Meetings are streamed online and are accessible through the City's website at <http://www.newwestcity.ca/council>

Pages

1. CALL TO ORDER

2. LAND ACKNOWLEDGEMENT

We recognise and respect that New Westminster is on the unceded and unsurrendered land of the Halkomelem speaking peoples. We acknowledge that colonialism has made invisible their histories and connections to the land. As a City, we are learning and building relationships with the people whose lands we are on.

3. CHANGES TO THE AGENDA

Urgent/time sensitive matters only

4. REPORTS AND PRESENTATIONS FOR COUNCIL DISCUSSION AND ACTION

4.1. Budget 2022: Five-Year Financial Plan 2022 - 2026, CFO/Director of Finance

a. Written Submissions (On Table)

7

b. Budget 2022: Five-Year Financial Plan 2022 - 2026

13

To request Council give three readings to the Five-Year Financial Plan Bylaw for the years 2022 - 2026.

Recommendation:

THAT Council give three readings to the Five-Year Financial Plan (2022-2026) Bylaw No. 8308, 2022 as presented in Attachment 1 of this report.

4.2. Joy and Whimsy Initiative, Director of Parks & Recreation

a. Presentation (On Table)

21

b. Joy and Whimsy Initiative

32

This report is provided to inform Council on how special events and initiatives brought joy and whimsy to New Westminster during the COVID- 19 pandemic in 2021.

Recommendation:

THAT this report be recieved for information.

5. CONSENT AGENDA

If Council decides, all the recommendations in the reports on the Consent Agenda can be approved in one motion, without discussion. If Council wishes to discuss a report, that report is removed from the Consent Agenda. A report may be removed in order to discuss it, because someone wants to vote against the report's recommendation, or because someone has a conflict of interest with the report. Any reports not removed from the Consent Agenda are passed without discussion.

Recommendation:

THAT Council adopt the recommendations for items # on consent.

5.1. Alcohol in Parks Program: 2021 Review

36

The purpose of this report is to share a summary of the feedback received from the community and an interdepartmental staff team as part of the evaluation of the new 2021 Alcohol in Parks Program. The findings from the review are presented in this report to identify what worked well and areas for improvement going forward.

Recommendation:

THAT the Alcohol in Parks Program, as enabled through Bylaw No. 8264, 2021, continue in seven designated zones with administrative changes for improvement as outlined in this report.

5.2. Amendments to the 2022 Schedule of Council Meetings

79

To seek Council's approval of amendments to the 2022 Council Meeting Schedule.

Recommendation:

THAT the 2022 Schedule of Council Meetings be updated as presented in Attachment 1.

5.3. BC Superweek Pro-Cycling Series: New West Grand Prix

This report is provided to inform Council that BC Superweek Pro Cycling Series - New West Grand Prix will not be hosted in the City in 2022 as a result of civil engineering construction projects on Columbia Street and in adjacent areas of the downtown.

Recommendation:

THAT Council receive this report for information, and

THAT Council direct staff to report back in the Fall of 2022, and in advance of the City's 2023 Operating budget deliberations, regarding opportunities to host future New West Grand Prix cycling events.

5.4. Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022 and Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022 Bylaws for First and Second Readings

87

For Council to consider bylaws which would allow the construction of an infill house on a Queen's Park property in exchange for heritage protection and conservation of a heritage house.

Recommendation:

THAT Council consider Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022 and Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022 for First and Second Readings, and forward the Bylaws to a Public Hearing.

THAT Council add 323 Regina Street to the City's Heritage Register following the adoption of Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022.

5.5. Rezoning Application for Duplex: 122 Eighth Avenue – Preliminary Report

225

To seek Council's approval to process the rezoning application for a duplex at 122 Eighth Avenue as outlined within this report.

Recommendation:

THAT Council direct staff to process the rezoning application for a duplex at 122 Eighth Avenue, as outlined in the "Consultation and Review Process" section of this report.

5.6. Rezoning Application for Infill Townhouse: 337 and 339 Keary Street – Preliminary Report to Council

242

To seek Council's approval to proceed with processing the proposed rezoning at 337 and 339 Keary Street.

Recommendation:

THAT Council direct staff to proceed with processing the proposed rezoning at 337-339 Keary Street, as outlined in the "Consultation and

Review Process” section of this report.

- 5.7. **Update regarding Downtown Livability Strategy** 267
To provide an update on the status of the immediate and short term actions from the Downtown Livability Strategy, previously presented to Council on October 18, 2021.

Recommendation:

THAT Council endorse the actions underway.

- 5.8. **Uptown Active Transportation Improvements Projects: Design and Engagement Update** 277
To seek Council’s endorsement to move forward to conceptual design and the next round of public engagement with the recommended configuration option for Rotary Crosstown Greenway upgrades and the recommended routing option for the New Westminster Secondary School Cycling Connector, based on results of the first round of engagement as well as recommendations from the findings of a multiple account evaluation for the Uptown Active Transportation Improvements projects.

Recommendation:

THAT Council receive the Uptown Active Transportation Improvements Engagement Summary Report and Multiple Account Evaluation memo for information.

THAT Council endorse configuration Option 2 for the Rotary Crosstown Greenway Upgrades.

THAT Council endorse routing Option 2 for the New Westminster Secondary School Cycling Connector.

THAT Council direct staff to implement an interim New Westminster Secondary School Cycling Connector along Sixth Street (routing Option 2) using high quality lower-cost materials, to test the feasibility and evaluate impacts of the routing option on transit and traffic operations.

- 5.9. **Proclamation: International Holocaust Remembrance Day, January 27, 2022** 345
- 5.10. **Minutes for adoption**
- a. **November 1, 2021 Special Council Workshop** 346

6. **OPPORTUNITY FOR THE PUBLIC TO SPEAK TO COUNCIL – 7:00 PM**

7. **BYLAWS**

- 7.1. **Bylaws for readings**
- a. **Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022** 351

To enable the construction of an infill house, larger than permitted under the laneway and carriage house program, at 323 Regina Street with relaxations for density and siting as well as a siting relaxation for the existing house. This bylaw is on the agenda for **TWO READINGS**.

A public hearing will be held regarding this bylaw.

- b. **Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022** 406
To designate the 1928 house at 323 Regina Street as a protected heritage property. This bylaw is on the agenda for **TWO READINGS**.

A public hearing will be held regarding this bylaw.

- c. **Five-Year Financial Plan (2022 - 2026) Bylaw No. 8308, 2022** 411
To approve the City's consolidated Financial Plan for 2022 through 2026. This bylaw is on the agenda for **THREE READINGS**.

7.2. Bylaws for adoption

- a. **Development Cost Charge Reserve Funds Expenditure Bylaw No. 8307, 2021** 416
To authorize expenditures from the City's Development Cost Charge Reserves for Queensborough drainage, water, transportation and parkland development, and Mainland transportation and parkland development. This bylaw is on the agenda for **ADOPTION**.

8. MOTIONS FROM MEMBERS OF COUNCIL

8.1. Smoking Bylaws Review, Mayor Cote

Recommendation:

THAT Council request staff to conduct a review and scan of smoking bylaws in municipalities in British Columbia and report back to Council with a preliminary assessment and options to enhance New Westminster smoking bylaws.

9. NEW BUSINESS

10. ANNOUNCEMENTS FROM MEMBERS OF COUNCIL

11. END OF THE MEETING

*Some personal information is collected and archived by the City of New

Westminster under Section 26(g)(ii) of the Freedom of Information and Protection of Privacy Act and for the purpose of the City's ongoing commitment to open and transparent government. If you have any questions about the collection of personal information please contact Legislative Services, 511 Royal Avenue, New Westminster, V3L 1H9, 604-527-4523.

Budget 2022 - Public Feedback

1	<p>Fender just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>Reduce budget for the NWPD and focus on areas that promote equity/increasing funding for emergency services that do not employ violence (e.g. fire dept), increase budget for parks to create accessible green spaces in all neighborhoods of all income levels</p> <p>OPTIONAL: Please share your postal code.</p> <p>New Westminster, BC, V3M6S2</p>
2	<p>sjt just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>Once again, residents of Queensborough would ask you consider different options for us when it comes to things like increasing property taxes. We are not like the rest of New West. We don't have access to the same city services. We don't have easy access to the same facilities (which is incredibly apparent when you look at our lack of even grocery and financial institution options). We have unique traffic troubles that the rest of the city is not faced with and we rarely get the same infrastructure attention like road, sidewalk and park maintenance. There's a reason why the homes cost less here. Not to mention a 4.4 per cent increase is a significant jump and therefore a hardship. Thank you for listening</p>
3	<p>Brownie just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>Seems like a responsible budget as always should be less than 5% especially with today's inflation rate. How has the Canada Games closure effected revenue/expenses in the 2022 budget?</p>
4	<p>deepspace just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>1. The highest capital expenditure is for affordable housing. While this is an important priority, many residents feel that New Westminster is shouldering a disproportionate amount in this regard. The City should place much more pressure on senior governments and neighbouring cities (especially large cities like Burnaby) to provide funding for affordable housing. 2. There are reportedly only about 6</p>

	<p>police officers on patrol in the entire city at any given time. A \$34 million budget seems excessive for that level of service, especially considering wait times of an hour or more on the non-emergency police line. Perhaps we need fewer shiny vehicles standing idle behind the police station and more officers, or better yet, spend some of this funding on mental health and addiction intervention.</p> <p>OPTIONAL: Please share your postal code.</p> <p>New Westminster, BC, V3L0B3</p>
5	<p>slechner just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>Hello, I see in the report that the police budget accounts for approx 24% of expenses. However from what I've read in news items, the police department has not been asked to reduce spending or work within a constrained budget environment. They received a \$1M additional funds from 2021 to 2022, and steady increases in subsequent years. The police service should be accountable to budget limitations as any other city service, especially one that only provides services to a limited population (their primary enforcement objective seems to rely on corporate interests, anti-poverty/loitering bylaws).</p> <p>OPTIONAL: Please share your postal code.</p> <p>New Westminster, BC, V3L1W3</p>
6	<p>Hello,</p> <p>The proposed increase is very huge. With rising costs of food and housing, current owners and tenants would have to look to move somewhere else.</p> <p>Also, proposed costs are the services that are not really needed! We prefer to be over our roof rather than spend millions of dollars on drug addicts! <small>text remove because of inappropriate language</small></p> <p>Please stop wasting our money on the services we don't need! We also need money and have family to feed!</p> <p>JY</p>
7	<p>Jango just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>I'm fed up with this council increasing property taxes and decreasing our services. You have to go searching for a garbage can in the city. Half of them have been removed. Our recycling station is gone and we're expected to drive through a major truck intersection to drop off our recycling in Coquitlam. Our pool and fitness centre is closed. And you decide to use the savings hiring more staff for diversity stuff</p>

	<p>rather than to find a way to replace the fitness centre elsewhere in the city. And you want to (without any consultation as usual) to halt fitness classes in the Community Centre. Everything you do is without thought or forward thinking. How about you start looking after the people who actually pay your wages rather than those that don't. Your priorities seem to be developers and anyone other than tax payers.</p> <p>OPTIONAL: Please share your postal code.</p> <p>New Westminster, BC, V3L5W1</p>
8	<p>Jango just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>Yesterday, December 19th I left my home to go for a hike in Port Moody at 8:30am. The roads were ice. No salting whatsoever had been done. All roads from Royal to Columbia were blocked off due to ice. I saw four pedestrians crossing Royal at Tipperary Park slip and fall. These were older folks either going to church or trying to get to the sky train. My friend saw two accidents at 4th near 6th Street due to ice and witnessed two pedestrians slip and fall. Once outside of New West, in Coquitlam and Port Moody the roads were salted and safe. AGAIN, what exactly do my taxes pay for. New Westminster seemed to be the only City that didn't bother salting the roads and people were injured because of it.</p> <p>OPTIONAL: Please share your postal code.</p> <p>New Westminster, BC, V3L5W1</p>
9	<p>Farad R just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedbackI'm disgusted that your raising the rates. In times like this, you should be showing your ability to cut what you can't afford, JUST LIKE THE REST OF US.</p> <p>OPTIONAL: Please share your postal code.</p> <p>New Westminster, BC, V3L3T3</p>
10	<p>garthtce just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>I noticed that 1 out of every three dollars collected in property tax was going towards police services. This alarming number could be reduced by increasing the property tax, or lowering the Police budget. I recommend both, as it would increase the ability of the City to supply services to its citizens.</p>
11	<p>Calico just submitted the survey Comments for Council with the responses below.</p>

	<p>Leave your feedback</p> <p>Not happy about this considering the huge hike in crime in New West that has personally affected us, twice this past year. Our house & car were hit within 6 weeks of moving into our new house. We had to eat the loss to prevent our deductibles/insurance costs rising more. In addition, 3/5 of the contractors we hired for our reno also experienced vandalism. Our roofer lost all his tools that were locked up in our backyard and 2 of our workers had their cars keyed, twice! (All was reported to NRPD). I would like to see what the city proposes to REDUCE crime & theft to take care of the victims of attracting more homelessness to New West by opening more shelters...</p>
12	<p>ConaughtHeightsresident just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>The highest percentage (29%) of New Westminster residents on the survey had answered with the tax increase being 3% (rate of inflation). The proposed 4.4% by Council is not paying attention to the survey and suggests the survey is done for optics only. Because we are still very much in a pandemic and this has caused financial stress on many New West residents, the Council should reconsider their proposed increase and set it at 3% which would allow maintenance of current services. When the pandemic is over the city can look at where the increase should be set at but not before</p>
13	<p>Vicarious just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>Why are we doing unnecessary side projects like the pedestrian scramble at 6th and 6th, bike lanes down 6th and beautifying the 6th street sidewalks if we're short enough on budget to require a property tax increase? Either scrap some of the pointless pet projects or figure out how to make the budget work. The residents shouldn't be having to foot the bill for something that won't improve their life in any capacity.</p> <p>OPTIONAL: Please share your postal code.</p> <p>New Westminster, BC, V3M5Z8</p>
14	<p>VI Boy just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>I understand the city's finances are complex, so I do not want to seem overly</p>

	<p>simplistic in my comments, but in my view a tax increase of 4.4% is excessive. The City ought to be focussing its priorities on the basics - utilities (water, sewer, electrical), streets maintenance and public safety. Having said that, it is my view that too much of the City's expenditures are devoted to Police and Fire services. While I am not an advocate of "defunding" police, it appears to me as if the police service is holding the city to ransom with its budget requirements. These are tough times financially, and tough decisions need to be made. One other example is Parks and Recreation. While the City has an impressive array of park facilities and recreation programs, again there needs to be a focus on core services. We cannot be all things to all people. In my opinion, the City's tax increases should be more in line with the rate of inflation, and these (admittedly general) comments are offered in that spirit.</p> <p>OPTIONAL: Please share your postal code.</p> <p>New Westminster, BC, V3M6C5</p>
15	<p>pj just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>It seems like the majority of owners want a cost of living increase or a decrease of property taxes so why is this band <small>text remove because of inappropriate language</small> raising it 4.4 %??</p>
16	<p>Cindy just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>Due to the financial struggles of all taxpayers, and the current pandemic situation I am not in favour of any increases at this time. There is already increase in all basic necessities, food, fuel, etc. I am sure council can come up with more creative ways to cut costs not increase them.</p>
17	<p>KC just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>Thank you for asking for my feedback on the draft Financial Plan bylaw that will read at the January 10 City Council meeting. While paying increased property and other taxes in not fun, I am willing to pay increases WHEN the fund are targeted for climate action priorities. Given that approximately 41% of greenhouse gas emissions in New Westminster originate from buildings, I strongly urge you to spend what is needed to find a way to accelerate the transition from gas to electric heating. We simply do not have time for New Westminster's current STEP code progression as we need to reduce global emissions by 45% by 2030 to have any hope of achieving net zero by 2050 and thereby keeping our global temperature increase in the 1.5 to 2 degrees Celcius range. Here are some things that other municipalities under the Community Charter are doing that might be helpful for city staff to dig into deeper: LCES on the North Shore District of North Vanc, below, from online North Shore News, December 10 2020 of passing new building regulations https://www.nsnews.com/local-news/district-of-north-van-tightens-greenhouse-gas-regulations-for-new-home-construction-3170645 DNV website: Energy Step Code District of North Vancouver (dnv.org) City of North Vanc, below, from November 25, 2020 of</p>

	<p>passing new building regulations: https://www.nsnews.com/local-news/city-of-north-van-steps-towards-climate-goals-with-new-building-regulations-3149068 CNV website: BC Energy Step Code for New Buildings (cnv.org) District of West Vanc, below https://www.nsnews.com/local-news/west-van-approves-carbon-cutting-policies-3117848 please see March 9, 2020 District West Vanc Council minutes at this link, specifically agenda item 10. 20mar09.pdf (westvancouver.ca) DWV website: BC Energy Step Code District of West Vancouver The LCES is in place in ALL three North Shore municipalities now (as of year 2021).</p>
18	<p>Banshee just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>The tax increase is reasonable, and my property taxes are still very low compared to the taxes I pay to victoria and ottawa through sales tax and income tax and all the other taxes I pay, and are much lower than I paid in Ontario. I hope the city will invest more in new west, in building bike lanes and expanding parks and refreshing schools, and I am happy to help pay for that. I also hope the city will invest in helping the people who are suffering the most in our community, as I look out at the snow falling and think of my homeless neighbors, I cannot believe people comfortable in their homes complain about property taxes.</p>
19	<p>mcj just submitted the survey Comments for Council with the responses below.</p> <p>Leave your feedback</p> <p>Glad to see cycling and greenways have a large portion this year. Would be nice to see the amounts for 2023 onward match this similarly. There's great potential to get more people cycling in this city with the rising popularity of ebikes and escooters!</p> <p>OPTIONAL: Please share your postal code.</p> <p>New Westminster, BC, V3M1A6</p>

REPORT

Finance

To: Mayor Cote and Members of Council **Date:** January 10, 2022

From: Harji Varn **File:** Doc # 1988175
CFO/Director of Finance

Item #: 2022-11

Subject: Budget 2022: Five-Year Financial Plan 2022 - 2026

RECOMMENDATION

THAT Council give three readings to the Five-Year Financial Plan (2022-2026) Bylaw No. 8308, 2022 as presented in Attachment 1 of this report.

PURPOSE

To request Council give three readings to the Five-Year Financial Plan Bylaw for the years 2022 - 2026.

DISCUSSION

On December 13, 2021 Council instructed staff to prepare the consolidated 2022 – 2026 Financial Plan Bylaw reflecting a property tax increase of 4.4% for 2022 and a 2022 capital budget of \$170.1M. The plan incorporates the General Fund and Utility Funds five-year plans including the approved 2022 Water, Sewer, Solid Waste and Electrical rates.

Attachment 1 presents the City's Five-Year Financial Plan (2022 - 2026) Bylaw No. 8308, 2022 prepared in accordance with Section 165 of the Community Charter. Schedule A to the bylaw provides the consolidated Financial Plan for 2022 through 2026. Schedules B and C provide supplementary detailed information regarding the City's capital program and reserves, respectively.

The Financial Plan has been prepared based on the City's financial policies and budget principles that strive to find a balance between affordability and providing resources to service a growing community for the long term. The plan advances Council's Strategic Priorities and the City's 7 Bold Steps while staff continue to maintain core services and manage through the ongoing impacts of the COVID-19 pandemic.

INTERDEPARTMENTAL LIAISON

All City departments participate in the annual budget and financial plan preparation process.

CONCLUSION

Under the provisions of the Community Charter, the City is required to adopt annually a five-year financial plan bylaw prior to May 15th. It is recommended, therefore, that the Five-Year Financial Plan (2022 – 2026) Bylaw No. 8308, 2022 be considered for three readings.

OPTIONS

There are two options for Council's consideration:

- 1) **THAT** Council give three readings to the Five-Year Financial Plan (2022-2026) Bylaw No. 8308, 2022 as presented in Attachment 1 of this report; or
- 2) **THAT** Council provide staff with alternate direction.

ATTACHMENT

Attachment 1 – Five-Year Financial Plan (2022 – 2026) Bylaw No. 8308, 2022

This report was prepared by:

Lorraine Lyle,
Senior Manager, Financial Services

This report was approved by:

Harji Varn
CFO/Director of Finance

Lisa Spitale
Chief Administrative Officer

Attachment #1

Five-Year Financial Plan (2022 – 2026)

Bylaw No. 8308, 2022

CORPORATION OF THE CITY OF NEW WESTMINSTER

BYLAW NO. 8308, 2022

A Bylaw of the City of New Westminster representing the Five-Year
Financial Plan for the years 2022 – 2026, inclusive

WHEREAS pursuant to Section 165 of the Community Charter, Council must establish a Five-Year Financial Plan for the period 2022 – 2026 inclusive;

NOW THEREFORE, the Council of the Corporation of the City of New Westminster ENACTS
AS FOLLOWS:

(1) This bylaw may be cited for all purposes as the “Five-Year Financial Plan (2022 – 2026),
Bylaw No. 8308, 2022”.

(2) Council does hereby adopt the Five-Year Financial Plan, for the years 2022 - 2026
inclusive, for each year of the plan, as set out in Schedule A.

(3) Schedules B and C provide supplementary information to the bylaw.

GIVEN FIRST READING this day of , 2022.

GIVEN SECOND READING this day of , 2022.

GIVEN THIRD READING this day of , 2022.

ADOPTED this day of , 2022.

Mayor Jonathan X. Cote

Jacque Killawee, City Clerk

CORPORATION OF THE CITY OF NEW WESTMINSTER

CONSOLIDATED FINANCIAL PLAN

Schedule 'A' to Bylaw No. 8308, 2022

	2022	Budget Projections			
	Budget	2023	2024	2025	2026
REVENUE					
Municipal Taxation (see below)	\$ 97,877,861	\$ 103,095,703	\$ 109,463,995	\$ 115,272,273	\$ 120,451,743
Utility Rates	100,119,172	105,126,857	110,290,843	115,769,312	121,585,481
Sale of Services	14,373,935	15,771,060	15,871,060	15,571,060	15,571,060
Grants from Other Governments (1)	8,920,339	5,893,400	2,871,600	2,871,600	2,871,600
Contributions (2)	24,086,630	24,174,930	18,839,530	17,170,930	10,983,930
Other Revenue	16,915,915	18,081,073	18,221,457	18,082,689	17,797,265
Total Revenues	262,293,852	272,143,023	275,558,485	284,737,864	289,261,079
EXPENSES					
General Services					
Police Services	34,408,503	34,729,375	35,717,010	36,385,994	36,975,831
Parks and Recreation	19,416,533	22,296,796	26,355,706	26,703,057	26,982,221
Fire and Rescue	17,868,972	18,554,183	19,290,982	19,670,640	20,219,813
Development Services	7,358,604	7,014,666	7,155,875	7,313,040	7,449,313
Engineering	29,776,519	30,592,468	31,214,919	31,519,297	32,086,538
General Government	29,850,878	30,366,173	30,452,417	30,563,408	31,384,291
Library	4,905,686	4,996,028	5,068,783	5,119,432	5,180,001
	143,585,695	148,549,689	155,255,692	157,274,868	160,278,008
Utilities Services					
Electrical Utility	40,883,766	41,706,514	42,581,978	45,037,184	45,867,089
Water Utility	9,358,198	9,967,152	10,999,891	12,093,204	13,346,871
Sewer Utility	15,330,615	17,646,962	20,145,656	23,121,261	28,848,140
Solid Waste Utility	3,994,991	4,112,023	4,210,458	4,295,997	4,424,510
	69,567,570	73,432,651	77,937,983	84,547,646	92,486,610
Fiscal Expenses					
Interest and Bank Charges	2,849,958	5,177,088	6,157,719	6,222,996	6,445,961
Total Expenses	216,003,223	227,159,428	239,351,394	248,045,510	259,210,579
INCREASE IN TOTAL EQUITY	46,290,629	44,983,595	36,207,091	36,692,354	30,050,500
Reconciliation to Financial Equity					
Amortization of Tangible Capital Assets	26,286,000	27,438,000	29,959,000	31,329,000	31,803,000
Capital Expenses (Schedule B)	(170,699,310)	(109,731,100)	(66,230,250)	(58,198,700)	(48,236,400)
Debt Retirement	(5,151,403)	(8,214,754)	(9,686,716)	(9,657,090)	(10,214,494)
Proceeds on Debt Issuance	72,184,585	30,772,800	8,000,000	7,500,000	500,000
CHANGE IN FINANCIAL EQUITY (Reserves)	(31,089,499)	(14,751,459)	(1,750,875)	7,665,564	3,902,606
Financial Equity, beginning of year	154,633,733	123,544,234	108,792,775	107,041,900	114,707,464
FINANCIAL EQUITY (Reserves), end of year	\$ 123,544,234	\$ 108,792,775	\$ 107,041,900	\$ 114,707,464	\$ 118,610,070

Notes:

(1) Includes capital grants noted on Schedule B.

(2) Includes capital contributions and DCCs noted on Schedule B.

Municipal Taxation					
Property Taxes	\$ 96,205,561	\$ 101,389,353	\$ 107,721,045	\$ 113,491,623	\$ 118,632,293
Parcel Taxes	19,200	17,750	17,750	17,750	17,750
Grant-in-Lieu of Taxes	1,184,200	1,219,700	1,256,300	1,294,000	1,332,800
Utilities 1%-in-Lieu of Taxes	468,900	468,900	468,900	468,900	468,900
	\$ 97,877,861	\$ 103,095,703	\$ 109,463,995	\$ 115,272,273	\$ 120,451,743

CORPORATION OF THE CITY OF NEW WESTMINSTER

CONSOLIDATED FINANCIAL PLAN

Schedule 'A' to Bylaw No. 8308, 2022

(continued)

Proportion of Revenues By Funding Source:

The following table shows the proportion of total revenue proposed to be raised from each funding source. Property taxes form the second largest portion of revenues. They provide a stable and consistent source of revenues to pay for many services, such as police and fire protection, that are difficult or undesirable to fund on a user-pay basis.

Utilities' rates are the City's largest component of planned revenues. These revenues pay for services including electricity, water, sewer and solid waste and are charged on a user-pay basis. This basis attempts to fairly apportion utility service costs to those that make use of these services.

Other revenue sources, including sale of services, government grants and contributions make up the remainder of total revenues. These revenues fluctuate due to economic conditions and City initiatives.

Revenue Source	% Total Revenue
Taxation	37%
Utility Rates	38%
Sale of Services	5%
Gov't Grants	3%
Contributions	9%
Other Revenue	6%
	100%

Distribution of Property Taxes Between Property Classes:

The following table provides the distribution of property tax revenue between property classes. The City's primary goal is to set tax rates that are sufficient, after maximizing non-tax revenues, to provide for service delivery; City assets; and maintain tax stability. This is accomplished by maintaining the historical relationship between the property classes and applying the same annual tax rate increase across all Classes. A secondary goal is to set tax rates that are competitive within the region; consequently, the City may, from time to time, adjust the property tax distribution between the Classes as deemed necessary.

Class No	Property Class	% Tax Burden
1	Residential	63%
2	Utilities	<1%
4	Major Industry	2%
5	Light Industry	3%
6	Business	31%
8	Recreation/Non-Profit	<1%
9	Farm	<1%
		100%

Use of Permissive Exemptions:

The City's Annual Municipal Report contains a list of permissive exemptions granted for the year and the amount of tax revenue foregone. Permissive tax exemption is granted to not-for-profit institutions including religious institutions, some recreational facilities, service organizations and cultural institutions that form a valuable part of our community.

Since the mid-90's the City has generally ceased granting new permissive exemptions from property taxes in order to preserve the tax revenue base. Organizations granted exemption prior to implementation of this practice continue to be considered for exemption provided they make an annual submission showing the use of the property subject to exemption has not been altered. All other applications for permissive exemption from property taxes are reviewed on a case-by-case basis.

CORPORATION OF THE CITY OF NEW WESTMINSTER

CONSOLIDATED CAPITAL PROGRAM

Schedule 'B' to Bylaw No. 8308, 2022

Note: This Schedule has been provided as an addendum to Schedule A. The figures in this Schedule are included in the consolidated figures in Schedule A.

	2022 Budget	Budget Projections			
		2023	2024	2025	2026
CAPITAL EXPENSES					
Land	\$ 500,000	\$ 4,733,800	\$ -	\$ -	\$ 2,000,000
Buildings	62,291,200	30,451,100	8,908,200	8,388,000	5,070,000
Vehicles/Equipment	7,503,500	5,530,800	6,250,650	4,883,500	4,753,500
Other Projects	8,519,295	5,164,800	3,601,500	3,709,300	3,135,000
Park Improvements	4,374,100	5,061,200	4,744,000	1,651,000	2,084,000
Engineering Structures	23,561,600	14,140,000	10,400,000	9,250,000	15,225,000
Water Infrastructure	6,011,500	6,838,900	5,338,900	5,838,900	5,838,900
Sewer Infrastructure	15,028,730	10,752,500	7,987,000	7,780,000	7,480,000
Electrical Distribution System	42,909,385	27,058,000	19,000,000	16,698,000	2,650,000
TOTAL	\$ 170,699,310	\$ 109,731,100	\$ 66,230,250	\$ 58,198,700	\$ 48,236,400
FUNDING SOURCES					
Reserve Funds	\$ 76,163,925	\$ 59,102,500	\$ 46,331,650	\$ 40,468,700	\$ 43,693,400
Development Cost Charges	3,734,150	2,660,000	1,321,600	375,000	263,000
Long Term Debt	72,184,585	30,772,800	8,000,000	7,500,000	500,000
Grants from Other Governments	5,660,800	3,021,800	-	-	-
Contributions	12,955,850	14,174,000	10,577,000	9,855,000	3,780,000
TOTAL	\$ 170,699,310	\$ 109,731,100	\$ 66,230,250	\$ 58,198,700	\$ 48,236,400

City of New Westminster - Development Cost Charge Funding Envelope Plan for the 2009 DCC Bylaw 7311

NOTES:

1. This DCC Funding Envelope Plan is based on the capital projects set out in the 2009 Development Cost Charge Review which forms the basis for the City's DCC Bylaw. The City's DCC Bylaw was amended in 2015 to reflect new rates based on an updated capital project plan.

2. City contributions will be from reserves while other contributions are from provincial / federal government grants.

3. The mainland waterfront parkland acquisition / development (\$16M) was initially funded with debt with the intention that the principal on the debt would be repaid over time using Parks DCCs.

	<u>Values</u>	<u>TTL</u>	<u>2009 - 2021</u>	<u>2022</u>	<u>2023</u>	<u>2024</u>	<u>2025</u>	<u>2026</u>	<u>2027 +</u>
Total DCCs	87,284,408	\$ 87,284,408	\$ 38,993,527	\$ 3,734,150	\$ 2,660,000	\$ 1,321,600	\$ 375,000	\$ 263,000	\$ 39,937,131
Total City & Other Contributions	34,985,065	34,985,065	11,932,581	2,349,257	3,406,586	2,395,886	1,339,286	939,286	12,622,183
	122,269,473	\$ 122,269,473	\$ 50,926,108	\$ 6,083,407	\$ 6,066,586	\$ 3,717,486	\$ 1,714,286	\$ 1,202,286	\$ 52,559,314

CORPORATION OF THE CITY OF NEW WESTMINSTER
CONSOLIDATED STATEMENT OF RESERVES AND DEVELOPMENT COST CHARGES
Schedule 'C' to Bylaw No. 8308, 2022

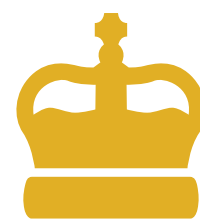
Note: This Schedule has been provided as an addendum to Schedule A. The reserve figures in this Schedule are included in the consolidated figures in Schedule A. Development Cost Charges are provided for information, but are deferred charges rather than reserves.

FINANCIAL EQUITY (RESERVES)	2022 Budget	Budget Projections			
		2023	2024	2025	2026
Revenues:					
Contributions	\$ 6,817,903	\$ 7,135,376	\$ 6,750,365	\$ 6,765,777	\$ 6,318,907
	6,817,903	7,135,376	6,750,365	6,765,777	6,318,907
Transfers (to) from:					
Operating Budget	38,256,523	37,215,665	37,830,410	41,368,487	41,277,099
Capital Budget	(76,163,925)	(59,102,500)	(46,331,650)	(40,468,700)	(43,693,400)
	(37,907,402)	(21,886,835)	(8,501,240)	899,787	(2,416,301)
Change in Financial Equity (Reserves)	(31,089,499)	(14,751,459)	(1,750,875)	7,665,564	3,902,606
Financial Equity, Beginning of Year	154,633,733	123,544,234	108,792,775	107,041,900	114,707,464
Financial Equity, End of Year	123,544,234	108,792,775	107,041,900	114,707,464	118,610,070
CHANGE IN RESERVES					
Non-Statutory Reserves	\$ (29,574,974)	\$ (10,346,144)	\$ (1,912,754)	\$ 7,500,447	\$ 5,734,188
Statutory Reserves					
Cemetery	52,025	53,066	54,127	55,210	56,314
Construction of Municipal Works	(1,628,826)	(4,521,902)	42,960	43,819	(1,955,305)
Parking Cash In Lieu	24,803	25,299	25,805	26,321	26,847
Park Land Acquisition	1,293	1,319	1,346	1,373	1,400
Tax Sale Land	36,180	36,903	37,641	38,394	39,162
Change in Reserves	\$ (31,089,499)	\$ (14,751,459)	\$ (1,750,875)	\$ 7,665,564	\$ 3,902,606
Statutory DCC Reserves					
Drainage DCC	\$ 61,370	\$ 123,998	\$ 126,478	\$ 129,007	\$ 131,588
Parkland DCC	1,143,242	(247,292)	788,161	1,383,325	1,392,291
Sewer DCC	(540,329)	378,414	386,483	449,212	458,196
Transportation DCC	(1,233,594)	(8,365)	271,468	570,897	694,315
Water DCC	(48,425)	180,406	184,015	187,695	191,448
Change in DCCs	\$ (617,736)	\$ 427,161	\$ 1,756,605	\$ 2,720,136	\$ 2,867,838
RESERVE BALANCES					
Non-Statutory Reserves	\$ 112,812,498	\$ 102,466,354	\$ 100,553,600	\$ 108,054,047	\$ 113,788,235
Statutory Reserves					
Cemetery	885,796	938,862	992,989	1,048,199	1,104,513
Construction of Municipal Works	6,669,881	2,147,979	2,190,939	2,234,758	279,453
Parking Cash In Lieu	1,264,932	1,290,231	1,316,036	1,342,357	1,369,204
Park Land Acquisition	65,961	67,280	68,626	69,999	71,399
Tax Sale Land	1,845,166	1,882,069	1,919,710	1,958,104	1,997,266
Total Reserves	\$ 123,544,234	\$ 108,792,775	\$ 107,041,900	\$ 114,707,464	\$ 118,610,070
Statutory DCC Reserves					
Drainage DCC	\$ 449,878	\$ 573,876	\$ 700,354	\$ 829,361	\$ 960,949
Parkland DCC	14,360,371	14,113,079	14,901,240	16,284,565	17,676,856
Sewer DCC	1,695,705	2,074,119	2,460,602	2,909,814	3,368,010
Transportation DCC	(468,272)	(476,637)	(205,169)	365,728	1,060,043
Water DCC	2,020,303	2,200,709	2,384,724	2,572,419	2,763,867
Total DCC Reserves	\$ 18,057,985	\$ 18,485,146	\$ 20,241,751	\$ 22,961,887	\$ 25,829,725



Joy and Whimsy

2021 at a Glance



NEW WESTMINSTER

ON TABLE
City Council Meeting
January 10, 2022
Item 4.2.a.

Joy and Whimsy in 2021

Joy and Whimsy came to New Westminster in 2021 as we found ways to help residents and staff deal with isolation, loss and struggles to find happiness during this difficult year.

Joy and Whimsy was supported in three different ways:



Supporting Community Organizations

Supported over 15 community events as they re-imagined event offerings during the pandemic

City Initiatives

Led 10 initiatives that brought the community together in safe ways

Staff Recognition

Recognizing staffs hard work and dedication

Supporting Community Organizations



**HYACK DRIVE-THRU
PARADE**



**HOLIDAY FARMERS
MARKET**



RECOVERY DAY



**REVIVAL MUSIC
FESTIVAL**



**DOWNTOWN SHOP
NIGHTS**



**UPTOWN LIVE
CHRISTMAS**

Supporting Community Organizations



**NATIONAL DAY FOR
TRUTH AND
RECONCILIATION**



**HOSPICE SOCIETY
QUILT FOR GRIEF AND
GRATITUDE**



MAY DAY



**FAMILY PLACE
HALLOWEEN PARTY**



**NEW WEST FARMERS
MARKET**



QUEST NEW WEST

City Initiatives



FAMILY DAY

Picking up a detective kit at the Youth Centre



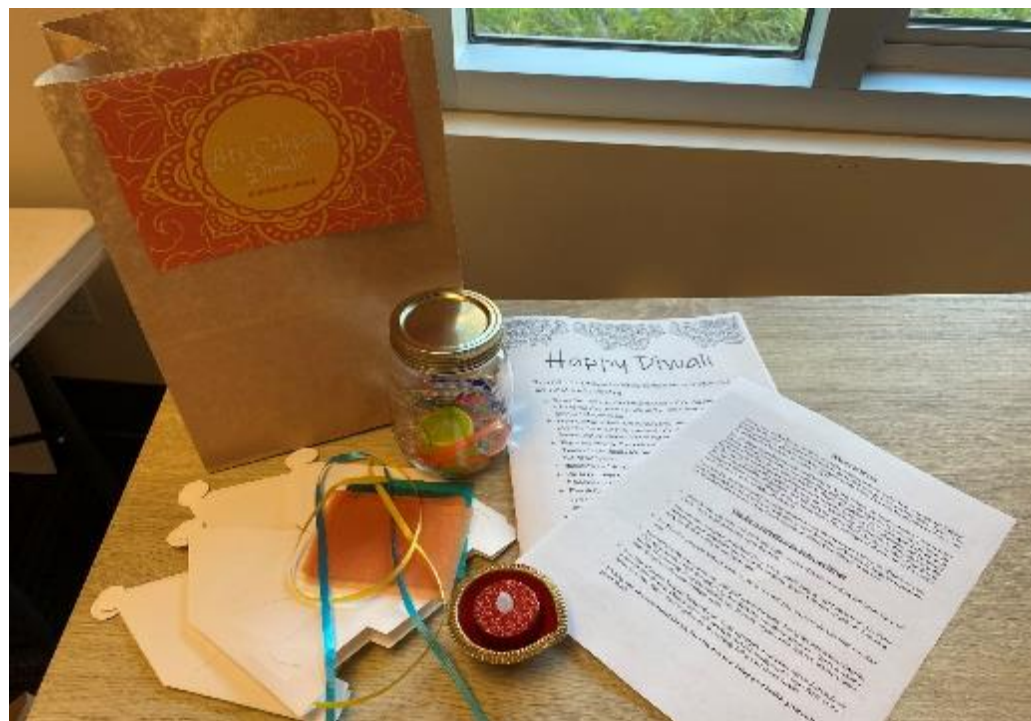
EASTER

Colouring contest winner picking up his prize



REMEMBRANCE DAY

Virtual ceremony



DIWALI

Take home celebration kits



PRIDE

Cardio and Queens drag fitness class



CANADA DAY

Family concert and picnic in the park

The Rest of New West



Tuesday Tunes at Moody Park



Royal City Sound Pier Nights



Nature Art Walk at Hume Park



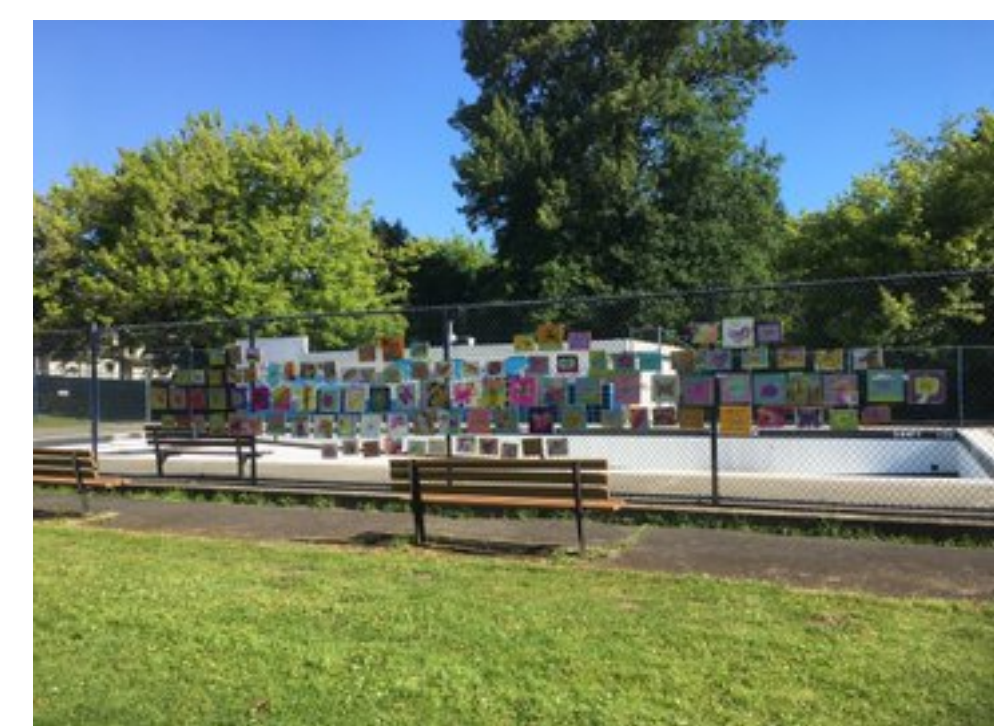
Rock Painting Workshop



Outdoor Play at Queen's Park



Outdoor Scavenger Hunt



Hume Park Pool Mosaic Art Project

Shine Bright New West



Queensborough Community Centre display by Queensborough Special Programs Committee



Sapperton Plaza interactive display



Community Christmas Tree at Century House



Queen's Park Winter Solstice Event



Temporary Winter Solstice Display at Queen's Park



School Choir at Ryall Park

Shine Bright Anvil Centre



WINDOW PERFORMANCES

Image: Kitka Dancers, Anvil Lobby



THEATRE PERFORMANCES

Clockwise from top left: The Kerplunks, Triology, Blackthorn, Shot of Scotch Highland Dancers, Locarno, Vitaly Beckman, RupLoops and (centre) Kellie Haines



Staff Recognition



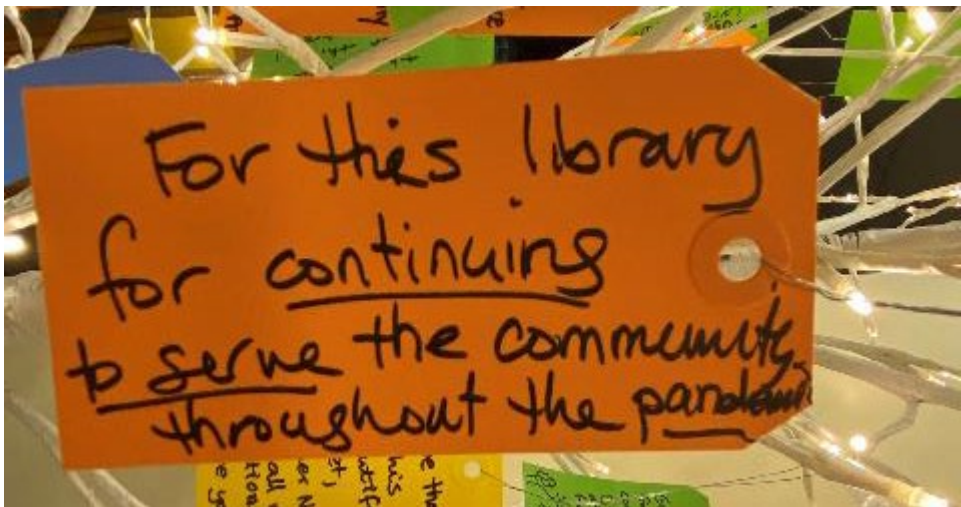
Staff Gratitude Lunches



Gratitude Trees



Setup in 4 locations with over 1500 messages of gratitude



Looking ahead to 2022

We were able to package up Joy and Whimsy through a variety of events in 2021.

More to come in 2022!



REPORT

Parks and Recreation

To: Mayor Cote and Members of Council **Date:** January 10, 2022

From: Dean Gibson,
Director of Parks and Recreation **File:** 1997286

Item #: 2022-24

Subject: Joy and Whimsy Initiative

RECOMMENDATION

THAT this report be recieved for information.

PURPOSE

This report is provided to inform Council on how special events and initiatives brought joy and whimsy to New Westminster during the COVID- 19 pandemic in 2021.

BACKGROUND

At a Council meeting on June 7th 2021, Council directed staff to investigate the support for community events that would draw the community out of the COVID-19 Pandemic and bring joy and whimsy to New Westminster.

A Joy and Whimsy Working Group comprised of staff from Parks and Recreation, Arts Services, Engineering and Finance was formed to look at ways to support external and internal initiatives to help residents and staff deal with isolation, loss and struggles to find happiness during this very difficult year. The working group looked at three (3) categories that would support joy and whimsy in the city:

1. City Supporting Community Organizations
2. City Initiatives
3. Staff Recognition

ANALYSIS

City Supporting Community Organizations

The City of New Westminster came alive with community spirit in 2021. There were a variety of opportunities for residents to spend time with family and friends in outdoor venues enjoying new experiences. City staff worked with many community organizations to support their efforts in contributing to the social fabric of the community since the onset of the pandemic. Many organizations struggled to find ways to operate as they worked through the ever changing Provincial Health Orders. The City's Special Events staff team played a pivotal role in helping community organizations reimagine their events resulting in fifteen (15) organizations providing joy and whimsy to New Westminster.

All of these community projects, events and initiatives were funded by the City Grants Program and supported by the Special Events staff team. This included a street gardening project; a Christmas Celebration; a Halloween family event; a holiday themed Farmers Market; Quilts for Grief and Gratitude; a Music and Art Street Festival; Downtown Shop Nights; a Mini 12th Street Festival; a Celebration on the Fraser River; Recovery Day; New West Pride celebrations; Queensborough Children's Festival; Hyack Festival Drive-thru Parade; Shoreline Cleanup in Queensborough; and Shine Bright Downtown.

During the spring intake of the One Time Small City Grants program, applicants were also encouraged to apply for projects or initiatives that will celebrate our City, lift community spirit, recognize loss and support our community's collective recover as we move forward from the pandemic.

City Initiatives

The City led ten (10) initiatives in 2021 that supported and lifted the spirits of our residents. These community opportunities, developed in collaboration with a number of local organizations, included Family Day, Drive in Outdoor Movies, The Rest of New West, National Indigenous Peoples Day, Easter Eggstravaganza, Canada Day, Pride Celebrations, Diwali Activities, Remembrance Day and Shine Bright New West/Shine Bright Anvil. For 2022, City staff will continue to support these initiatives and seek out opportunities to deepen collaborations with community partners as well as implement a series of community art projects.

Staff Recognition

It has also been recognized that city staff played a vital role in keeping our city operating and to express gratitude for their work a special food truck lunch was offered in October to show thanks for a job well done. Mayor and Council, the CAO and Department Directors were on hand to acknowledge the staff efforts as they worked through the pandemic. Staff were able to choose one of the three days to attend a Gratitude Lunch following all Provincial Health Orders in order to gather safely.

A special gratitude tree was also introduced which gave staff the opportunity to express gratitude by writing messages and placing them on a tree. This initiative was so extremely well received that another four trees were set up for the community to participate at the Library Main Branch, City Hall, Queensborough Community Centre and Centennial Community Centre. In December, the trees became wishing trees where staff and community members were invited to hang ornaments and to express their thoughts and wishes.

In 2022, Thankful Thursday will be launched where staff teams will be recognized on how staff stepped up to support each other and the community during a very uncertain and challenging time. This initiative will be a pictorial journey on social media.

SUSTAINABILITY IMPLICATIONS

The City's Special Events staff team will continue to provide support to non-profit organizations as they offer their community events in New Westminster. Through the pandemic many working relationships have been strengthened and new ones were formed. Many community organizations looked to the Special Events staff team for guidance during this challenging time as well as seeking other ways they could collaborate with the City to produce memorable events in New Westminster.

FINANCIAL IMPLICATIONS

Many of the community projects and initiatives were supported under the City's Grant program and the City led initiatives were supported by the Special Events operating budget.

INTERDEPARTMENTAL LIAISON

Office of the CAO, Finance and Parks and Recreation worked closely in supporting all the Joy and Whimsy initiatives.

OPTIONS

1. Receive this report for information;
2. Provide staff with additional direction.

Option #1 is recommended

This report was prepared by:

Renee Chadwick, Manager, Special Projects and Community Partnerships

This report was reviewed by:

Todd Ayotte, Manager, Community Arts and Theatre

Lisa Kemp, Program Coordinator, Special Events

This report was approved by:

Dean Gibson, Director of Parks and Recreation

Lisa Spitale, Chief Administrative Officer

REPORT

Parks & Recreation

To: Mayor Cote and Members of Council **Date:** January 10, 2022

From: Dean Gibson,
Director of Parks and Recreation **File:** 1978576

Item #: 2022-18

Subject: **Alcohol in Parks Program – 2021 Review**

RECOMMENDATION

THAT the Alcohol in Parks Program, as enabled through Bylaw No. 8264, 2021, continue in seven designated zones with administrative changes for improvement as outlined in this report.

PURPOSE

The purpose of this report is to share a summary of the feedback received from the community and an interdepartmental staff team as part of the evaluation of the new 2021 Alcohol in Parks Program. The findings from the review are presented in this report to identify what worked well and areas for improvement going forward.

SUMMARY

The Alcohol in Parks Program, the “Program”, was officially launched on May 17, 2021 to respond to community needs for socializing safely outdoors in the wake of COVID-19; to provide more outdoor spaces for social connectivity (especially for those without access to private backyards); and to test and monitor the implications of allowing alcohol consumption in parks.

Post Program implementation engagement activities included an inter-departmental City staff workshop, a community survey and other feedback tools on the [Be Heard New West](#) engagement website. With approximately 196 active participants in the engagement process, staff are now reporting back to City Council on the engagement results and proposed next steps.

BACKGROUND

On May 17, 2021 Council adopted Bylaw No. 8264, 2021, a bylaw to permit the consumption of liquor in public spaces. Key elements of the Bylaw include:

- Permitted spaces where liquor may be consumed – Designated zones within seven City parks (as illustrated in Schedule “A” of the Bylaw) including:
 - Port Royal Park
 - Grimston Park
 - Moody Park
 - Hume Park
 - Sapperton Park
 - Westminster Pier Park
 - Queen’s Park
- Permitted hours of consumption – 11:00 a.m. until dusk, seven days a week, for those 19+ years of age.
- Requirement to post signs setting out the boundaries at each site and the hours during which liquor may be consumed. Requirements as to the locations and size of signs.
- Provision for enforcement and offences in accordance with the *Liquor Control and Licensing Act*.

A complete copy of the Bylaw is included with this report as Attachment A.

Concurrent with the adoption of the Consumption of Liquor in Public Places Bylaw, staff committed to undertaking a review of the Program in the fall of 2021.

EXISTING POLICY AND PRACTICE

Bylaw No. 8264, 2021, permits the consumption of liquor in public spaces in New Westminster. The provisions of the Provincial Liquor Control and Licensing Act remain in effect.

The City’s public engagement practices are guided by the Public Engagement Strategy (2016) and Public Engagement Policy (2021). Staff from the Public Engagement Team (Office of the CAO) worked closely with Parks & Recreation staff to plan, implement and report back on engagement to seek feedback on the 2021 Alcohol in Parks Program.

ANALYSIS

In late September 2021, a dedicated Be Heard New West webpage was created and an internal staff workshop was hosted to collect feedback on the Program. The primary objective was to hear back on the public's and staff experience of consumption of alcohol in the designated zones within the seven city parks through the spring/ summer months of 2021. With this feedback and analysis, staff are now in a position to recommend administrative improvements to the Program, largely associated with park grounds and facilities maintenance and operations.

The engagement activities included:

- Be Heard New West Project Web Page launched on September 29, 2021 (308 unique visitors)
- Online survey active from September 29th to October 27th (186 responses)
- Online Poll (14 responses)
- Online Mapping Tool (4 responses)
- Interdepartmental Workshop with City staff on September 17th (10 participants)

The results of the online survey and staff workshop are summarized below. For a fulsome analysis of all feedback, please see Attachment B, the Engagement Summary Report.

Community Engagement Feedback

In response to the evaluation of the Alcohol in Parks Program online engagement, 71% (132/186) respondents reported that they took advantage of the Program; 29% (54/186) did not. Other key feedback included:

- Majority of participants consumed alcohol in Queen's Park (77/129), Westminster Pier Park (71/129) and Hume Park (33/129).
- 81% (148/184) of participants agree that the Program worked well.
- 82% (151/185) of participants are satisfied with how the community is managing their consumption and behavior in parks where alcohol is permitted.
- 65% (118/183) of participants are satisfied with the level of enforcement and oversight of the Program, 11% (20/183) are neutral and 14% (25/ 183) disagree.
- 33% (61/184) of participants think the designated zones are good as-is; 46% (85/184) think they could be expanded; and 21% (38/184) feel the zones should be reconsidered.
- 48% (86/180) rate the program as excellent, 39% (71/180) as good/ alright and 13% (23/180) as not good/ terrible.

The online survey also included an open-ended question regarding suggestions on how the Program can be improved. With 102 responses, the key themes included:

- 1) Offer recycling bins/ Concerns about litter / garbage with the program (29 comments)
- 2) Expand the program to other parks / areas of the city (23)
- 3) General Support/ continue the program (16)
- 4) Expand the existing zones (10)
- 5) End the program / general opposition (9)
- 6) Increase enforcement / monitoring (7)
- 7) Improve signage / add signage about not leaving litter (7)

Interdepartmental Staff Workshop

Overall, staff did not identify any major concerns related to the Alcohol in Parks program, and, did not report a large increase in workloads related to increased cleaning and garbage collection. However, staff did not recommend expanding the program to additional parks, as additional staffing resources would be needed to increase cleaning and maintenance levels any further. Overall, staff participants in the workshop were supportive of continuing the program.

Suggestions for improvements from staff included:

- Explore potential solutions for clearer / better separation of recycling and waste, such as trying different containers, container designs, etc.
- Continue to monitor garbage volumes in parks where alcohol is allowed, and increase pick-ups as needed / possible.
- Continue to monitor how the alcohol-allowed zones are being used for gatherings/events, and consider developing guidelines for staff and the public when it comes to larger gatherings and bookable spaces in New West parks.
- Observe how the use of the Alcohol in Parks program may change / need to adapt based on changing restrictions and behaviours related to the COVID-19 pandemic.

NEXT STEPS

As a result of positive feedback, coupled with minimal concerns, the continuation of the Program is supported with staff review and implementation of improvements related to:

- 1) On-site signage locations and information to mitigate any confusion related to the designated zone boundaries, public washroom locations and how to report issues;
- 2) Number/ type/ location/ servicing of receptacles for disposing of waste/ recycling properly at each park;
- 3) Where and how additional seating may be accommodated in the designated zones;
- 4) How to receive specific feedback from users of bookable picnic shelters and implement changes as required; *and*
- 5) Increased monitoring of each park site to ensure they are safe and welcoming for all.

SUSTAINABILITY IMPLICATIONS

This Program provides an opportunity to enjoy the benefits of socializing outdoors, especially to those who do not have access to a private backyard. This has been particularly important for maintaining social connections during the pandemic. In addition, as a result of the Program, residents are spending more time in local parks and connecting with nature/ the environment.

Continuing the program will likely have a neutral economic impact. At the beginning of the Program there may have been concerns from local restaurants, however, many started to offer take away or “picnic” options through the pandemic. The impact to businesses should continue to be monitored as the program continues.

FINANCIAL IMPLICATIONS

There are no additional financial implications for continuing the Program anticipated at this time.

INTERDEPARTMENTAL LIAISON

Staff from the following Departments participated in the evaluation of the 2021 Alcohol in Parks Program: Office of the CAO, Climate Action, Planning and Development Department, Engineering Services Department, Parks and Recreation Department. The City’s Police Department has reviewed this report.

OPTIONS

The following are options for Council's consideration:

- 1) Continue the Alcohol in Parks Program, as endorsed by Bylaw No. 8264, 2021, in seven designated zones with administrative changes for improvement as outlined in this report.; or
- 2) Provide alternate direction to staff.

Option #1 is recommended.

CONCLUSION

Continuation of the Program provides residents who are gathering in parks for a picnic or special occasion with friends or family, the opportunity to enjoy a beverage outdoors safely and responsibly. Adults who choose to drink in the designated zones within the seven parks are expected to manage their consumption, appropriately dispose of litter, and conduct themselves in a responsible manner. This has largely been the case during the spring and summer of 2021. As City of New Westminster parks are for everyone, it is important that sites remain welcoming and safe for all.

ATTACHMENTS

Attachment A: Bylaw No. 8264, 2021

Attachment B: What We Heard: Alcohol in Parks Pilot – Community Engagement (November 2021)

This report was prepared by:

Erika Mashig, Manager-Parks & Open Space Planning, Design and Construction

Jennifer Miller, Manager of Public Engagement

This report was approved by:

Dean Gibson, Director of Parks and Recreation

Lisa Spitale, Chief Administrative Officer

Attachment "A"

Bylaw No. 8264, 2021

CORPORATION OF THE CITY OF NEW WESTMINSTER

**NEW WESTMINSTER CONSUMPTION OF LIQUOR IN PUBLIC PLACES
BYLAW NO. 8264, 2021**

A Bylaw to Permit Consumption of Liquor in Public Places

WHEREAS pursuant to section 73 (2) of the *Liquor Control and Licensing Act* Council of the City of New Westminster is empowered to designate a public place that it has jurisdiction over as a place where liquor may be consumed;

AND WHEREAS the Council of the City of New Westminster wishes to designate parts of certain parks as a place where liquor may be consumed responsibly;

NOW THEREFORE BE IT RESOLVED that the City Council of the Corporation of the City of New Westminster in open meeting assembled, ENACTS AS FOLLOWS:

1. Citation

This Bylaw may be cited for all purposes as “New Westminster Consumption of Liquor in Public Places Bylaw No. 8264, 2021.”

2. Definitions

- a) **City** means the City of New Westminster;
- b) **City Park** means a Park/Facility as defined in the Parks Regulation Bylaw, 3646, 1959;
- c) **Director or Designate** means the Director of Parks and Recreation or appointed designate(s);
- d) **Dusk** means astronomical dusk, being the dark part of twilight, when the daylight has almost gone but it is not yet completely dark;
- e) **Liquor** means, subject to the *Liquor Control and Licensing Regulations*, beer, wine, spirits or other product that is intended for human consumption and that contains more than 1% of alcohol by volume;
- f) **Liquor Control and Licensing Act** means *Liquor Control and Licensing Act*, SBC 2015, Chapter 19, as amended;

- g) **Liquor Control and Licensing Regulations** means the Liquor Control and Licensing Regulation, B.C. Reg.241/2016 as amended;
- h) **Permitted Space** means that part of a City Park that has been designated under section 3.1 of this Bylaw;
- i) **Public Place** means a place, building or vehicle to which the public is invited or has or is allowed access;
- j) **Sign** has the same meaning as a “sign” under the Sign Bylaw, 7867, 2017.

3. Designation of Permitted Spaces and Setting of Hours

- 3.1 Those outdoor portions of the City Park shown as outlined in thick dashed line in Schedule “A” attached to this Bylaw are hereby designated as Permitted Spaces where liquor may be consumed.
- 3.2 The hours that liquor may be consumed in any of the Permitted Spaces are from 11:00 a.m. until dusk, seven days a week.
- 3.3 Liquor may not be consumed in parts of Public Places that are outside of the boundaries of the Permitted Spaces identified by signage posted in accordance with this Bylaw.

4. Required Signs

- 4.1 The Director or Designate must post signs setting out the boundaries of each Permitted Space and the hours during which liquor may be consumed in the Permitted Space and such signs must:
 - 4.1.2 be at least 0.13 square metres in size;
 - 4.1.3 for each City Park containing a Permitted Space, have at least four (4) signs to designate the boundaries of the Permitted Space, and posted at the main entry points to that City Park;
 - 4.1.4 be posted on posts or affixed to other features forming the boundary of the Permitted Space; and
 - 4.1.5 state the hours that Liquor may only be consumed.

5. Enforcement

Offences set out in the *Liquor Control and Licensing Act* will apply to persons in violation of this Bylaw.

6. Severability

If any part, section, clause or sub-clause of this Bylaw is, for any reason, held to be invalid by the decision of a court of competent jurisdiction, it will be severed and the validity of the remaining provisions of this Bylaw will not be affected.

GIVEN FIRST READING THIS 10th day of May 2021.

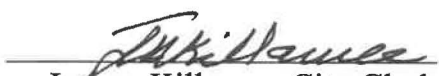
GIVEN SECOND READING THIS 10th day of May 2021.

GIVEN THIRD READING THIS 10th day of May 2021.

ADOPTED THIS 17th day of May 2021.



Mayor Jonathan X. Cote



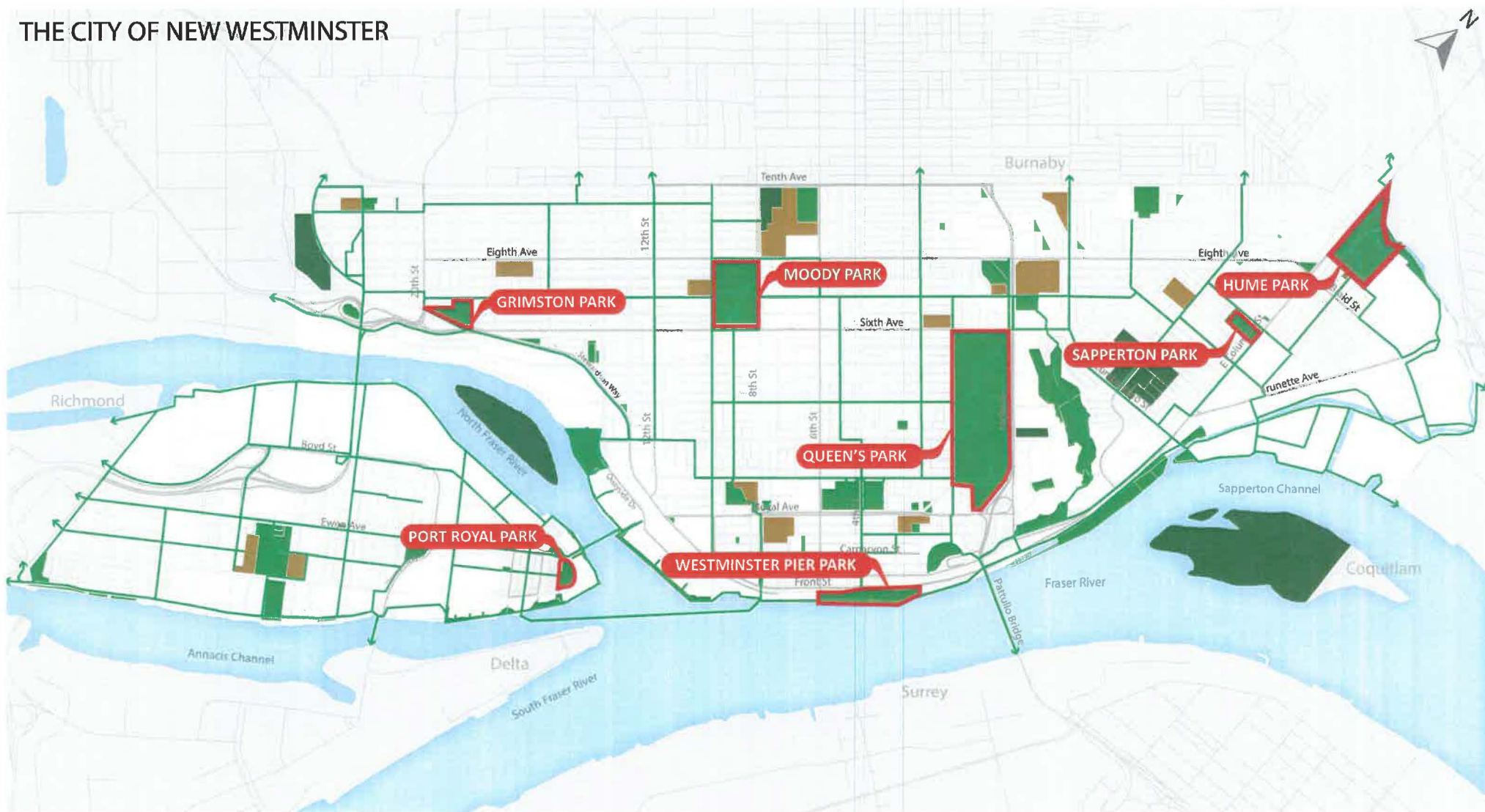
Jacque Killawee, City Clerk

Schedule “A”

Permitted Public Spaces Where Liquor May Be Consumed

NEW WESTMINSTER CONSUMPTION OF LIQUOR IN PUBLIC PLACES BYLAW
NO. 8264, 2021

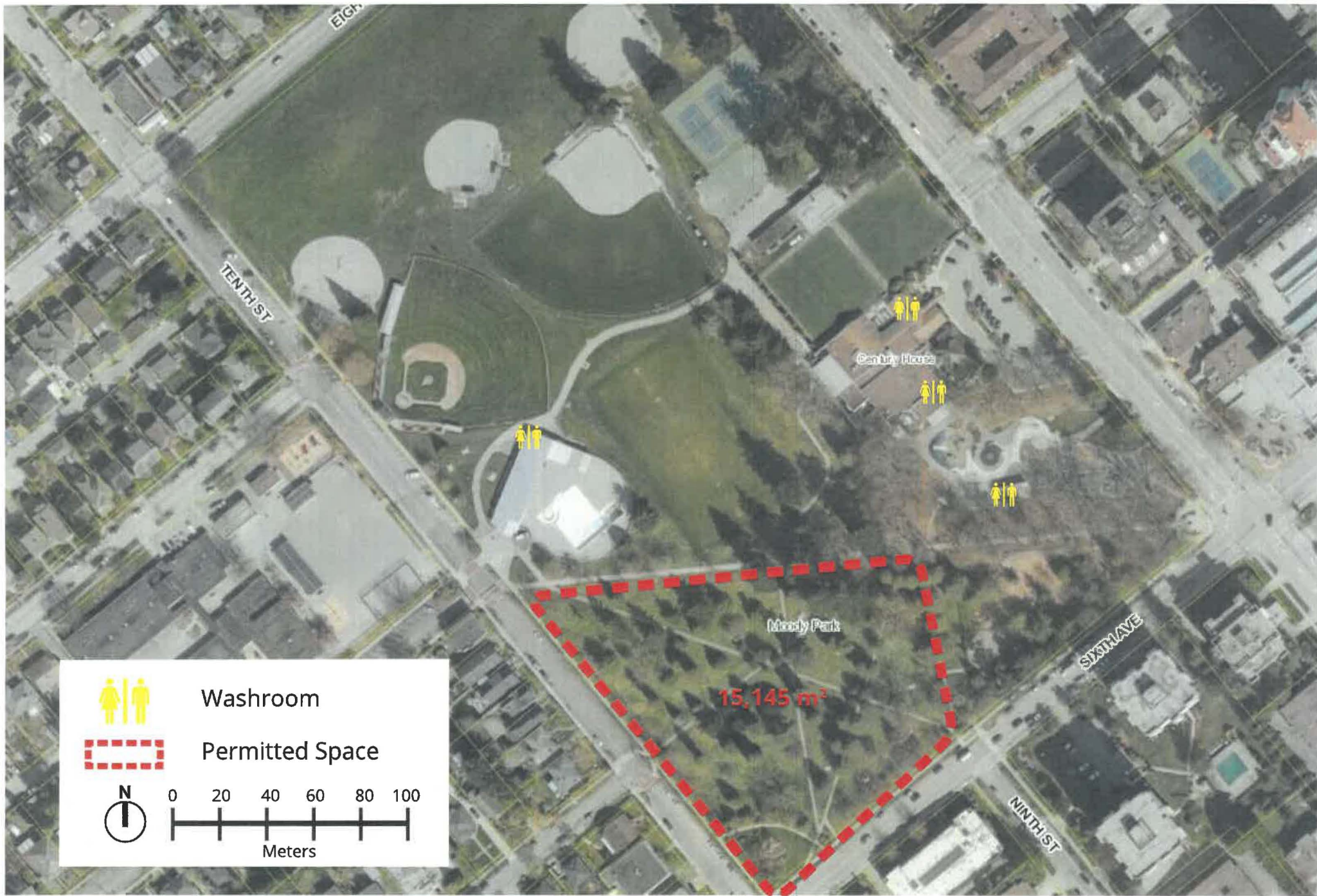
THE CITY OF NEW WESTMINSTER



SEVEN PARKS WITH DESIGNATED AREAS



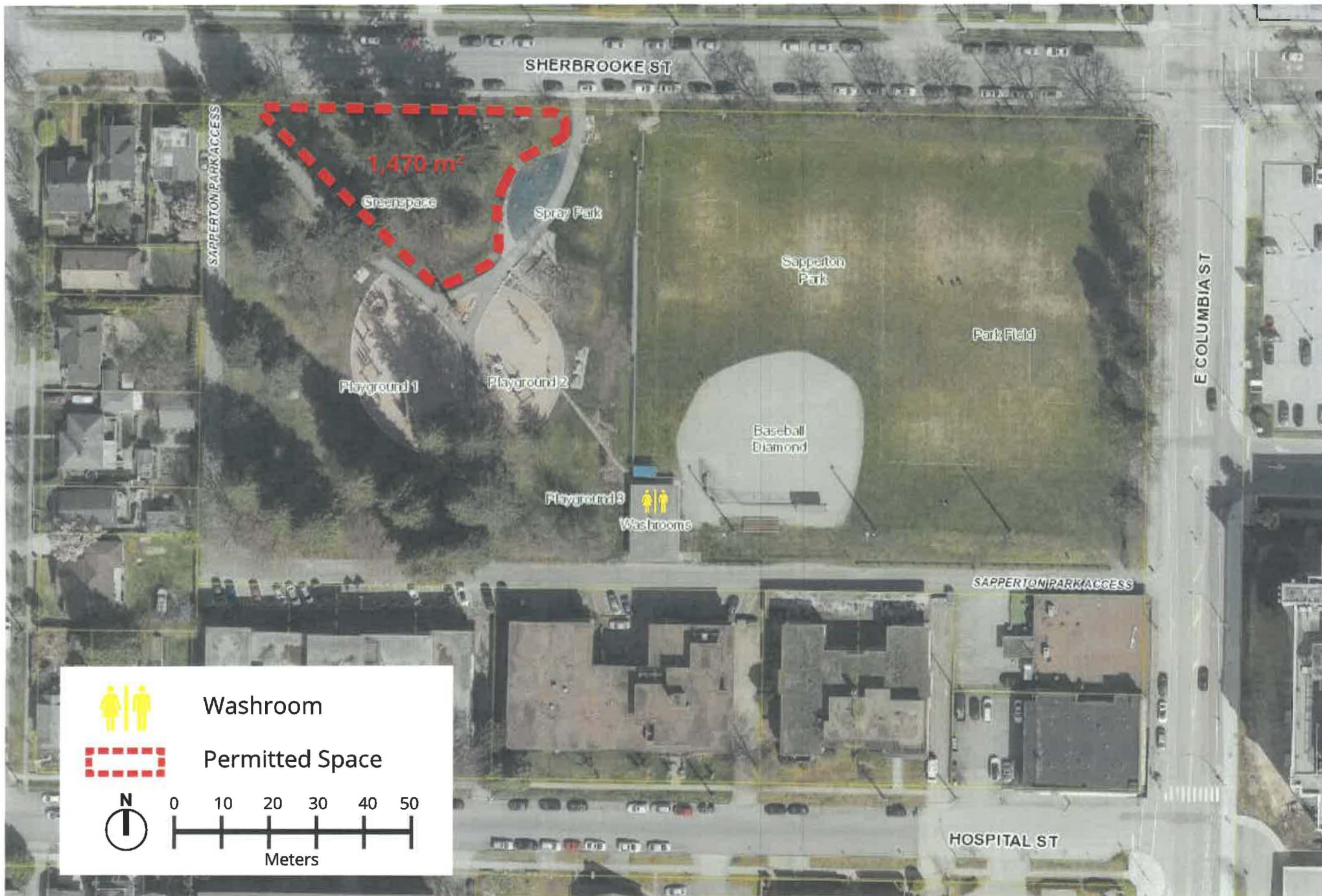
1. PORT ROYAL PARK - Designated Area



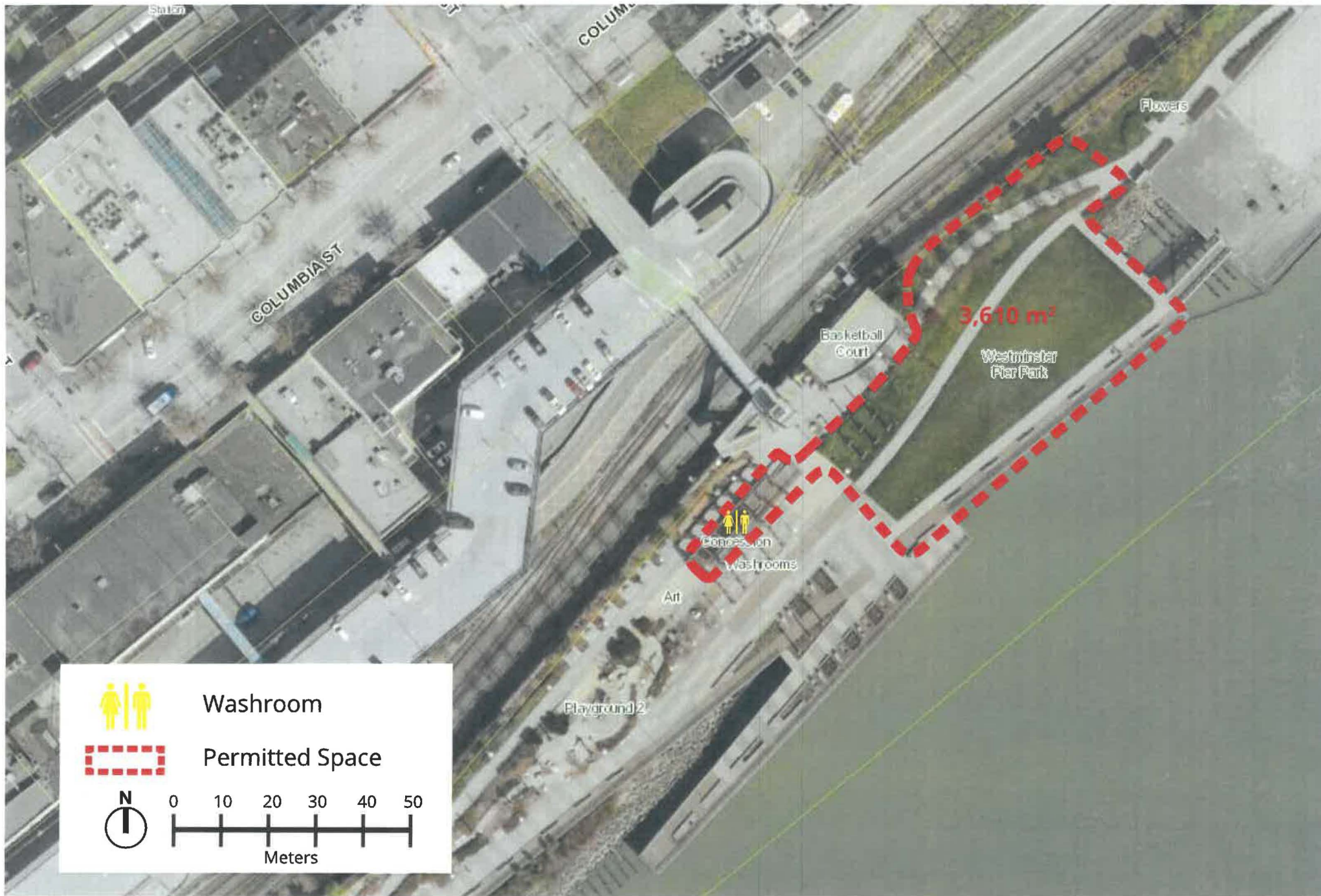
3. MOODY PARK - Designated Area



4. HUME PARK - Designated Areas (x2)



5. SAPPERTON PARK - Designated Area



6. WESTMINSTER PIER PARK - Designated Area



7. QUEEN'S PARK - Designated Areas (x2)

Attachment "B"

What We Heard: Alcohol in Parks Pilot –
Community Engagement (November 2021)



NEW WESTMINSTER

What We Heard: Alcohol in Parks Pilot – Community Engagement

November 2021



Alcohol Allowed Zones



NEW WESTMINSTER

We recognize and respect that New Westminster is on the unceded and unsundered land of the Halkomelem speaking peoples. We acknowledge that colonialism has made invisible their histories and connections to the land. As a City, we are learning and building relationships with the people whose lands we are on.



Alcohol in Parks Pilot Engagement Summary Report

November 13, 2021

Introduction

In May 2021, the City of New Westminster launched a pilot program enabling adults to drink responsibly within designated areas in seven New Westminster parks.

Alcohol consumption is permitted in Grimston Park, Hume Park, Moody Park, Port Royal Park, Queen's Park, Sapperton Park and Westminster Pier Park. Consumption is permitted between 11:00 am until dusk, seven days a week, for those 19+.

As the summer season came to a close, the City was interested in finding out from the community if they took advantage of the new program and how they feel the experience has been.

The purpose of this summary report is to share with City Council, engagement participants, and the New Westminster community:

1. [What engagement activities were completed](#)
2. [A summary of what we heard](#)
3. [Demographic information about participants & representation analysis](#)
4. [Next steps](#)

Engagement Process

The primary purpose of the engagement was to seek feedback from both community members and City staff about how the new Alcohol in Parks program went over the summer, and what people experienced. Engagement activities included an inter-departmental City staff workshop, a community survey and other feedback tools on [Be Heard New West](#).

Engagement Activities

- **Be Heard New West Project Webpage**
 - Launched September 29, 2021
 - 308 unique visitors as of November 13, 2021
- **Online Survey**
 - September 29 – October 27
 - 186 responses
- **Online Poll**
 - September 29 – October 27
 - 14 responses
- **Online Mapping Tool**
 - September 29 – October 27
 - 4 responses

- **Inter-Departmental Workshop with City Staff**
 - September 17 workshop
 - Facilitated by Manager of Public Engagement with participants from Engineering Operations, Parks & Recreation, Communications, Integrated Services (Bylaw), Building Services (Engineering).

The “Ask a Question” tool was also available on the Be Heard New West project page, but no questions have been submitted.

The opportunities to engage were communicated through the following methods:

- Email notification to various City email databases:
 - Be Heard New West subscribers
 - Parks & Recreation newsletter subscribers
 - CityPage online newsletter subscribers
- Social media posts (Facebook, Instagram, Twitter)
- Notices in CityPage in the Record newspaper

Important Note: this summary of engagement input does not reflect a representative sample of the New Westminster community. The input captured here reflects the views of those who self-selected to participate, and may not be representative of the views of other community members and stakeholders. Please see section three – About Participants, starting on Page 22 – for some demographic information and representation analysis.

What We Heard

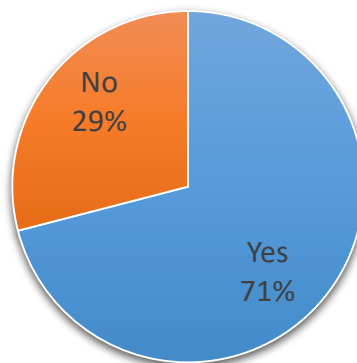
Online Survey

The online survey was open on Be Heard New West from September 29 through October 27 and received 186 responses. Below is a summary of the results.

Question 1: Did you take advantage of the Alcohol in Parks Program (i.e. have you consumed alcohol in one of the designated park areas since May 18, 2021)?

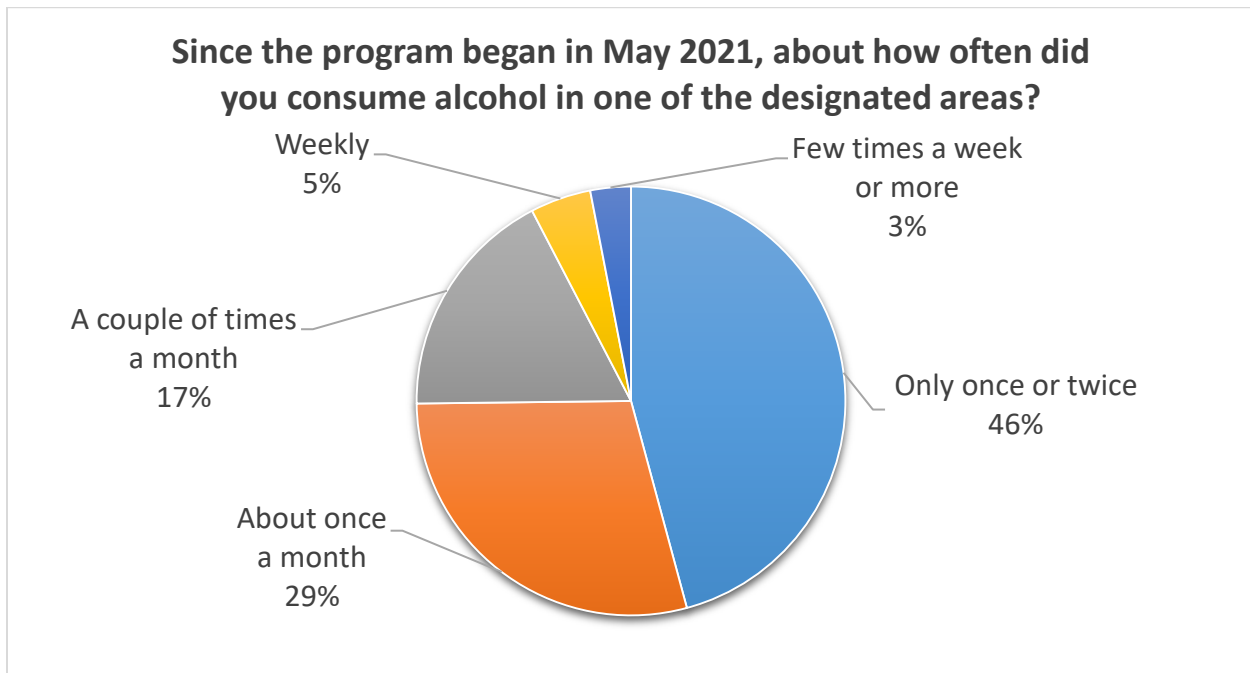
Choice	Percentage	Count
Total Responses: 186		
Yes	71%	132
No	29%	54

Did you take advantage of the Alcohol in Parks Program (i.e. have you consumed alcohol in one of the designated park areas since May 18, 2021)?



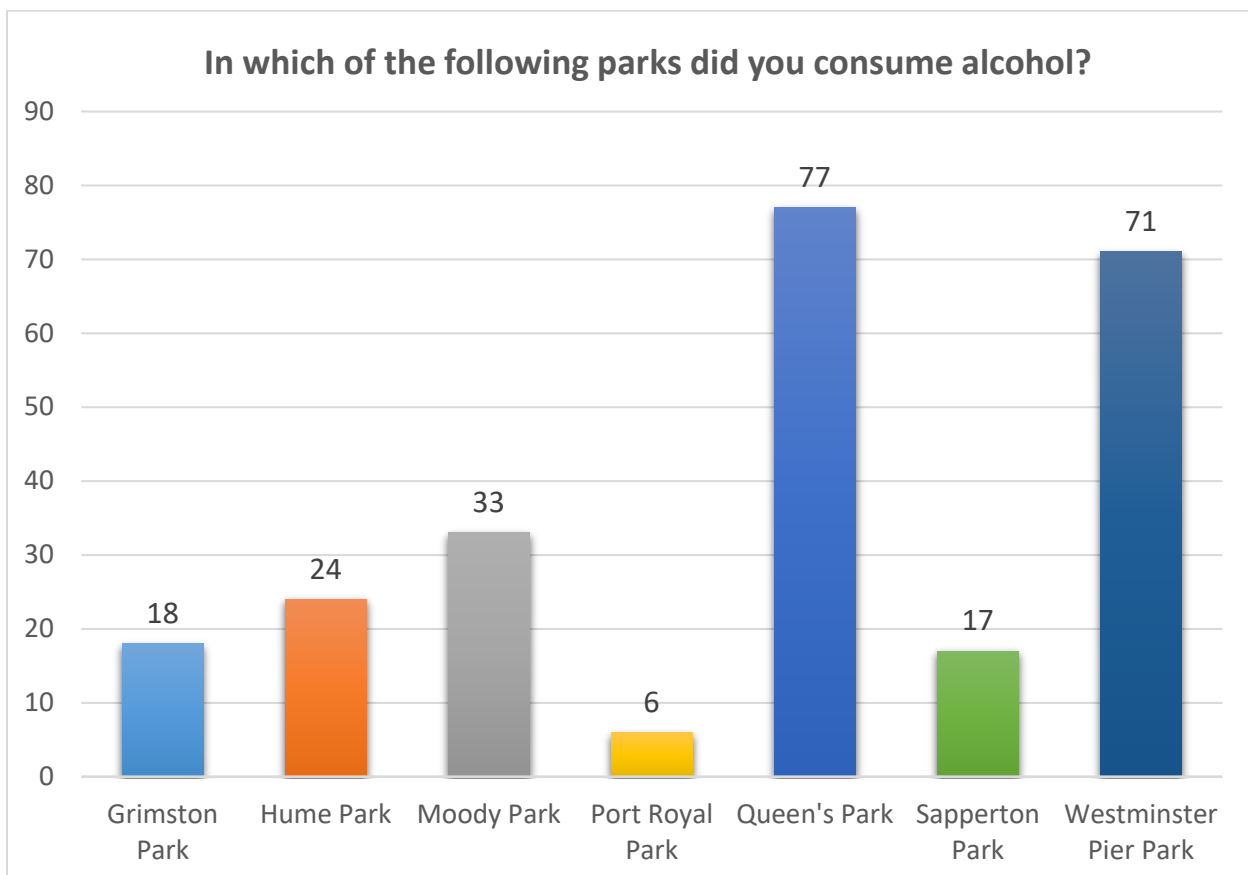
Question 2: Since the program began in May 2021, about how often did you consume alcohol in one of the designated areas?

Choice	Percentage	Count
Total Responses: 131		
Only once or twice	46%	60
About once a month	29%	38
A couple of times a month	17%	23
Weekly	5%	6
Few times a week or more	3%	4



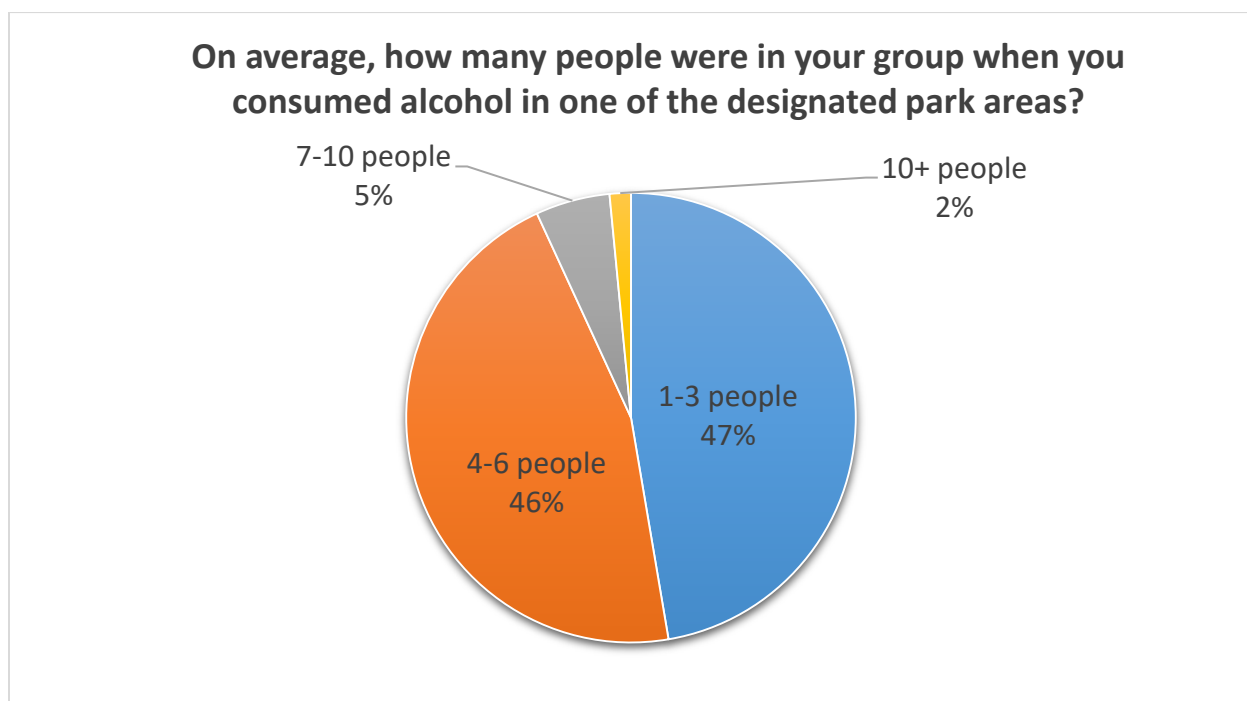
Question 3: In which of the following parks did you consume alcohol? (please select all that apply).

Choice	Count
Total Responses: 246 (from 129 participants)	
Grimston Park	18
Hume Park	24
Moody Park	33
Port Royal Park	6
Queen's Park	77
Sapperton Park	17
Westminster Pier Park	71



Question 4: On average, how many people were in your group when you consumed alcohol in one of the designated park areas?

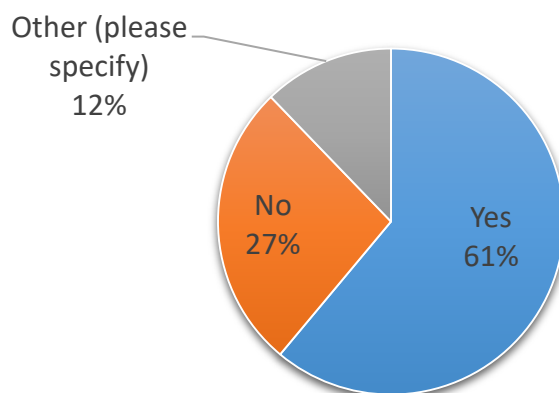
Fee Structure	Percentage	Count
Total responses: 131		
1-3 people	47%	62
4-6 people	46%	60
7-10 people	5%	7
10+ people	2%	2



Question 5: Was your decision to take part in the pilot influenced by the COVID-19 pandemic / public health orders? For example, choosing to socialize outdoors, limiting your indoor contacts, etc.

Choice	Percentage	Count
Total Responses: 131		
Yes	61%	80
No	27%	35
Other (please specify)	12%	16

Was your decision to take part in the pilot influenced by the COVID-19 pandemic / public health orders? For example, choosing to socialize outdoors, limiting your indoor contacts, etc.



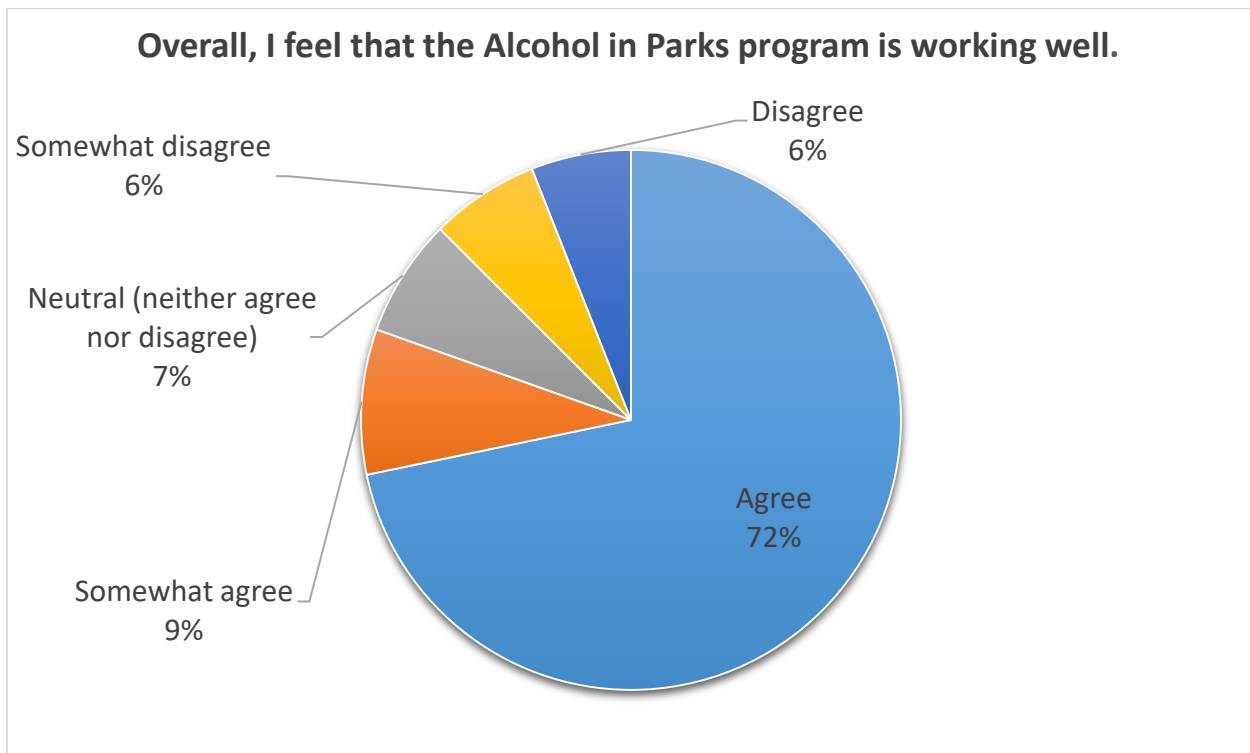
A total of 16 comments were provided by those who chose “other, please specify.” Below is a summary of the key themes across the comments. In some cases, several themes have been applied to one individual response, based on the content; this means that a single response can be counted multiple times.

Theme	# of Mentions	Sample Comments
Yes and no	11	<ul style="list-style-type: none"> - “Yes and no. Definitely limiting indoor gathering and picnics were a great alternative. But at the same time picnics and a drink are nice regardless.” - “yes to covid but also primarily on a nice day, why be inside? I thoroughly enjoyed the freedom. Thanks” - “Pandemic, but also it is nice to have a glass of rosé with your summer picnic, or in listening to music in the park.”
Enjoyment of the outdoors	7	<ul style="list-style-type: none"> - “It was both influenced by the pandemic but also just enjoying being outdoors in a park setting” - “Enjoy being outside in general.” - “I don’t have outdoor space (I live in an apartment)”

Enjoyment of the freedom to drink outdoors	4	<ul style="list-style-type: none"> - "It's just a good idea to drink outside like in Europe" - "Should have had this all along so I took advantage of it been legal now." - "While it was influenced by the pandemic to an extent, primarily I believe out door drinking laws are archaic when people who will drink irresponsibly outdoors will do so regardless."
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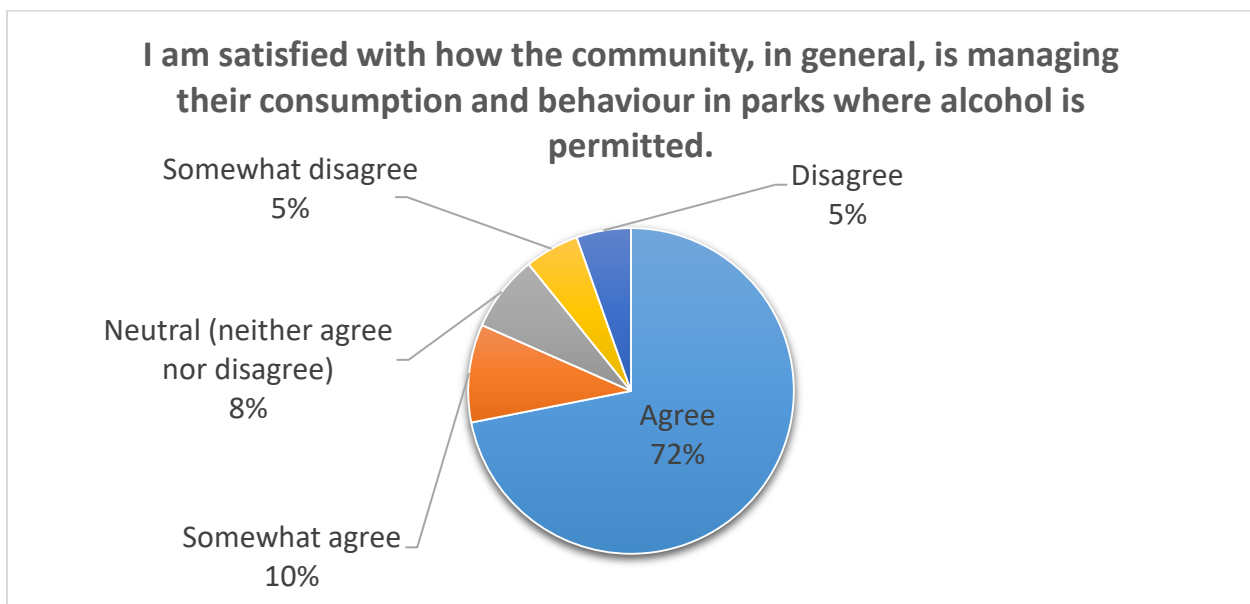
Question 6: Overall, I feel that the Alcohol in Parks program is working well.

Choice	Percentage	Count
Total Responses: 184		
Agree	72%	132
Somewhat agree	9%	16
Neutral (neither agree nor disagree)	7%	13
Somewhat disagree	6%	12
Disagree	6%	11



Question 7: I am satisfied with how the community, in general, is managing their consumption and behaviour in parks where alcohol is permitted.

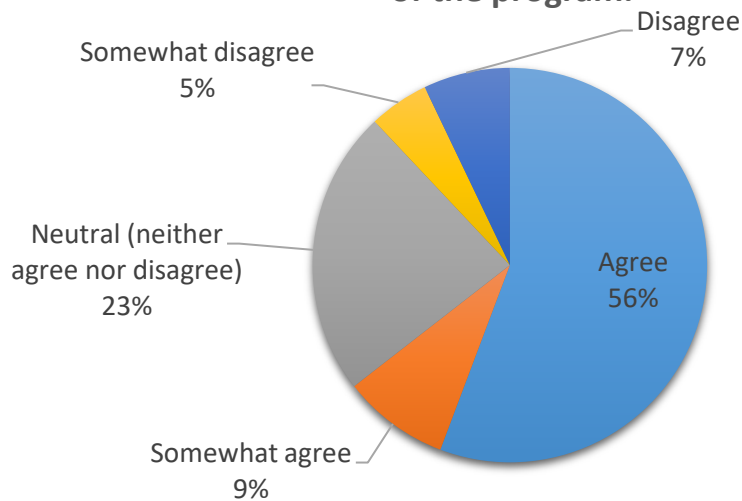
Choice	Percentage	Count
Total Responses: 185		
Agree	72%	133
Somewhat agree	10%	18
Neutral (neither agree nor disagree)	8%	14
Somewhat disagree	5%	10
Disagree	5%	10



Question 8: I am satisfied with the level of City enforcement and oversight of the program.

Choice	Percentage	Count
Total Responses: 183		
Agree	56%	102
Somewhat agree	9%	16
Neutral (neither agree nor disagree)	23%	43
Somewhat disagree	5%	9
Disagree	7%	13

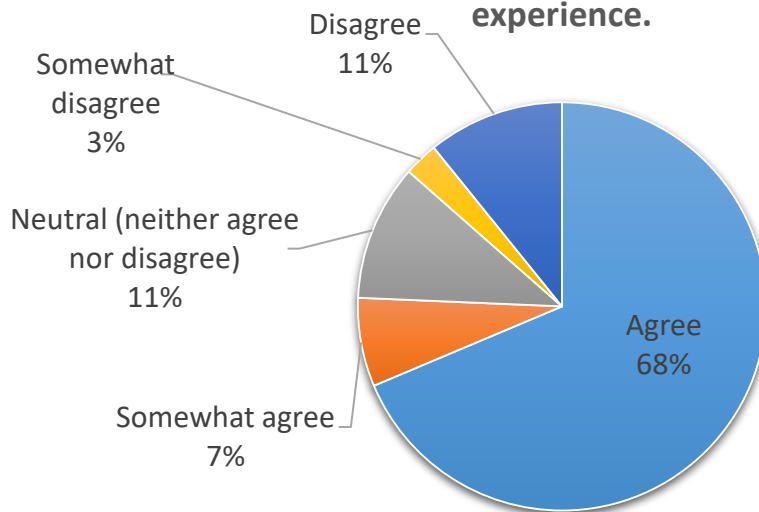
I am satisfied with the level of City enforcement and oversight of the program.



Question 9: This pilot program has had a positive impact on my park experience.

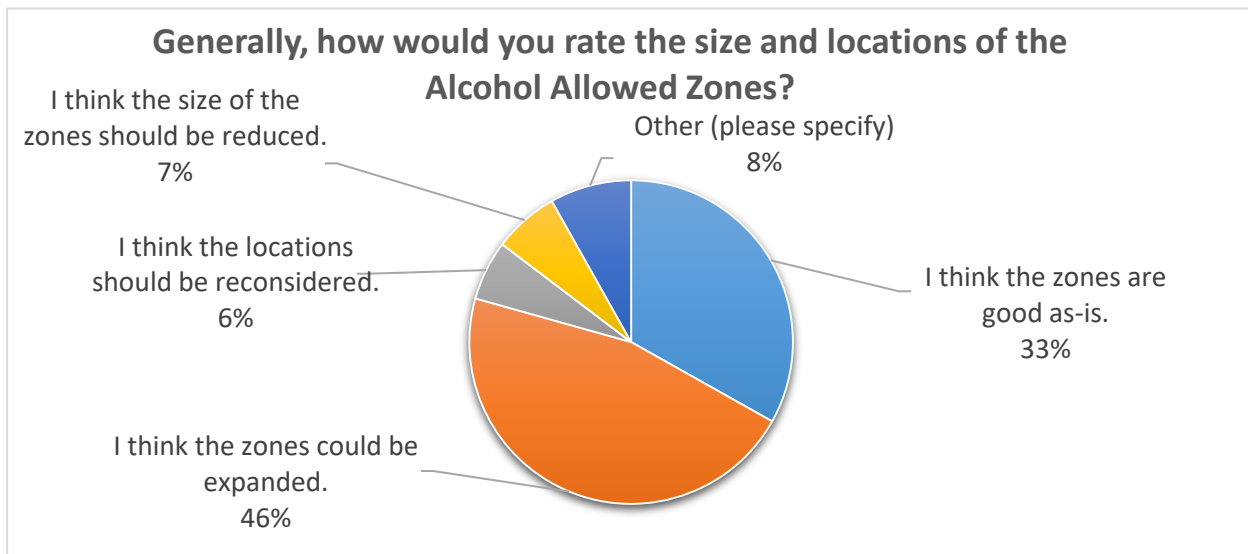
Choice	Percentage	Count
Total Responses: 185		
Agree	69%	127
Somewhat agree	7%	13
Neutral (neither agree nor disagree)	11%	20
Somewhat disagree	3%	5
Disagree	11%	20

This pilot program has had a positive impact on my park experience.



Question 10: Generally, how would you rate the size and locations of the Alcohol Allowed Zones? (Note: for comments specific to one park or zone, please share your feedback using the mapping tool.)

Choice	Percentage	Count
Total Responses: 184		
I think the zones are good as-is.	33%	61
I think the zones could be expanded.	46%	85
I think the locations should be reconsidered.	6%	11
I think the size of the zones should be reduced.	7%	12
Other (please specify)	8%	15



A total of 15 comments were provided by those who chose “other, please specify.” Below is a summary of the key themes across the comments. In some cases, several themes have been applied to one individual response, based on the content; this means that a single response can be counted multiple times. Note that sample comments are not edited for spelling and grammar – they are reported exactly as entered by participants.

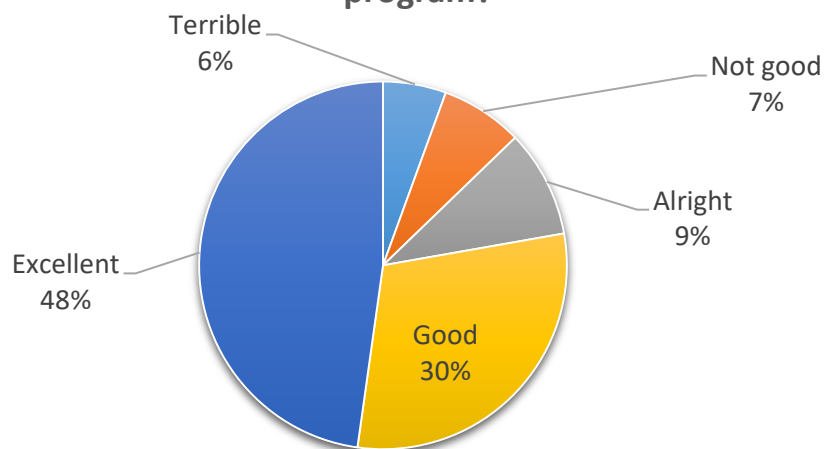
Theme	# of Mentions	Sample Comments
Not sure / don't know where the Zones are	6	<ul style="list-style-type: none"> - “Not sure where the zones were.” - “Not sure, I imagine it remains to be seen? People/communities kinda organically interact with these things and redefine them in unexpected ways.” - “I have no idea what size the zones are. Was looking for another neutral option.”

Disagree with alcohol consumption in park	4	<ul style="list-style-type: none"> - "I disagree with alcohol in parks" - "I don't think alcohol is needed in parks. If there is a party, rent a space and get a license" - "I don't think parks is appropriate location for alcohol consumption."
Remove the Zones	2	<ul style="list-style-type: none"> - "I'm unsure why we need zones." - "Repeal zones and allow alcohol throughout City parks."
Other	4	<ul style="list-style-type: none"> - "I do not believe this experiment has accomplished much of anything" - "I don't believe alcohol should be allowed by playgrounds" - "I think the Moody Park area should be expanded to include the area between the Pool and Century House." - "Queens Park have washrooms, which is great. I work next to Sapperton Park, and there are lots of consumption of Alcohol during the day/evening I am not sure if the washrooms closed at a certain time but I had to deal with feces and urine around the building which is next to the park. More washrooms/water fountain needed moving forward."

Question 11: Based on what you have seen and experienced in the parks and zones where alcohol is permitted, how would you rate this program?

Choice	Percentage	Count
Total Responses: 180		
Terrible	6%	10
Not good	7%	13
Alright	9%	17
Good	30%	54
Excellent	48%	86

Based on what you have seen and experienced in the parks and zones where alcohol is permitted, how would you rate this program?



Question 12: Do you have any suggestions on how the Alcohol in Parks Program could be improved?

This was an optional open-response question and a total of 102 responses were received. Below is a summary of the key themes across the responses. In some cases, several themes have been applied to one individual response, based on the content; this means that a single response can be counted multiple times. Note that sample comments are not edited for spelling and grammar – they are reported exactly as entered by participants.

Theme	# of Mentions	Sample Comments
Expand the program to other parks / areas of the city (Note: this theme includes requests from five participants specifically to add Tipperary Park to the program.)	23	<ul style="list-style-type: none"> - "Expand it to some other parks and plazas" - "Open it up to all parks and don't worry about zones" - "Expand consumption to the boardwalk area along the fraser river" - "Just allow it in all parks." - "Make it City wide!" - "yeah. make it legal everywhere in public. why restrict it to parks?" - "Tipperary Park needs to be included... but we need to get washrooms in there first I presume. Washrooms in Tipperary are sorely needed, outside of this program anyway."

Offer recycling bins	17	<ul style="list-style-type: none"> - "recycling bins in the parks where people can drink" - "We need to manage garbage/recycling facilities better. In many of these parks, I've seen beer cans or bottles sitting around in the morning. We need increased bins to ensure there is a place to put garbage, and increased city workers to pick it up." - "We need recycling bins so people can properly dispose bottles/ cans instead of throwing them in the garbage." - "more recycling totes and signage encouraging patrons to pick up litter"
General support for the program	16	<ul style="list-style-type: none"> - "Great job" - "I feel as though we're finally becoming a civilized place, where adults can be trusted to have a beer or two. Awesome work on the program!" - "I haven't seen any issues - lets roll this out further." - "I think it's a great initiative. I didn't take advantage of it last summer but it would be a lovely thing to do to have a picnic and some wine."
Concerns about litter / garbage associated with the program	12	<ul style="list-style-type: none"> - "Manage the garbage generated by those who won't carry out their empties and waste." - "Responsible adults have been drinking and parks for years unnoticed. What I'm seeing now are irresponsible Drinkers leaving their garbage everywhere and slowly pushing families out of the area. Pier Park in particular" - "The amount of bottles left or shattered near parks where kids play is ridiculous and there needs to be improvements in how parks will stay clean" - "I'm not impressed with the amount of empty bottles and cans left around at night time. I'm not sure if we need more recycling bins, or signage to encourage cleaning up after drinking."
Expand the existing zones	10	<ul style="list-style-type: none"> - "Hume Park zone should include the picnic area in Lower Hume Park" - "I think it should be permitted in the entire park. For instance, the picnic shelter or even near the playground. I think the majority of people manage themselves. I am very used to a European type approach." - "As a parent, I am more likely to be doing a picnic dinner with the kids, which means we would want to be near the playground and in parks with playgrounds. I'd like to see the

		drinking zones expanded to better allow for families with young children.”
End the program / general opposition	9	<ul style="list-style-type: none"> - “Don't agree with the program at all” - “Yes, by ending this program! I have seen many people abusing this program in those areas, increasing the noise and even safety of those around. The police has better things to do than babysitting people who gets drunk in these parks.” - “Eliminate it. Adding alcohol to a park experience is not a positive thing. It is an addictive substance that causes and negatively affects families and society. If you want or need to drink in a park, rent a space and pay for a liquor license.”
Increase enforcement / monitoring	7	<ul style="list-style-type: none"> - “If you are going to have this continue you need much MORE police presence.” - “There are some groups at some parks (specifically Westminster Pier Park) that make the park feel unsafe. These are folks that I think consume alcohol there regardless of the alcohol consumption rules. Would like to see more security presence to feel safer” - “There should be monitoring or action on reports of abuse that triggers police monitoring to deter further abuse. There is a lot of teen/20's drinking that brings drunken behavior, litter, ruining the program for responsible adults (and families)”
Improve signage / add signage about not leaving litter	7	<ul style="list-style-type: none"> - “Signage may be of assistance to ensure approved areas are clear for all park users.” - “Signage in the parks was ok but a pic of area better.” - “I'm not impressed with the amount of empty bottles and cans left around at night time. I'm not sure if we need more recycling bins, or signage to encourage cleaning up after drinking.”

Additional themes with six mentions each included adding more public washrooms in parks (again with several participants specifically naming Tipperary Park), and making the program permanent. Themes with five mentions each included comments that the zones are confusing/unclear, and that alcohol sales should be offered in the alcohol-allowed parks. A theme with four mentions was to add more tables and/or seating.

Online Mapping Tool

The interactive mapping tool was launched on the Be Heard New West project page as an opportunity for community members to provide feedback specific to the “Alcohol Allowed Zones” established for the parks included in the program. Participants were invited to place a pin on the map to share their feedback on the specific areas – blue pins were for general feedback (“comments”) and pink pins were for suggested changes (“concerns”).

The mapping tool was open on Be Heard from September 29 through October 27 and received four submissions. Given the small number of contributions, the verbatim comments and concerns are provided below. Submissions are provided exactly as entered by participants and have not been edited for spelling and grammar.

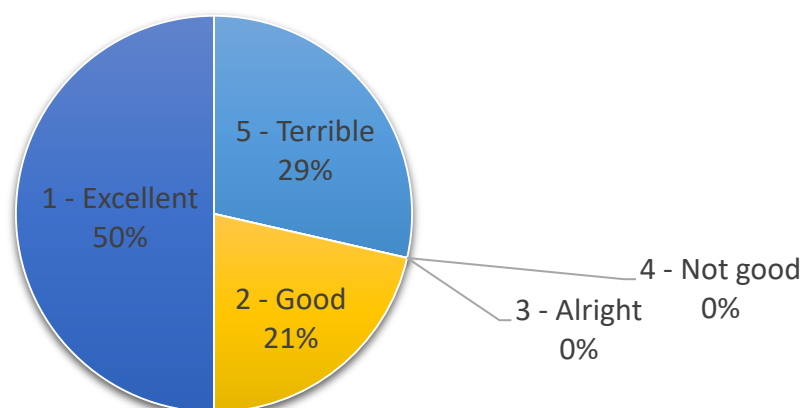
Pin Type	Pin Location	Comment
Comment	Westminster Pier Park Alcohol Allowed Zone	“I would support expanding the permitted alcohol area of the park to include all of the park except for the children's area.”
Concern	Westminster Pier Park Alcohol Allowed Zone	“In the summer I saw many people drinking responsibility while enjoying the park (the picnic tables have been a great addition). Since the weather has changed however, the only people drinking are hard core alcoholics who just sit and get wasted. I have seen more than one person passed out here since the alcohol in parks program began. I have also seen a very definite deterioration of conditions in the park. Used condoms and discarded clothing in the "trails", garbage everywhere (I met a woman in the summer who came to the park every morning and cleaned up the garage herself. Just a good Samaritan). I believe that the alcohol in parks program should be seasonal.”
Concern	Queen's Park Alcohol Allowed Zone	“Too small. No one wants to hang out at the rose garden simply to drink. We want to drink near the kids water park as we watch our kids and picnic.”
Comment	Moody Park Alcohol Allowed Zone	“Although I laughed out loud when i read the comment about a woman wanting to drink by the kids she is in a way correct. These areas are set up for people going to the park to do nothing more than drink. Which is good for Alcoholics, for Health and Safety reasons easier to monitor them if they don't have to hide. I work in Security (and Safety!) Also encourages rapid consuming before you go to the event you intend to attend.”

Quick Poll

A quick, one-question poll was launched on Be Heard New West to gather the community's first impression of the Alcohol in Parks Pilot program. The poll was open from September 29 through October 27, 2021 and received 14 responses. The results were divided, with 71% rating the program either “Excellent” or “Good” and 29% rating it “Terrible”.

Choice	Percentage	Count
Total Responses: 14		
5 -Terrible	29%	4
4- Not good	0%	0
3- Alright	0%	0
2- Good	21%	3
1- Excellent	50%	7

On a scale of 1 (excellent) to 5 (poor) how would you rate the Alcohol in Parks pilot program overall?



Staff Workshop

On September 17, 2021, an inter-departmental workshop was held with City staff, facilitated by the Public Engagement team, to understand how the program was experienced from a staff perspective, any operational impacts, and any concerns or suggestions for changes.

Approximately 10 staff members participated in the workshop, across different departments and operational responsibilities related to: parks maintenance, garbage collection, facility cleaning (including cleaning of park washrooms), bylaw enforcement, parks administration, programming and management, communications, and facilities management.

Overall, staff did not identify any major concerns related to the Alcohol in Parks program, and, despite worries when the program was introduced about potential operational impacts, staff did not report a large increase in workloads related to increased cleaning and garbage collection.

However, staff did not recommend expanding the program to additional parks, as additional staffing resources would be needed to increase cleaning and maintenance levels any further. Overall, participants in the workshop were supportive of continuing the program.

Concerns

While no major concerns were identified, staff noted the following feedback from their teams and members of the public:

- People drinking outside the designated zones – particularly in Westminster Pier Park, Queen's Park and Sapperton Park.
- Increase in overflowing garbage bins, and recyclables ending up in garbage bins and vice-versa.
- Issues related to after-hours drinking, though it was noted this is a general issue that occurred before the Alcohol in Parks program began.
- Concerns around prevalent drinking in Hyack Square (not a designated zone and not recommended to add as a zone).
- Concerns specific to Queen's Park:
 - Public comments about the designated zone being too close to the playground.
 - Complaints from park goers when Queen's Park Arena washrooms were not available (after hours or during private booking).
 - Observed increase in events/gatherings of 15+ people where a permit was not obtained, and sometimes large, organized events such as a wedding with approximately 70 people. Staff noted that an increase in these types of events was observed from the start of the COVID-19 pandemic, but a further increase was observed when the Alcohol in Parks program was introduced. While no major issues were observed related to these events, an increase in garbage was noted.
 - Increase in booking requests for picnic shelters where alcohol is permitted.
 - Complaints about overfull garbage cans, alcohol cans left on tables, etc. in the picnic shelters.

Suggestions Moving Forward:

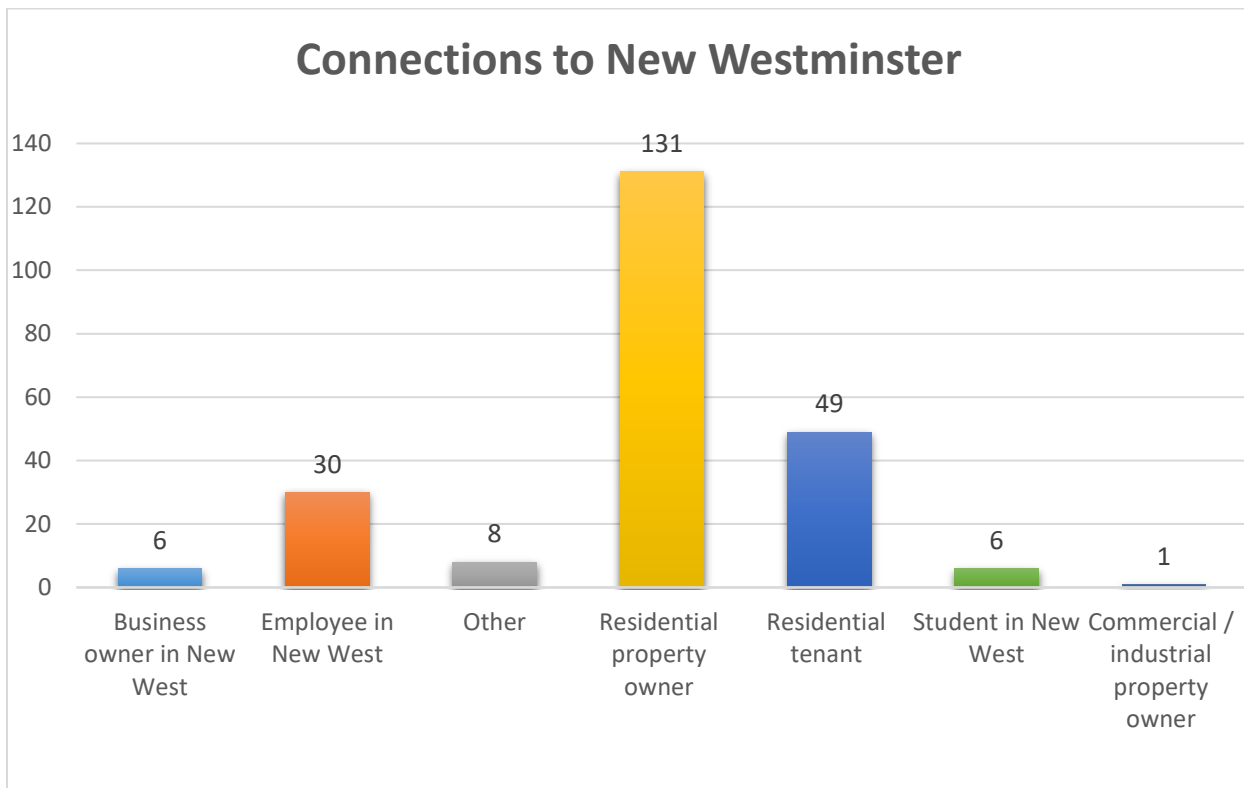
- Explore potential solutions for clearer / better separation of recycling and waste, such as trying different containers / container designs, etc.
- Continue to monitor garbage volumes in parks where alcohol is allowed, and increase pick-ups as needed / possible.
- Continue to monitor how the alcohol-allowed zones are being used for gatherings / events, and consider developing guidelines for staff and the public when it comes to larger gatherings in New West parks.
- Observe how use of the Alcohol in Parks program may change / need to adapt based on changing restrictions and behaviours related to COVID-19.

About Participants

Connections to the City *(select all that apply)*

231 responses from 186 participants

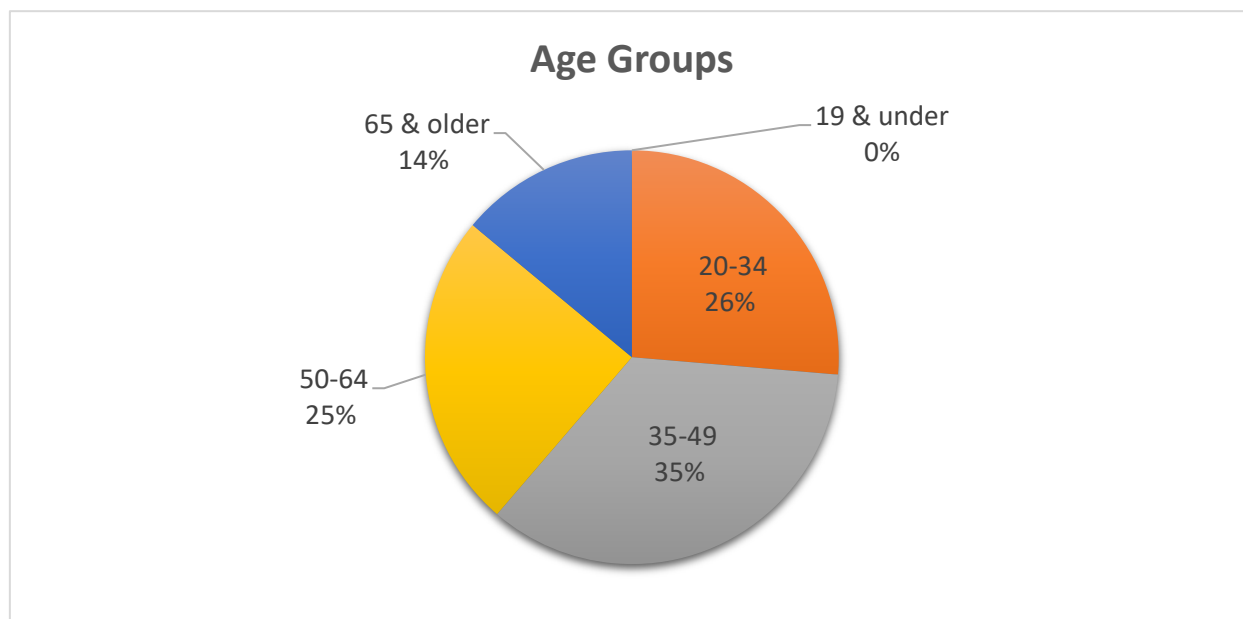
Choice	Percentage	Count
Business owner in New West	3%	6
Employee in New West	16%	30
Other	4%	8
Residential property owner (condo, townhouse, house, etc.) in New West	70%	131
Residential tenant (renter) in New West	26%	49
Student in New West	3%	6
Commercial (including rental buildings) / industrial property owner in New West	1%	1



Age Groups

186 responses

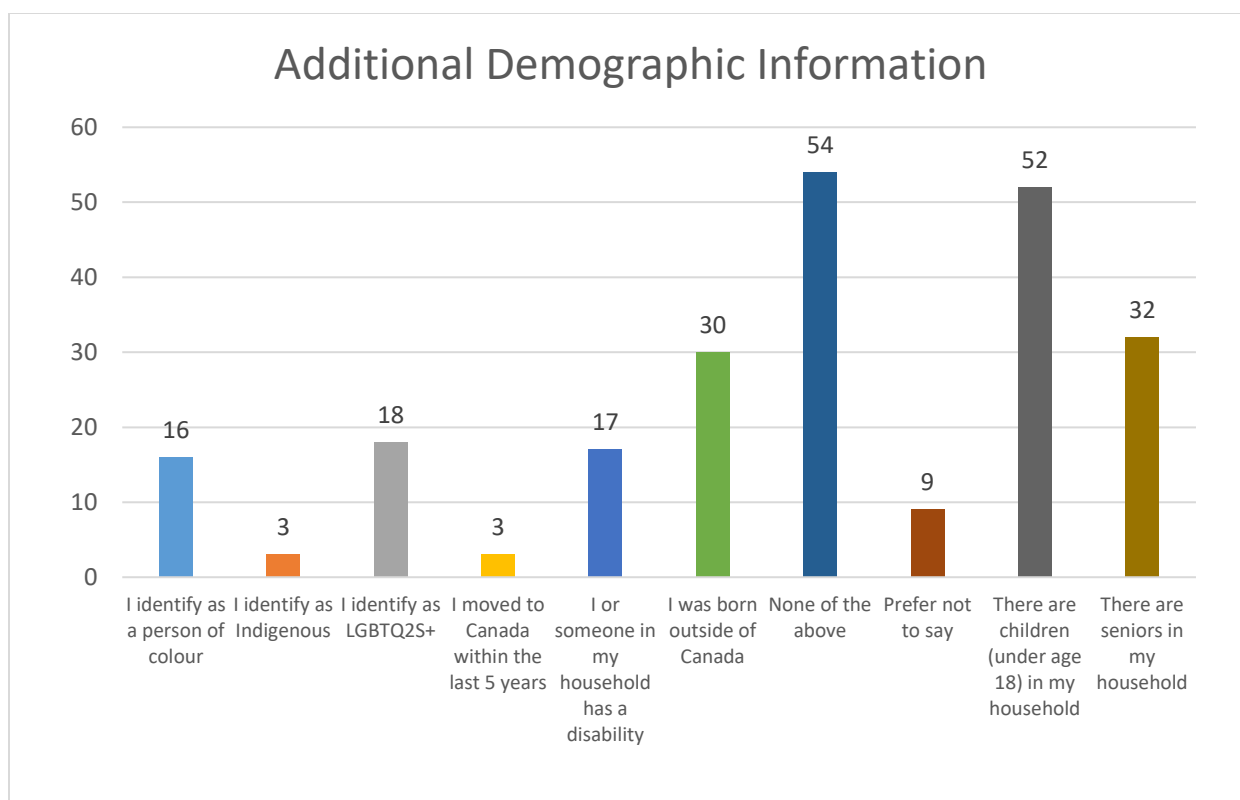
Age Group	Percentage	Count
19 & under	0%	0
20-34	26%	49
35-49	35%	65
50-64	25%	46
65 & older	14%	26



Additional Demographic Information *(select any / all that apply)*

234 responses from 186 participants

Choice	Percentage	Count
I identify as a person of colour	9%	16
I identify as Indigenous	2%	3
I identify as LGBTQ2S+	10%	18
I moved to Canada within the last 5 years	2%	3
I or someone in my household has a disability	9%	17
I was born outside of Canada	16%	30
None of the above	29%	54
Prefer not to say	5%	9
There are children (under age 18) in my household	28%	52
There are seniors in my household	17%	32



Demographic Analysis

As we see in most City of New Westminster engagements, residential tenants were underrepresented (26% of participants; 44% of residents according to Census), while property owners were over represented (70% of participants; 56% of residents according to Census). One hundred and fifty two unique New Westminster postal codes, and two postal codes from outside of the New West, were provided by the 186 participants on Be Heard.

For age ranges of engagement participants, there were no responses from residents age 19 or younger, so this age group was highly underrepresented compared to Census data. However, given the topic of the engagement, and in accordance with provincial liquor laws, no responses from those under the legal drinking age should be expected. Conversely, the 35-49 year old age group was overrepresented (35% of participants; 22% of residents according to Census). Other age groups (20 -34, 50-64 and 65+) were similar (within 5%) to Census representation.

In terms of other demographic information provided by survey participants, we can compare with Census data on Indigenous, immigrant, new immigrant (arrived within past five years) and visible minority proportions of the New Westminster community. Based on this comparison, immigrants and visible minorities were underrepresented among Be Heard participants. Indigenous people and new immigrants were similar (within 5%) to Census representation.

Next Steps

This engagement summary report will be provided to Council at its December 13, 2021 regular meeting, and shared on the Be Heard New West project page. Based on the input from both staff and the community, City staff will provide recommendations about the future of the Alcohol in Parks program for Council's consideration.

REPORT

To:	Mayor Cote and Members of Council	Date:	January 10, 2022
From:	Jacque Killawee City Clerk	File:	05.1035.10
		Item #:	2022-17
Subject:	Amendments to the 2022 Schedule of Council Meetings		

RECOMMENDATION

THAT the 2022 Schedule of Council Meetings be updated as presented in Attachment 1.

PURPOSE

To seek Council's approval of amendments to the 2022 Council Meeting Schedule.

DISCUSSION

At the November 15, 2021 meeting Council approved a schedule for 2022 Council meetings. Upon further review it was determined to revise the schedule by removing the Public Hearings on May 30 and June 27. This change will allow the public to have clarity in the timing of Council work.

OPTIONS

- 1) THAT the 2022 Schedule of Council Meetings be updated as presented in Attachment 1.
- 2) THAT Council provide other direction.

ATTACHMENTS

Attachment 1: Updated 2022 Schedule of Council Meetings

This report was prepared by:

Gillian Day, Agenda Secretary

This report was approved by:

Jacque Killawee, City Clerk

Lisa Spitale, Chief Administrative Officer

Attachment 1

Updated 2022 Schedule of Council Meetings

2022 SCHEDULE OF COUNCIL MEETINGS

Live Webcast: Please note City Council Meetings, Public Hearings, Council Workshops and some Special City Council meetings are streamed online and are accessible through the City's website at: <https://www.newwestcity.ca/council>

Meeting	Time	Location
Closed Council Meeting (confidential) (Closed)	9:00 a.m.	Held electronically
Council Workshop (Workshop)	3:00 p.m.	Held in Council Chamber and electronically
City Council Meeting (Council)	6:00 p.m.	In Council Chamber and electronically
Public Hearing (PH)	6:00 p.m.	In Council Chamber and electronically
On Public Hearing days, a City Council Meeting follows the Public Hearing (Council*)	Follows the Public Hearing	In Council Chamber and electronically

JANUARY	3 No Meeting	10 Closed Council	17 No Meeting	24 No meeting	31 Closed Workshop PH Council*
FEBRUARY	7 No meeting	14 Closed Council	21 No Meeting	28 Closed Workshop PH Council*	
MARCH	7 Closed Council	14 No Meeting	21 No Meeting	28 Closed Workshop PH Council*	
APRIL	4 No Meeting	11 Closed Council	18 No Meeting	25 Closed Workshop PH Council*	
MAY	2 No Meeting	9 Closed Council	16 No Meeting	23 No Meeting	30 Closed Workshop Council
JUNE	6 No meeting	13 Closed Council	20 No Meeting	27 Closed Workshop Council	

JULY	4 No Meeting	11 Closed Council	18 No Meeting	25 No Meeting	
AUGUST	1 No Meeting	8 No Meeting	15 No Meeting	22 No Meeting	29 Closed Workshop Council
SEPTEMBER	5 No Meeting	12 Closed Council	19 Closed Workshop Council (at Qboro)	26 No Meeting	
OCTOBER	3 Closed Council	10 No Meeting	17 No meeting.	24 No Meeting	31 No meeting
NOVEMBER	7 Inaugural Council Meeting	14 No Meeting	21 No meeting	28 Closed Workshop Council	
DECEMBER	5 No Meeting	12 Closed Workshop PH Council*	19 No Meeting	26 No meeting	

Notes:

- Closed Council Meetings are closed to the public and begin at 9:00 a.m.
- Council Workshops are open to the public and begin at 3:00 p.m.
- City Council Meetings are open to the public and begin at 6:00 p.m.
- Public Hearings are open to the public and begin at 6:00 p.m.
- City Council Meetings held to deal with the bylaws considered at the Public Hearings begin immediately following each Public Hearing. These meetings are open to the public.

Meeting dates, times or venues may change from month to month, but notice of the time, date and place of meetings will be given.

*Some personal information is collected and archived by the City of New Westminster under Section 26(g)(ii) of the Freedom of Information and Protection of Privacy Act and for the purpose of the city's ongoing commitment to open and transparent government. If you have any questions about the collection of personal information please contact Legislative Services, 511 Royal Avenue, New Westminster V3L 1H9, 604-527-4523.

REPORT

Parks and Recreation

To: Mayor Cote and Members of Council, **Date:** January 10, 2022

From: Dean Gibson,
Director of Parks & Recreation **File:** 1998069

Item #: 2022-23

Subject: BC Superweek Pro-Cycling Series – New West Grand Prix

RECOMMENDATION

THAT Council receive this report for information, and

THAT Council direct staff to report back in the Fall of 2022, and in advance of the City's 2023 Operating budget deliberations, regarding opportunities to host future New West Grand Prix cycling events.

PURPOSE

This report is provided to inform Council that BC Superweek Pro Cycling Series - New West Grand Prix will not be hosted in the City in 2022 as a result of civil engineering construction projects on Columbia Street and in adjacent areas of the downtown.

SUMMARY

New West Grand Prix (NWGP) has been hosted in the City of New Westminster in 2017, 2018, 2019 and was scheduled to be held in 2020 until the COVID-19 pandemic began. The BC Superweek Pro Cycling Series was cancelled in 2020 and 2021 as a result of Provincial Health Orders (PHO's) regarding gatherings and events. Due to the uncertainty of the pending PHO's and the timeline for the completion of associated construction with the Metro Vancouver sewer on Columbia Street and the Pattullo Bridge replacement, a commitment cannot be made for NWGP in 2022.

BACKGROUND

BC Superweek has existed for over a decade and consists of six separate events including The Tour de Delta, New West Grand Prix, Global Relay Gastown Grand Prix, Giro di Burnaby, PoCo Grand Prix and the Tour de White Rock. The series has a total prize purse of more than \$140,000 and has fast become one of the most prestigious pro-cycling events in North America. In 2017 cyclists from around the world gathered in New Westminster for the inaugural New West Grand Prix (NWGP). An estimated 4,000 spectators gathered in downtown New Westminster to watch the races. Given the success of the first year, Council committed to hosting the NWGP for the remaining three years (2018, 2019 and 2020).

An event of this magnitude relies heavily on the support and cooperation of the Downtown BIA, Tourism New Westminster, many businesses and hundreds of volunteers. Past events have been funded through the City's Special Events operating budget and with additional sponsorship opportunities. In 2019, the financial commitment from the City was \$105,199 (Expenditures - \$155,592 & Sponsorship - \$50,393) for this one-day event.

ANALYSIS

The Special Events team has been in discussions with the organizers of BC Superweek Pro Cycle Series as well as other host cities regarding the BC Superweek Pro Cycling Series scheduled for July 8-17, 2022 (Delta 8-10th / New West 12th / Gastown 13th / Burnaby 14th / PoCo 15th / White Rock 16-17th assuming all host cities return to their previous days of the series). At this time, many host cities have yet to confirm their commitment due to a number of contributing factors such as uncertainty regarding PHO's that will be in place for the series and associated financial impact.

In New Westminster, Parks and Recreation staff have consulted with the Engineering Department regarding the feasibility of hosting the event this summer in light of the two ongoing major construction projects that will impact street closures as well as businesses on the event day. To date timelines have been reviewed and there is no confirmed date of completion of the Metro Vancouver sewer replacement on Columbia Street, and it is unknown as to what condition the road will be in to support a world-class cycling event that would be safe for cyclists and spectators. The Pattullo Bridge project will also be requiring a prolonged closure of Front Street, but the timeline for that is also uncertain. If Front Street is closed by July, a concurrent weekday closure of Columbia Street for the NWGP would be very challenging from a traffic management perspective.

There is also a high degree of uncertainty regarding the amount of sponsorship dollars that would be able to be realized due to many businesses having struggled for the past two years through the COVID-19 pandemic. Businesses have also been impacted by the construction that began in the summer of 2021.

Due to the uncertainty of the pending PHO's and the timeline for the completion of associated construction with the Metro Vancouver sewer on Columbia Street and the Pattullo Bridge replacement, a commitment cannot be made for NWGP in 2022. Staff propose that the City consider the opportunity to host the 2023 NWGP later in this year.

INTERDEPARTMENTAL LIAISON

Parks and Recreation has been in discussions with the Engineering Department regarding confirmation of a timeline for the two constructions projects. Neither department is in a position to make any commitment to this series at this time.

OPTIONS

1. That Council receive this report for information
2. That Council direct staff to report back in the Fall of 2022, and in advance of the with City's 2023 Operating budget deliberations, regarding opportunities to host future New West Grand Prix cycling events.
3. Provide alternate direction.

Options #1 & #2 are recommended.

This report was prepared by:

Renee Chadwick - Manager Special Projects and Community Partnerships

This report was reviewed by:

Mike Anderson – Acting Manager, Transportation

Lisa Kemp – Special Events Coordinator

Dean Gibson – Director Parks and Recreation

Lisa Leblanc – Director of Engineering

This report was approved by:

Dean Gibson, Director Parks and Recreation

Lisa Spitale, Chief Administrative Officer

REPORT

Climate Action, Planning and Development

To: Mayor Cote and Members of Council **Date:** January 10, 2022

From: Emilie K. Adin, MCIP **File:** HER00810
Director, Climate Action, Planning and HER00811
Development

Item #: 2022-4

Subject: **Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022 and Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022 Bylaws for First and Second Readings**

RECOMMENDATION

THAT Council consider Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022 and Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022 for First and Second Readings, and forward the Bylaws to a Public Hearing.

THAT Council add 323 Regina Street to the City's Heritage Register following the adoption of Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022.

PURPOSE

For Council to consider bylaws which would allow the construction of an infill house on a Queen's Park property in exchange for heritage protection and conservation of a heritage house.

EXECUTIVE SUMMARY

A Heritage Revitalization Agreement (HRA) application has been received for 323 Regina Street. Proposed through the HRA (Attachment 1) is a 132 sq. m. (1,420 sq. ft.) rental infill house, and retention and protection of the existing 1928 house with a Heritage Designation Bylaw (Attachment 2). This is one of two remaining in-stream applications in the Queen's Park neighbourhood which were not covered by the pause placed on new HRA applications in June, 2021.

The infill house is proposed to be larger than permitted in the laneway program; however the overall lot density including both buildings is consistent with the density allowed by the Zoning Bylaw and lower than the Conservation Area's incentive program. Two minor zoning setback relaxations would also be required, one for each house.

The proposal is consistent with the Official Community Plan (OCP) land use designation for the site, the Queen's Park Conservation Area's goals of heritage retention and sensitively designed infill, and the current Policy for the Use of Heritage Revitalization Agreements (2011). Applicant-led public consultation was undertaken and the applicant responded to community feedback in three areas: rental tenure, reduced building bulk, and heritage conservation. The proposal was also presented to and supported by the Community Heritage Commission (CHC). Given this, staff recommend that Council consider First and Second Readings, and forward the Bylaws to a Public Hearing.

BACKGROUND

Previous Land Use and Planning Committee Feedback

In July 2021 the proposal was reviewed by the Land Use and Planning Committee (LUPC), which provided feedback on stratification, infill house size, and heritage merit. LUPC directed staff to work with the applicant to resolve the identified issues, which the applicant has done to staff's satisfaction. Minutes from this meeting is attached to this report as Attachment 6.

Policy and Regulations

The site is located in the Queen's Park Heritage Conservation Area, though is not a protected property; protection was removed through the Special Limited Study. The application is consistent with the Conservation Area's goals of protecting heritage buildings while allowing sensitive and appropriate new construction.

The proposal meets the property's Official Community Plan (OCP) land use designation of "Residential Detached and Semi-Detached Housing". Laneway houses are permitted in the property's RS-4 zone, though the proposed infill house is not consistent with those regulations so a rezoning or Heritage Revitalization Agreement (HRA) is required to allow it. An HRA is considered the appropriate tool, as it provides the opportunity to protect the heritage house.

This is one of two remaining in-stream HRA applications in the Queen's Park neighbourhood which were not covered by the pause placed on such applications in June, 2021. The proposal was evaluated against the current Policy for the Use of Heritage Revitalization Agreements (2011). The design of both houses was evaluated against both the Conservation Area's design guidelines as well as the *Standards and Guidelines for the Conservation of Historic Places in Canada*. Further information on the policy and regulatory context of this application is available in Attachment 3.

Site Characteristics and Context

The subject property is 749 sq. m. (8,057 sq. ft.) in size. It is located in the Queen's Park neighbourhood, an area of single-detached dwellings. The property is a corner lot with frontages on Regina Street, Fourth Street, and Sydney Street. All streets are classified as local roads, though Sydney Street is narrow, similar in width to a lane. A site context map and aerial image is provided in Figure 1 below:

Figure 1: Site Context and Aerial Map showing 323 Regina Street highlighted in blue



Information on proximity to transit service and other sustainable transportation options is provided in Attachment 4.

PROJECT PROPOSAL

Overview

An HRA has been proposed for this site which would allow the construction of a 132 sq. m. (1,420 sq. ft.) rental infill house fronting Fourth Street. The existing 1928 house would remain in its current location and would not be enlarged. Both houses would be family friendly, and no secondary suites are proposed. Private outdoor space and vehicle parking requirements would be met for both houses. Project drawings are included in the HRA Bylaw (Attachment 1), and project statistics are available in Attachment 5 and summarized in the following section.

Project Statistics and Relaxations

The density of the existing heritage house is nearly 40% smaller than the maximum density permitted for protected houses in the Conservation Area and roughly 15% smaller than permitted for non-protected houses. The infill house is proposed to exceed

the allowable density under the laneway program. Overall, the total site density would be consistent with the property's zoning entitlement and lower than other similar HRAs (average at 0.65 FSR). A comparison table is below:

Table 1: Comparison of project statistics to regulations

Zoning		QP Incentives	HRA Proposal
Heritage House			
Density (FSR)	0.5	0.68	0.43
Floor Area	374.3 sq. m. (4,029 sq. ft.)	509 sq. m. (5,479 sq. ft.)	320 sq. m. (3,443 sq. ft.)
Infill House			
Density (FSR)	0.1	0.12	0.18
Floor Area	74.9 sq. m. (806 sq. ft.)	89.8 sq. m. (967 sq. ft.)	132 sq. m. (1,420 sq. ft.)
Site Total			
Density	0.6	0.8	0.604
Floor Area	449.1 sq. m. (4,834.5 sq. ft.)	598.8 sq. m. (6,445.5 sq. ft.)	452 sq. m. (4,863 sq. ft.)

Two Zoning Bylaw relaxations related to siting would also be required:

1. Existing (east) side yard setback from the heritage house to the neighbour (smaller by 0.9 m. / 2.9 ft.)
2. Reduced setback from Sydney Street for the infill house (by 0.6 m. / 2 ft.) to lane setback regulations

DISCUSSION

Overall Evaluation

When Council considers entering into a Heritage Revitalization Agreement (HRA) with a property owner, one of the objectives is to balance the benefits to the property owner with the benefits to the public. Additionally, Zoning Bylaw relaxations should be suited to the context of the site and consistent with the City's policies. Three Zoning Bylaw relaxations are proposed to facilitate this project: (1) re-allocation of existing site density; (2) regularize an existing side yard setback (heritage house) and, (3) reduce a side yard setback (infill house) to Sydney Street, to be consistent with the setback requirement for a lane.

Staff considers the relaxations to be minor and that the project is consistent with the City's policy on HRAs and other housing related policies, and to represent a balance of development benefits with community benefits. Given this, the proposal is considered reasonable. Further discussion of the proposed relaxations needed for this project is included below.

Density

Infill House

Through the review process, the density of the infill house was reduced from 0.22 FSR to 0.18 FSR. Although still be larger than permitted, it is consistent with similar past HRA applications. The infill house would be 0.08 FSR (57.1 sq. m. / 614 sq. ft.) larger than a laneway house permitted on this site, and 0.06 FSR (42.1 sq. m. / 453 sq. ft.) larger than the Conservation Area's incentives program would allow. The basement would account for 0.05 FSR (34.4 sq. m. / 370 sq. ft.) which would reduce building bulk from the streetscape. Without the basement, the infill house would be 0.13 FSR (97.5 sq. m. / 1,050 sq. ft.), which is 0.01 FSR (7.5 sq. m. / 81 sq. ft.) above the Conservation Area's incentives program allowance.

Overall Site

Additional density would not be required to facilitate the project. Rather, the unused density from the principal heritage house is proposed to be reallocated to the new infill house. The total combined site density would be 0.604 FSR which is: 1) consistent with the total density allowed by the Zoning Bylaw; 2) lower than the Conservation Area's incentive program; and 3) lower than other similar HRAs (average at 0.65 FSR).

The larger size of the infill building, which does not require additional site density, is considered reasonable in exchange for the Heritage Designation of the principal house. The provision of a ground-oriented, two bedroom unit with recreational spaces and yard space, also fulfills the intentions of the City's goals to develop more ground-oriented family-friendly housing in low density neighbourhoods. Given this, the relaxations proposed are considered reasonable.

Side Yard Setbacks

Relaxations are required for the side yard setback for the heritage house, from 1.5 m. (5 ft.) to 0.6 m. (2.1 ft.), and the setback to Sydney Street for the infill house, from 1.5 m. (5 ft.) to 0.9 m. (3 ft.). The setback relaxation for the heritage house will allow it to remain in its current location, regularizing an existing non-conformity. The setback relaxation for the infill house is against Sydney Street, at the intersection with Fourth Street. Sydney Street has a width of 6.04 m. (19.8 ft.) and functions like a lane, although it is named and considered a street, which results in a larger setback requirement. The proposed relaxation would be consistent with requirements for a lane. Given the above the setback relaxations are considered reasonable.

Heritage Considerations

Heritage Value and Protection

As part of the Queen's Park Heritage Conservation Area's Special Limited Study (see Attachment 3), Council removed protection from this property due to its lack of social-cultural value, i.e. the house is not associated with a significant person, event, tradition, or practice. However, recent historic research by the applicant found a newspaper article which showcased the building and provided details on the various contractors and craftsman, many well-known in the community. With this new information, the Edgar House has been evaluated to have historic, aesthetic, and cultural value. The Heritage Conservation Plan describes its heritage value and includes photographs (Attachment 1, in Appendix 2 of the HRA Bylaw).

At their October 5, 2021 meeting, the Community Heritage Commission (CHC) endorsed the historic values of the house, and its addition to the City's Heritage Register (minutes in Attachment 7).

Heritage Conservation

Updating and restoration work has already been completed on the house (2020). As a non-protected property, this work was not required at that time to be reviewed against the neighbourhood's design guidelines, and a Heritage Alteration Permit was not required. Staff have since reviewed the changes and consider them to be consistent with the Conservation Area's design guidelines. The work has also been evaluated by the project's Heritage Professional who determined that it met *The Standards and Guidelines for the Conservation of Historic Places in Canada*. The Heritage Conservation Plan describes this work (Attachment 1, in Appendix 2 of the HRA Bylaw). The CHC also reviewed the work at their meeting on October 5, 2021 (minutes in Attachment 7).

Applicant Response to Feedback

In response to consultation feedback, the applicant has made changes to their proposal in the following key areas which are considered to address feedback received:

- changed the proposed tenure for the infill house from stratified to rental;
- had previously completed work evaluated by a Heritage Professional confirming its consistency with the Standards and Guidelines for the Conservation of Historic Places in Canada; and
- reduced infill building density (0.22 to 0.18 FSR) and height, upper floor size, and size of the front entry landing which eliminated a relaxation request.

Although greenspace reduction was identified during consultation, the site coverage of the infill house is consistent with the laneway and carriage house development permit guidelines so no relaxations to site coverage are proposed, and there are no protected trees on site.

CONSULTATION

Community Heritage Commission

The project proposal was reviewed by the Community Heritage Commission (CHC) at their meeting on October 6, 2021 (minutes in Attachment 7). In addition to the Conservation Plan, the CHC was also provided with an assessment of conservation work completed in 2019-2020 prior to an HRA application being submitted, against *The Standards and Guidelines for the Conservation of Historic Places in Canada*. Although, there were some concerns identified, the application, heritage designation and registration were supported by the CHC.

Applicant-led Community Consultation

The applicants conducted public consultation, which included a survey that indicated over half the respondents supported the overall project (near 70%). The following issues were cited: too much density for the site; heritage conservation work completed prior to the HRA process; previous removal of Conservation Area protection; and reduction in green space. A summary of the applicant-led consultation, including timeline, notifications, and events and feedback responses are included in Attachment 8. Further information on how these items were addressed are included in the Applicant Response and Revisions section above.

REVIEW PROCESS

The steps in this project's review were as follows, with the current step highlighted in grey:

Table 2: Application Review Stages

#	Stage	Date
1	Formal Application	March 2021
2	Preliminary report to Land Use and Planning Committee	July 12, 2021
3	Preliminary report to Council	August 30, 2021
4	Applicant-led Public Consultation including dissemination of information through the local Residents Association	September 28, 2021 to October 27, 2021
5	Review by the Community Heritage Commission	October 6, 2021
6	Applicant-led online open house	October 13, 2021
8	Council consideration of First and Second Reading of Bylaws (we are here)	January 10, 2022
9	Public Hearing and Council consideration of Third Reading and Adoption of Bylaws	Winter 2022

As there are fewer than five units proposed for the lot, and the form of development is consistent with the Official Community Plan, the application was not forwarded to the New Westminster Design Panel nor the Advisory Planning Committee for review and comment.

NEXT STEPS

Staff is recommending Council forward the HRA Bylaw (Attachment 1) and Heritage Designation Bylaw (Attachment 2) to Public Hearing. A notification sign for the application would be installed on the property and notifications for the Public Hearing would occur in accordance with the City's procedures. Following the Public Hearing, should the Bylaws be adopted, permits issued by the Director of Climate Action, Planning and Development (Heritage Alteration Permit, Building Permit, and Tree Permit) would be required prior to construction.

Servicing, off-site works, and arboricultural requirements have been provided to the applicant. The attached Engineering Services Memo (Attachment 9) outlines the improvements that would be required to facilitate the proposed development. Such improvements would need to be provided in accordance with City standards, as determined by the Director of Engineering Services.

INTERDEPARTMENTAL LIAISON

The City has a project-based team approach for reviewing development applications. A staff-led project team was assigned for reviewing this project consisting of staff from Engineering (Servicing and Transportation), Fire, Electrical, Parks and Recreation, and Climate Action, Planning and Development (Building, Planning, Trees, and Heritage) Departments who provided comments throughout the development review process.

OPTIONS

The following options are available for Council's consideration:

1. That Council consider Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022 and Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022 for First and Second Readings, and forward the Bylaws to a Public Hearing.
2. That Council add 323 Regina Street to the City's Heritage Register following the adoption of Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022.
3. That Council provide staff with alternative direction.

Staff recommend option 1 and 2.

ATTACHMENTS

- Attachment 1: Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022
- Attachment 2: Heritage Designation (323 Regina Street) Bylaw No. 8305, 2022
- Attachment 3: Policies and Regulations Summary
- Attachment 4: Proximity to Transit Service and Other Sustainable Transportation Options
- Attachment 5: Proposed Project Statistics and Relaxations
- Attachment 6: Extract of July 12, 2021 Land Use and Planning Committee (LUPC) Meeting Minutes
- Attachment 7: Extract of Oct 6, 2021 Community Heritage Commission (CHC) Meeting Minutes
- Attachment 8: Applicant-led Consultation Feedback and Correspondence Received
- Attachment 9: Engineering Servicing Memo

APPROVALS

This report was prepared by:
Kathleen Stevens, Heritage Planning Analyst

This report was reviewed by:
Britney Dack, Senior Heritage Planner
Rupinder Basi, Supervisor of Development Planning
Jackie Teed, Manager of Climate Action, Planning and Development

This report was approved by:
Emilie K. Adin, Director, Climate Action, Planning and Development
Lisa Spitale, Chief Administrative Officer

Attachment 1

Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022

**THE CORPORATION OF THE CITY OF NEW WESTMINSTER
HERITAGE REVITALIZATION AGREEMENT (323 Regina Street)
BYLAW NO. 8304, 2022**

**A Bylaw to enter into a Heritage Revitalization Agreement under
Section 610 of the *Local Government Act***

WHEREAS the City of New Westminster and the owners of the property located at 323 Regina Street in New Westminster wish to enter into a Heritage Revitalization Agreement in respect of the property;

NOW THEREFORE, the Council of the City of New Westminster enacts as follows:

Citation

1. This Bylaw may be cited as “Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022”.

Heritage Revitalization Agreement

2. The City of New Westminster enters into a Heritage Revitalization Agreement with the registered owners of the property located at 323 Regina Street legally described as PID: 013-593-285; LOT 12 OF LOT 4 SUBURBAN BLOCK 10 PLAN 2620.
3. The Mayor and City Clerk are authorized on behalf of the City of New Westminster Council to sign and seal the Heritage Revitalization Agreement attached to this Bylaw as Schedule “A”.

READ A FIRST TIME this _____ day of _____, 2022.

READ A SECOND TIME this _____ day of _____, 2022.

PUBLIC HEARING held this _____ day of _____, 2022.

READ A THIRD TIME this _____ day of _____, 2022.

ADOPTED this _____ day of _____, 2022.

MAYOR JONATHAN X. COTE

JACQUE KILLAWEE, CITY CLERK

SCHEDULE "A"**HERITAGE REVITALIZATION AGREEMENT (323 Regina Street)**

THIS AGREEMENT dated for reference the 1st day of December, 2021 is

BETWEEN:

GARY JOHN HOLISKO and ROSANNE MARIE HOOD,
323 Regina Street, New Westminster, BC
V3L 1S8

(together, the "Owners")

AND:

THE CORPORATION OF THE CITY OF NEW WESTMINSTER, City Hall, 511 Royal
Avenue, New Westminster, BC V3L 1H9

(the "City")

WHEREAS:

- A. The Owners are the registered owners in fee simple of the land and all improvements located at 323 Regina Street, New Westminster, British Columbia, legally described as PID: 013-593-285; LOT 12 OF LOT 4 SUBURBAN BLOCK 10 PLAN 2620 (the "Land");
- B. There is one principal building situated on the Land, known as the Edgar House (the "Heritage Building"), which is shown on the site plan attached as Appendix 1 (the "Site Plan") labelled as "323 Regina Street";
- C. The City and the Owner agree that the Heritage Building has heritage value and should be conserved;
- D. The Owner wishes to make certain alterations to restore and rehabilitate the Heritage Building (the "Work");
- E. The Owners intend to construct a two storey infill house on the lands, measuring approximately 132 square meters in size (the "Infill House") on that portion of the Land labelled on the Site Plan as "471 Fourth Street Coach House";
- F. Section 610 of the *Local Government Act*, RSBC 2015, Chapter 1 authorizes a local government to enter into a Heritage Revitalization Agreement with the owner of heritage property, and to allow variations of, and supplements to, the provisions of a bylaw or a permit issued under Part 14 or Part 15 of the *Local Government Act*;

- G. The Owner and the City have agreed to enter into this Heritage Revitalization Agreement setting out the terms and conditions by which the heritage value of the Heritage Building is to be preserved and protected, in return for specified supplements and variances to City bylaws;

THIS AGREEMENT is evidence that in consideration of the sum of ten dollars (\$10.00) now paid by each party to the other and for other good and valuable consideration (the receipt of which each party hereby acknowledges) the Owner and the City each covenant with the other pursuant to Section 610 of the *Local Government Act* as follows:

Conservation of Heritage Building

1. Upon execution of this Agreement, the Owner shall promptly commence the Work in accordance with the Heritage Conservation Plan prepared by Katie Cummer, PhD CAHP, of Cummer Heritage Consulting dated July 24, 2021, a copy of which is attached hereto as Appendix 2 (the "Conservation Plan"), and the design plans and specifications prepared by Nancy G Dheilley, dated AUG 6, 2021, NOV 8, 2021, and NOV 17, 2021, a copy of which is attached hereto as Appendix 5 (the "Approved Plans"), full-size copies of which plans and specifications are on file at the New Westminster City Hall.
2. Prior to commencement of the Work, the Owner shall obtain from the City all necessary permits and licenses, including a heritage alteration permit, building permit, and tree permit.
3. The Owner shall obtain written approval from the City's Director of Climate Action, Planning and Development for any changes to the Work, and obtain any amended permits that may be required for such changes to the Work, as required by the City.
4. The Owner agrees that the City may, notwithstanding that such permits may be issuable under the City's zoning and building regulations and the BC Building Code, withhold a heritage alteration permit or building permit applied for in respect of the Heritage Building if the work that the Owner wishes to undertake is not in accordance with the Conservation Plan or the Approved Plans.
5. The Work shall be done at the Owner's sole expense in accordance with generally accepted engineering, architectural, and heritage conservation practices. If any conflict or ambiguity arises in the interpretation of Appendix 2, the parties agree that the conflict or ambiguity shall be resolved in accordance with the "Standards and Guidelines for the Conservation of Historic Places in Canada", 2nd edition, published by Parks Canada in 2010.
6. The Owner shall, at the Owner's sole expense, erect on the Land and keep erected throughout the course of the Work, a sign of sufficient size and visibility to effectively notify contractors and tradespersons entering onto the Land that the Work involves protected heritage property and is being carried out for heritage conservation purposes.
7. The Owner shall, at the Owner's sole expense, engage a member of the Architectural Institute of British Columbia or the Association of Professional Engineers and Geoscientists of British Columbia or the British Columbian Association of Heritage Professionals with

specialization in Building or Planning (the “Registered Professional”) to oversee the Work and to perform the duties set out in section 8 of this Agreement, below.

Role of Registered Professional

8. The Registered Professional shall:
 - (a) prior to commencement of the Work, and at any time during the course of the Work that a Registered Professional has been engaged in substitution for a Registered Professional previously engaged by the Owner, provide to the City an executed and sealed Confirmation of Commitment in the form attached as Appendix 3 and, if the Registered Professional is a member of the Canadian Association of Heritage Professionals, the Registered Professional shall provide evidence of their membership and specialization when submitting such executed Confirmation of Commitment;
 - (b) conduct field reviews of the Work with the aim of ensuring compliance of the Work with the Conservation Plan in Appendix 2;
 - (c) provide regular reports to the City’s Climate Action, Planning and Development Department, on the progress of the Work;
 - (d) upon substantial completion of the Work, provide to the City an executed and sealed Certification of Compliance in the form attached as Appendix 4; and
 - (e) notify the City within one business day if the Registered Professional’s engagement by the Owner is terminated for any reason.

Heritage Designation

9. The Owner irrevocably agrees to the designation of the Heritage Building as protected heritage property, in accordance with Section 611 of the *Local Government Act*, and releases the City from any obligation to compensate the Owner in any form for any reduction in the market value of the Lands or the Heritage Building that may result from the designation.
10. Following completion of the Work, the Owner shall maintain the Heritage Building in good repair in accordance with the Conservation Plan in Appendix 2 and the maintenance standards set out in City of New Westminster Heritage Properties Minimum Maintenance Standards Bylaw No. 7971, 2018, as amended or replaced from time to time, and, in the event that Bylaw No. 7971 is repealed and not replaced, the Owner shall continue to maintain the building to the standards that applied under Bylaw No. 7971 immediately prior to its repeal.
11. Following completion of the Work in accordance with this Agreement, the Owner shall not alter the heritage character or the exterior appearance of the Heritage Building, except as permitted by a heritage alteration permit issued by the City.

Damage to or Destruction of Heritage Building

12. If the Heritage Building is damaged, the Owner shall obtain a heritage alteration permit and any other necessary permits and licenses and, in a timely manner, shall restore and repair the Heritage Building to the same condition and appearance that existed before the damage occurred.
13. If, in the opinion of the City, the Heritage Building is completely destroyed, the Owner shall construct a replica, using contemporary material if necessary, of the Heritage Building that complies in all respects with the Conservation Plan in Appendix 2, the Approved Plans in Appendix 5, and with City of New Westminster Zoning Bylaw No. 6680, 2001 as amended (the "Zoning Bylaw"), as varied by this Agreement, after having obtained a heritage alteration permit and any other necessary permits and licenses.
14. The Owner shall use best efforts to commence and complete any repairs to the Heritage Building, or the construction of any replica building, with reasonable dispatch.

Construction of the Infill House

15. The Owners shall construct the Infill House in strict accordance with the Site Plan and the Approved Plans prepared by Nancy G Dheilly, dated NOV 8, 2021 and NOV 17, 2021, a copy of which is attached hereto as Appendix 5, full-size copies of which plans and specifications are on file at the New Westminster City Hall.
16. Prior to commencement of construction of the Infill house, the Owner shall obtain from the City all necessary approvals, permits, and licenses, including a heritage alteration permit, building permit, and tree permit.
17. The Owner shall obtain written approval from the City's Director of Climate Action, Planning and Development for any changes to the Infill House, and obtain any amended permits that may be required for such changes to the Infill House, as required by the City.
18. The Owner agrees that the City may, notwithstanding that such permits may be issuable under the City's zoning and building regulations and the BC Building Code, withhold a heritage alteration permit or building permit applied for in respect of the Infill House if the work that the Owner wishes to undertake is not in accordance with the Approved Plans.
19. The construction of the Infill House shall be done at the Owner's sole expense and in accordance with generally accepted engineering and architectural practices.

Timing and Phasing

20. The Owner shall commence and complete all actions required for the completion of the Work, as set out in the Conservation Plan in Appendix 2, within three years following the date of adoption of the Bylaw authorizing this Agreement.

21. The Owner shall not construct the Infill House on the Land until the Owner has completed the Work in respect of the Heritage Building to the satisfaction of the City's Director of Climate Action, Planning and Development, has provided the Certification of Compliance described in section 8(d) above.
22. The City may, notwithstanding that such a permit may be issuable under the City's zoning and building regulations and the BC Building Code, withhold a building permit or heritage alteration permit applied for in respect of the Infill House if the Owner has not completed the Work in respect of the Heritage Building, to the satisfaction of the City's Director of Climate Action, Planning and Development.
23. The Owner shall complete all actions required for the completion of the Infill House, as set out in Approved Plans in Appendix 5, within five years following the date of adoption of the Bylaw authorizing this Agreement.

No Subdivision

24. The Owners shall not subdivide the Lands or the buildings located on the Lands by any method, including by way of a building strata plan under the provisions of the Strata Property Act (British Columbia), or any successor legislation dealing with the creation of separate titles to buildings or portions of a building.

Inspection

25. Upon request by the City, the Owners shall advise or cause the Registered Professional to advise, the City's Climate Action, Planning and Development Department, of the status of the Work.
26. Without limiting the City's power of inspection conferred by statute and in addition to such powers, the City shall be entitled at all reasonable times and from time to time to enter onto the Land for the purpose of ensuring that the Owner is fully observing and performing all of the restrictions and requirements in this Agreement to be observed and performed by the Owner.
27. The Owner agrees that the City may, notwithstanding that a final inspection may be issuable under the City's zoning and building regulations and the BC Building Code, withhold a final inspection or occupancy certificate applied for in respect of the Heritage Building or the Infill House if the Owner has not completed the Work with respect to the Heritage Building or construction of the Infill House to the satisfaction of the City's Director of Climate Action, Planning and Development.

Conformity with City Bylaws

28. The City of New Westminster Zoning Bylaw No. 6680, 2001, is varied and supplemented in its application to the Land in the manner and to the extent provided and attached as Appendix 6.

29. The Owner acknowledges and agrees that, except as expressly varied by this Agreement, any development or use of the Land, including any construction, alteration, rehabilitation, restoration and repairs of the Heritage Building or Infill house, must comply with all applicable bylaws of the City.

No Application to Building Interiors

30. Unless otherwise stated in this Agreement or set out in the Conservation Plan, the terms and conditions of this Agreement respecting the Heritage Building and Infill House apply only to the structure and exterior of the buildings, including without limitation the foundation, walls, roof, and all exterior doors, stairs, windows and architectural ornamentation.

Enforcement of Agreement

31. The Owner acknowledges that it is an offence under Section 621(1)(c) of the *Local Government Act* to alter the Land or the Heritage Building in contravention of this Agreement, punishable by a fine of up to \$50,000.00 or imprisonment for a term of up to 2 years, or both.
32. The Owner acknowledges that it is an offence under Section 621(1)(b) of the *Local Government Act* to fail to comply with the requirements and conditions of any heritage alteration permit issued to the Owner pursuant to this Agreement and Section 617 of the *Local Government Act*, punishable in the manner described in the preceding section.
33. The Owner acknowledges that, if the Owner alters the Land, the Heritage Building or the Infill House in contravention of this Agreement, the City may apply to the British Columbia Supreme Court for:
- (a) an order that the Owner restore the Land or the Heritage Building or the Infill House, or all, to their condition before the contravention;
 - (b) an order that the Owner undertake compensatory conservation work on the Land, the Heritage Building, or the Infill House;
 - (c) an order requiring the Owner to take other measures specified by the Court to ameliorate the effects of the contravention; and
 - (d) an order authorizing the City to perform any and all such work at the expense of the Owner.
34. The Owner acknowledges that, if the City undertakes work to satisfy the terms, requirements or conditions of any heritage alteration permit issued to the Owners pursuant to this Agreement upon the Owner's failure to do so, the City may add the cost of the work and any incidental expenses to the taxes payable with respect to the Land, or may recover the cost from any security that the Owner has provided to the City to guarantee the performance of the terms, requirements or conditions of the permit, or both.

35. The Owner acknowledges that the City may file a notice on title to the Land in the Land Title Office if the terms and conditions of this Agreement have been contravened.
36. The City may notify the Owner in writing of any alleged breach of this Agreement and the Owner shall have the time specified in the notice to remedy the breach. In the event that the Owner fails to remedy the breach within the time specified, the City may enforce this Agreement by:
- (a) seeking an order for specific performance of the Agreement;
 - (b) any other means specified in this Agreement; or
 - (c) any means specified in the *Community Charter* or the *Local Government Act*,
- and the City's resort to any remedy for a breach of this Agreement does not limit its right to resort to any other remedy available at law or in equity.

Statutory Authority Retained

37. Nothing in this Agreement shall limit, impair, fetter, or derogate from the statutory powers of the City, all of which powers may be exercised by the City from time to time and at any time to the fullest extent that the City is enabled.

Indemnity

38. The Owner hereby releases, indemnifies and saves the City, its officers, employees, elected officials, agents and assigns harmless from and against any and all actions, causes of action, losses, damages, costs, claims, debts and demands whatsoever by any person, arising out of or in any way due to the existence or effect of any of the restrictions or requirements in this Agreement, or the breach or non-performance by the Owner of any term or provision of this Agreement, or by reason of any work or action of the Owner in performance of its obligations under this Agreement or by reason of any wrongful act or omission, default, or negligence of the Owner.
39. In no case shall the City be liable or responsible in any way for:
- (a) any personal injury, death or consequential damage of any nature whatsoever, howsoever caused, that be suffered or sustained by the Owner or by any other person who may be on the Land; or
 - (b) any loss or damage of any nature whatsoever, howsoever caused to the Land, or any improvements or personal property thereon belonging to the Owner or to any other person,

arising directly or indirectly from compliance with the restrictions and requirements in this Agreement, wrongful or negligent failure or omission to comply with the restrictions and requirements in this Agreement or refusal, omission or failure of the City to enforce or

require compliance by the Owner with the restrictions or requirements in this Agreement or with any other term, condition, or provision of this Agreement.

No Waiver

40. No restrictions, requirements, or other provisions of this Agreement shall be deemed to have been waived by the City unless a written waiver signed by an officer of the City has first been obtained, and without limiting the generality of the foregoing, no condoning, excusing or overlooking by the City on previous occasions of any default, nor any previous written waiver, shall be taken to operate as a waiver by the City of any subsequent default or in any way defeat or affect the rights and remedies of the City.

Interpretation

41. In this Agreement, "Owner" shall mean all registered owners of the Land or subsequent registered owners of the Land, as the context requires or permits.

Headings

42. The headings in this Agreement are inserted for convenience only and shall not affect the interpretation of this Agreement or any of its provisions.

Appendices

43. All appendices to this Agreement are incorporated into and form part of this Agreement.

Number and Gender

44. Whenever the singular or masculine or neuter is used in this Agreement, the same shall be construed to mean the plural or feminine or body corporate where the context so requires.

Joint and Several

45. If at any time more than one person (as defined in the *Interpretation Act* (British Columbia)) owns the Land, each of those persons will be jointly and severally liable for all of the obligations of the Owner under this Agreement.

Successors Bound

46. All restrictions, rights and liabilities herein imposed upon or given to the respective parties shall extend to and be binding upon their respective heirs, executors, administrators, successors and assigns.

IN WITNESS WHEREOF the Owner and the City have executed this Agreement as of the date written above.

Signed, Sealed and Delivered in the
presence of:

)
)
)
)
)

Name

GARY JOHN HOLISKO

Address

ROSANNE MARIE HOOD

Occupation

ROSANNE MARIE HOOD

THE CORPORATION OF THE CITY OF NEW WESTMINSTER

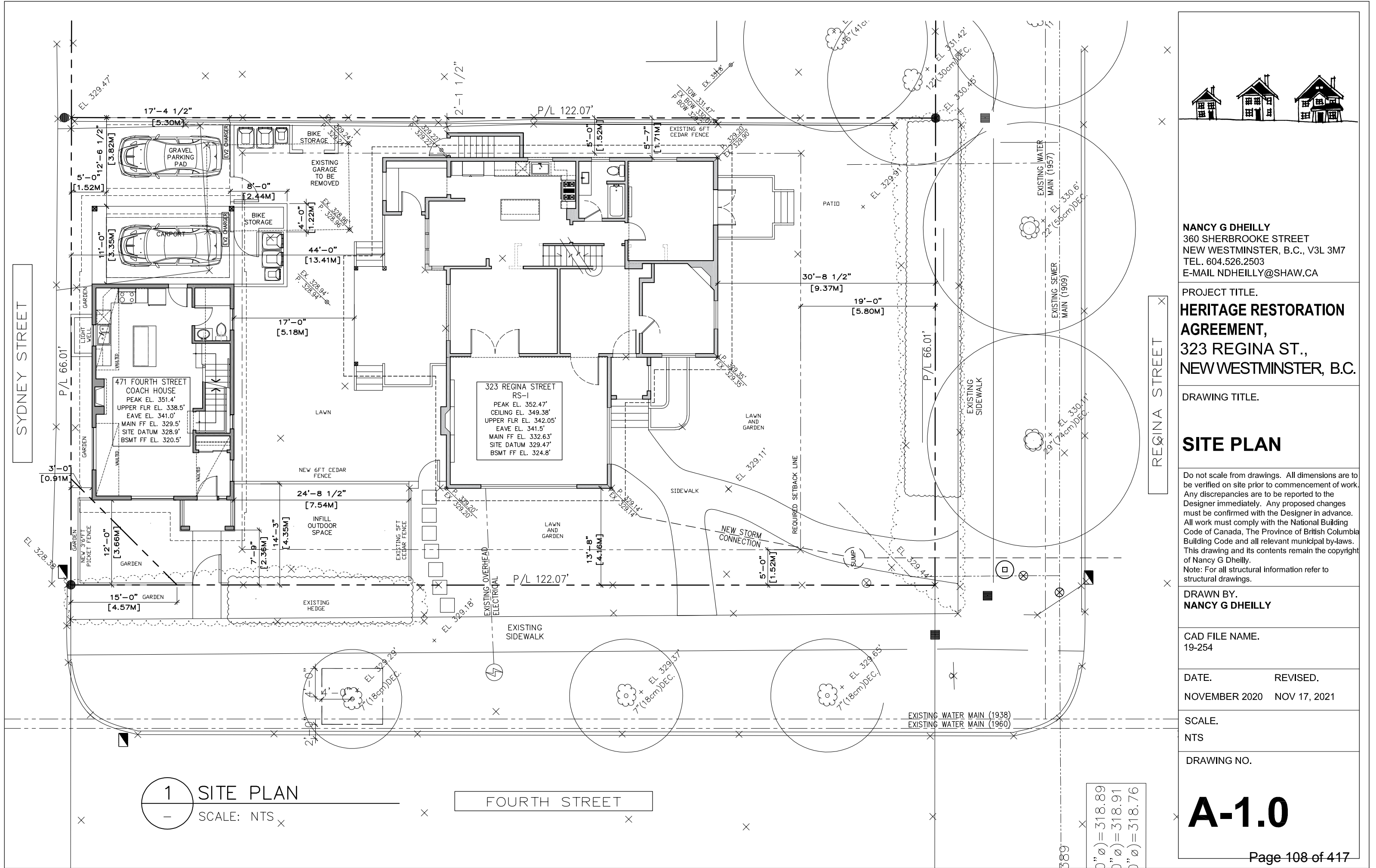
by its authorized signatories:

Mayor Jonathan X. Cote

Jacqueline Killawee, City Clerk

APPENDIX 1

SITE PLAN



1 SITE PLAN
— SCALE: NTS

FOURTH STREET



NANCY G DHEILLY
360 SHERBROOKE STREET
NEW WESTMINSTER, B.C., V3L 3M7
TEL. 604.526.2503
E-MAIL NDHEILLY@SHAW.CA

PROJECT TITLE.
HERITAGE RESTORATION AGREEMENT,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.

SITE PLAN

Do not scale from drawings. All dimensions are to be verified on site prior to commencement of work. Any discrepancies are to be reported to the Designer immediately. Any proposed changes must be confirmed with the Designer in advance. All work must comply with the National Building Code of Canada, The Province of British Columbia Building Code and all relevant municipal by-laws. This drawing and its contents remain the copyright of Nancy G Dheilly. Note: For all structural information refer to structural drawings.

DRAWN BY.
NANCY G DHEILLY

CAD FILE NAME.
19-254

DATE.	REVISED.
NOVEMBER 2020	NOV 17, 2021

SCALE.
NTS

DRAWING NO.

A-1.0

APPENDIX 2
CONSERVATION PLAN

Heritage Conservation Plan

Edgar House, 323 Regina Street, New Westminster, BC

July 24, 2021



Fig. 1: View of the front of Edgar House at 323 Regina Street, New Westminster, BC, 2020, as visible from the corner of Regina Street and Fourth Street. (Source: Holisko)

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528.0 Introduction

The subject house, Edgar House, is a Storybook style, one and a half storey, stuccoed, wood-frame construction with concrete foundation located at 323 Regina Street in New Westminster (Fig. 2). It is located in the northwest corner of the Queen's Park neighbourhood in New Westminster.



Fig. 2: Map of the area surrounding 323 Regina Street, outlined in yellow. (Source: City of New Westminster Map Viewer, CityViews, 2020)

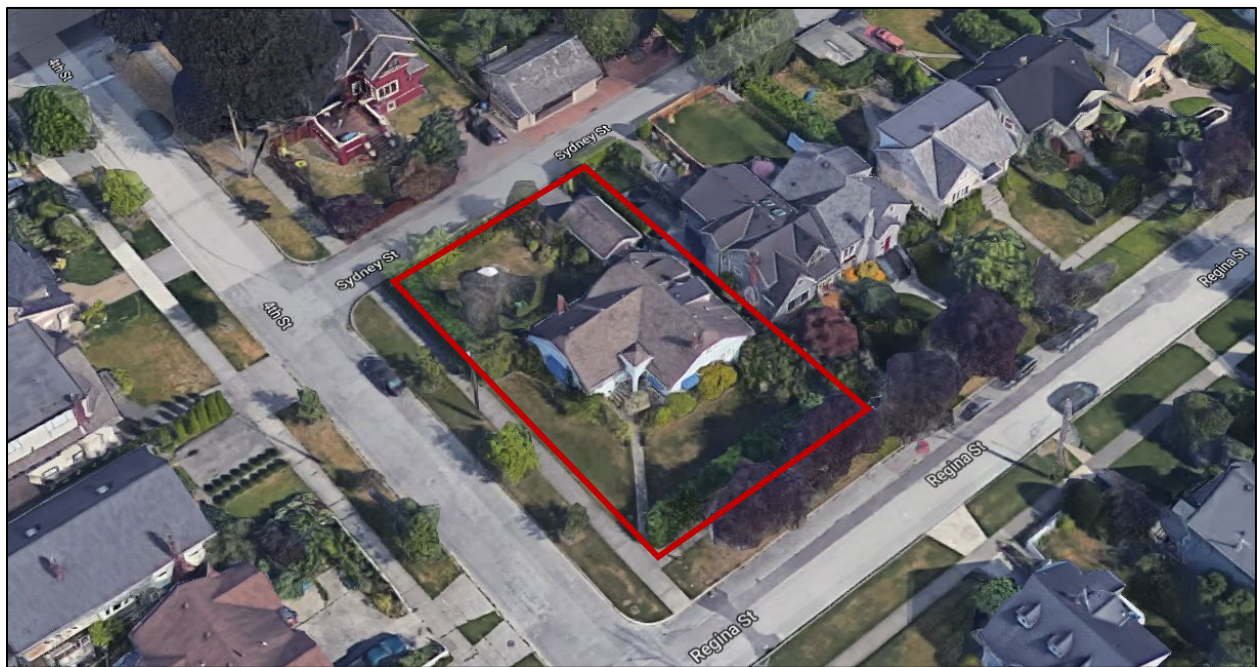
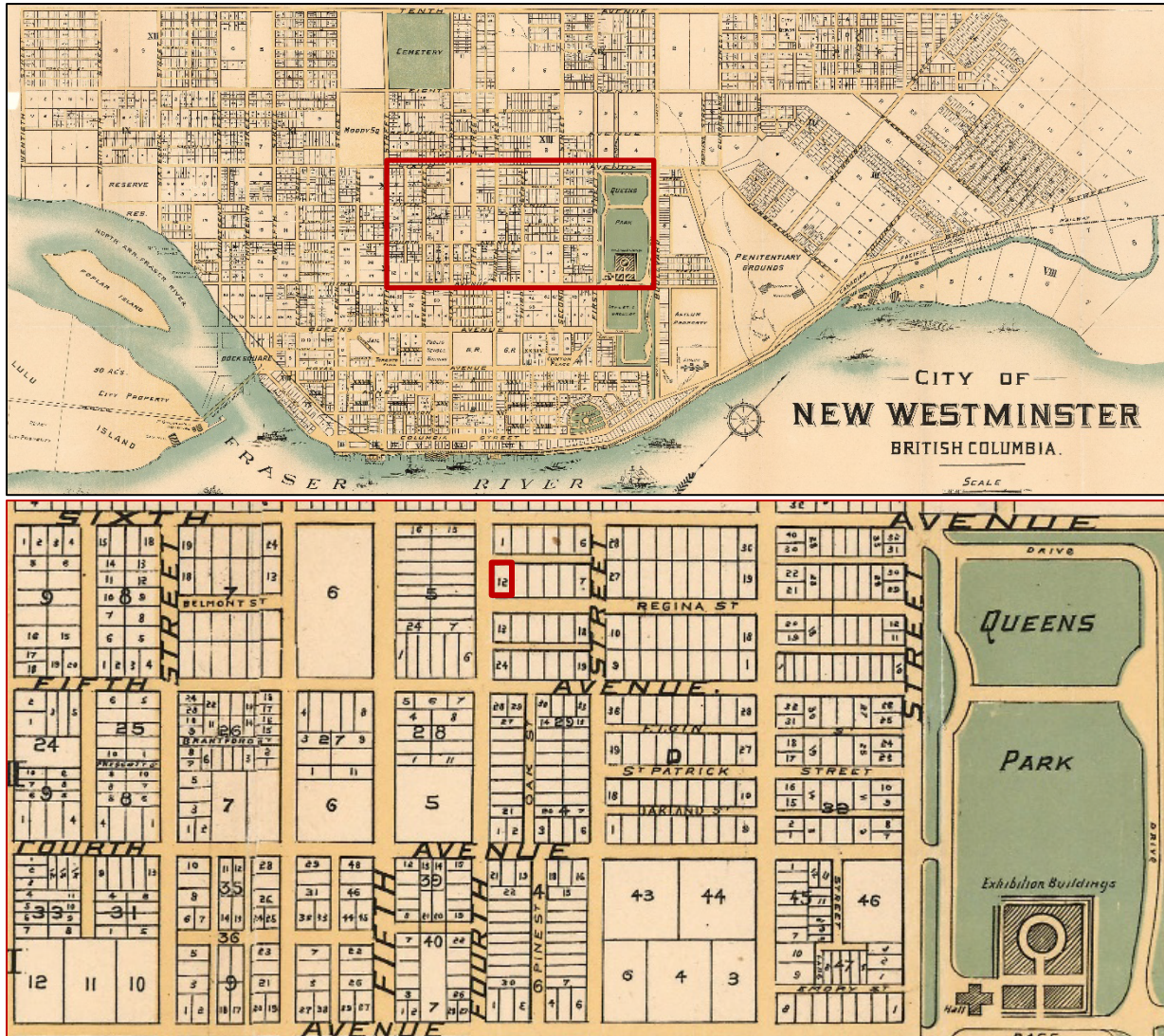


Fig. 3: Aerial view of 323 Regina Street, outlined in red. (Source: Google, 2019)

2.0 Historic Context

In 1859, the British Royal Engineers surveyed the area to become known as New Westminster, which at the time was to be the new colonial capital of the crown colony of British Columbia (Hainsworth and Freund-Hainsworth 2005, pp. 18-19). They overlaid a grid pattern on the natural topography of the area (Fig. 4a), parallel to the Fraser River (Mather and McDonald 1958, p. 22). The design, still present today, had the streets running up the hill, perpendicular to the river, and the avenues across the area, parallel to the river. The head engineer, Colonel Richard Moody, envisioned a formally planned “Garden City” with prominent public parks and elegant wide avenues (Wolf 2005, pp. 18-20). These well-landscaped parks and avenues are clearly visible in the 1928 aerial photograph of the area (Fig. 7 below).



Figs. 4a and 4b: Fig. 4a (above) shows the wider context of the City of New Westminster, 1892. Note the grid pattern of the streets and avenue. In Fig. 4a (above), the neighbourhood of 323 Regina Street is outlined in red. Its lot is outlined in bold red in Fig. 4b (below). (Source: City of Vancouver Archives, AM1594-MAP 617)

“The Royal Engineers marked out the area now known as Queen’s Park including road allowances for wide streets and landscaped boulevards, land reserves, and squares in 1859. The next year the Royal Engineers surveyed 75.5 acres for what became Queen’s Park itself. The area very soon began to attract merchants and entrepreneurs seeking a prestigious location away from the noise and pollution of the downtown and river front.” (DCD *et al.* 2009, p. 41). Shortly thereafter, New Westminster experienced two major building

booms. The first beginning in the 1880s with the extension of the Canadian Pacific Railway line and the second in the 1900s, following the destructive fire of 1898 that destroyed much of Downtown (Mather and McDonald 1958). At the beginning of the 20th century, Queen's Park "was filled up as an elite residential neighbourhood. In 1906 Queen's Park acquired paved street and concrete sidewalks, in 1912 a sewer system, and a year later street curbs, making it the first fully serviced neighbourhood in New Westminster" (DCD *et al.* 2009, p. 42).

The subject property at 323 Regina Street is located in the northwest quadrant of this "elite residential neighbourhood" known as Queen's Park. Interestingly, it was a relatively later development in the neighbourhood, being built in 1928, compared to the numerous Edwardian era constructions, distinctly visible in a 1913 Fire Insurance Map (Figs. 5a and 5b). It is worth comparing this to a 1957 Fire Insurance Map (Fig. 6), which shows a few additional developments built during the interim decades, including the captioned study site, which is visible in a 1928 aerial photograph of the area, showing the property being developed (Fig. 7). A newspaper advert from the same year, illustrates and promotes the house and its numerous qualities (Fig. 8).

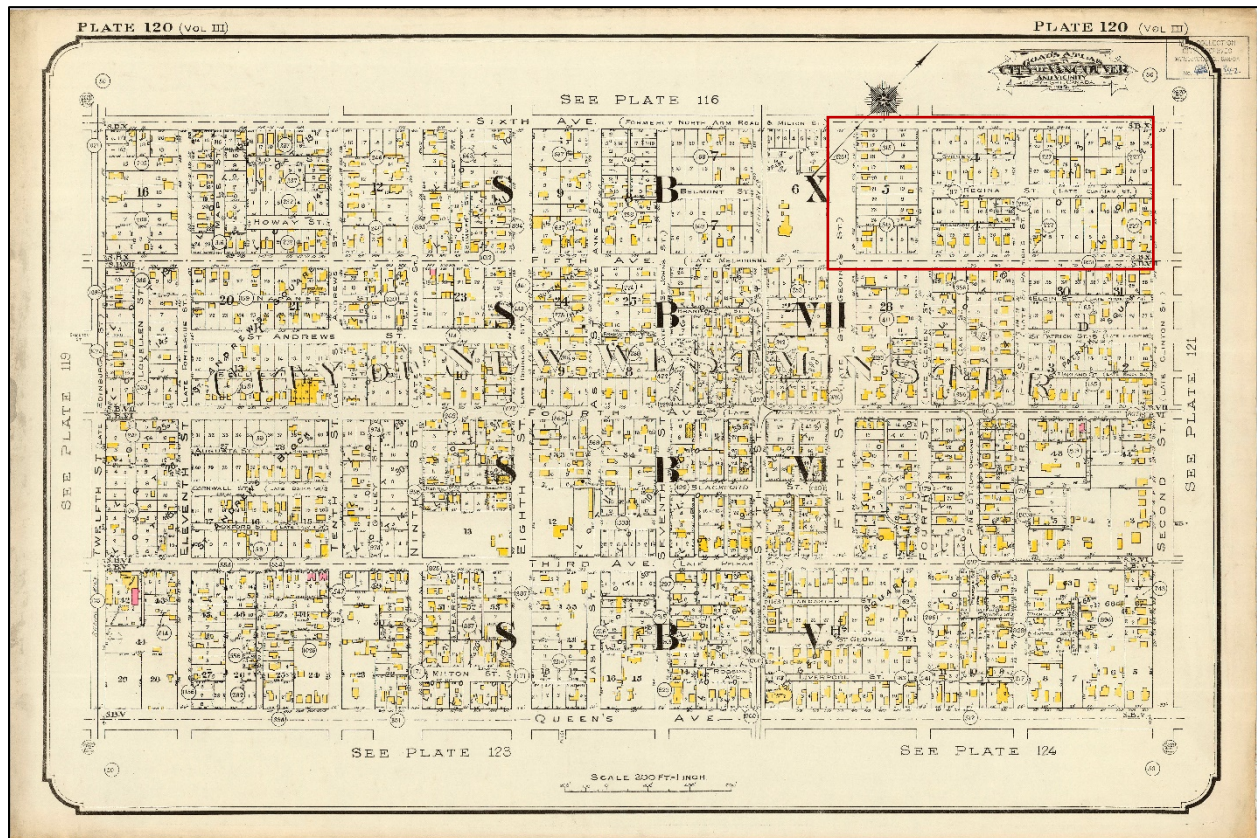


Fig. 5a: Fire Insurance Map of New Westminster, 1913. The neighbourhood of 323 Regina Street is outlined in red. The property is outlined in bolded red in Fig. 5b (below). (Source: City of Vancouver Archives, 1972-472.07, Plate 120)

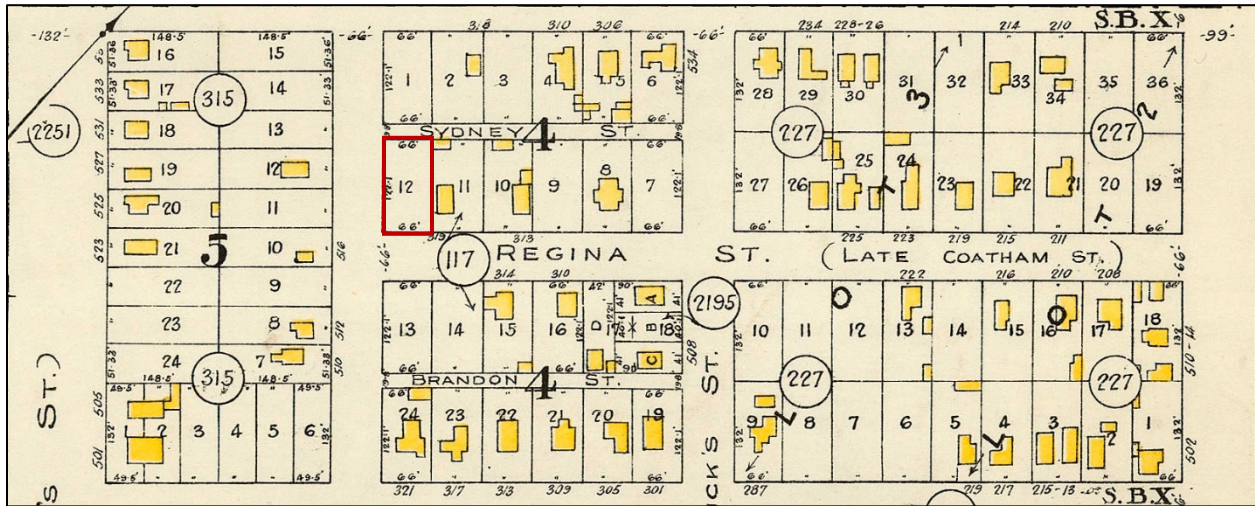


Fig. 5b: Excerpt of Fire Insurance Map of New Westminster, 1913. The empty lot of 323 Regina Street is outlined in red. (Source: City of Vancouver Archives, 1972-472.07, Plate 120)

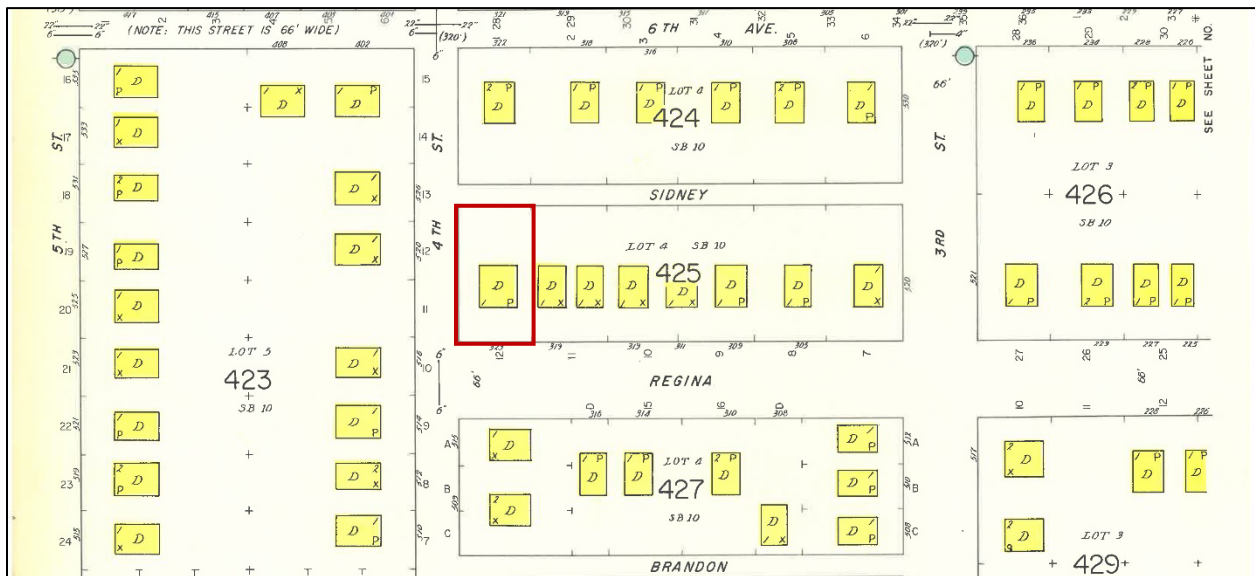



Fig. 6: Fire Insurance Map of New Westminster, 1957. The developed lot of 323 Regina Street is outlined in red. (Source: City of New Westminster Archives 1957, sheet 42)



Fig. 7: Section from a Royal Canadian Air Force aerial photograph of New Westminster, 1928. Note that 323 Regina Street has been cleared for development, however, no structure is yet built on the lot. (Source: Library & Archives Canada, AA287_058)

PRESENTS ATTRACTIVE APPEARANCE



Embodying many of the latest modern features and ideas, with both the interior and exterior very attractively designed, and exceptionally well built throughout, the spacious new residence of E. A. Edgar, local manager of the Tip Top Tailors, at the corner of Fourth and Regina streets, is a splendid addition to the large list of imposing new homes which have been built in New Westminster. The dwelling is of the semi-bungalow type and was built to plans prepared by Mr. Edgar and K. R. Matheson, the contractor.

A striking feature of the dwelling is the use of arches and graceful curves to replace the usual sharp angles, which adds greatly to its attractiveness. The curve effect is not only carried out in the interior but also on the outside walls and on the roof, which has a rounded edge.

Rooms on the ground floor include a living room, dining room, kitchen with breakfast room attached, bedroom, den, a large hall and a bathroom, and on the second floor there are two large bedrooms and a sleeping porch besides a second washroom. Hardwood floors are laid in all rooms on the main floor, except the kitchen and the bedroom in which 3-inch edge grain fir has been used.

At one end of the living room, which is 18 feet wide and 22 feet long, a beautiful large fireplace, with bookcases fitted with leaded glass doors on each side, has been built. A gas fireplace is located in the other. A feature of the dining room is an arched alcove at one end to accommodate a buffet.

With the exception of the dining room which has an elaborate drop fixture, all rooms are lighted by wall fixtures and an attractive effect has been obtained. Between the rooms on the ground floor, French doors with leaded plate glass have been placed and each door is fitted with cut glass knobs.

The kitchen is replete with built-in fixtures and a built-in sink with tile drain boards. Above the stove a vent has been placed to draw off the odors of cooking and ventilate the room. The bathroom is equipped with a built-in tub and other fixtures.

Besides the two bedrooms on the second floor, there is a moth-proof clothes closet, lined throughout with cedar. There is also a laundry chute which leads direct to the basement and has an opening in the kitchen.

Another feature worthy of mention is the artistic decorative scheme which has been carried out. The walls of the living and dining rooms and den are tinted, with a pencil effect, while the kitchen has been enamelled in gray and blue throughout, to match the sink. The three large bedrooms are finished in distinctive tones.

A beautiful effect has also been obtained in the exterior finish. The walls are of cream California stucco, with the arched windows and doors trimmed in black and white. On the roof cream and red colored material has been laid, the cream to match the walls and red the concrete walk of that color laid on the grounds.

There is a full sized cement basement in which a hot water furnace is located. Attached to the house is a fireproof garage, which will also be finished in stucco to match the main building.

Besides K. R. Matheson, other contractors engaged on the dwelling included Hugh Gifford, who installed the plumbing and furnace; Archie Cowie who built the fireplaces and the chimneys; V. Cooper and Sons, who did the plastering and stucco work and E. Hagen, the interior and exterior decorating.

Fig. 8: Newspaper article on 323 Regina Street. (Source: The British Columbian, October 8, 1928, p. 7)

From the above newspaper clipping, the elements of particular note include (transcribed here for ease of reading):

- "The spacious new residence of E. A. Edgar, local manager of the Tip Top Tailors, at the corner of Fourth and Regina streets, is a splendid addition to the large list of imposing new homes which have been built in New Westminster."
- "The dwelling is of the semi-bungalow type and was built to plans prepared by Mr. Edgar and K.R. Matheson, the contractor."
- "A striking feature of the dwelling is the use of arches and graceful curves to replace the usual sharp angles, which adds greatly to its attractiveness. The curve effect is not only carried out in the interior, but also on the outside walls and on the roof, which has a rounded edge."

- "A beautiful affect has also been obtained in the exterior finish. The walls are of cream California stucco with the arched windows and doors trimmed in black and white. On the roof cream and red colored material has been laid, the cream to match the walls and red the concrete walk of that color laid on the grounds."
- "Attached to the house is a fireproof garage, which will also be finished in stucco to match the main building."
- "Besides K.R. Matheson, other contractors engaged on the dwelling included Hugh Gifford, who installed the plumbing and furnace; Archie Cowie who built the fireplaces and the chimneys; V. Cooper and Sons who did the plastering and stucco work and E. Hagen, the interior and exterior decorating."

The design of this house has elements of the English Storybook tradition, however, it most closely resembles the French Storybook style, which are typically "small and whimsical...with hipped or side-gabled roofs and a projecting living room wing (under an L-shaped roof, in some cases), with a turret tucked into the L and forming a shelter over the front door... Windows may have arched tops, and an arched, quoined opening in the turret may frame the front door. Their cladding is coloured stucco. Roof edges may be rolled as in the English Storybook Style" (VHF). This style, along with the English Storybook style, "emerged in North America after WWI. Soldiers returning from European battlefields brought with them a familiarity with architectural styles. Among these were French farmhouses and castles. Builders translated elements of these traditional buildings into practical cottages. After a period of upheaval, the value of the picturesque and the traditional increased following the war. This contributed to the development of the French Storybook style, with its quaint tower and European flair" (*ibid.*). Its cat-slide and jerkinhead roof connects to the English Storybook style as well (VHF).

323 Regina Street was recognized in the 1980s as having heritage significance and added to the City of New Westminster's Heritage Resource Inventory, being photographed and described as follows (Fig. 9). These elements have persisted and directly influence the site's Statement of Significance, outlined in the following section.

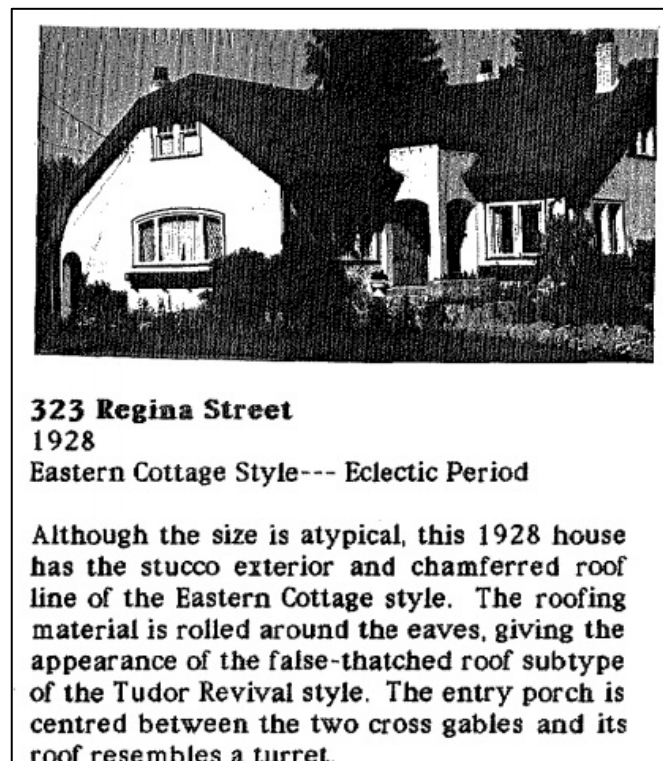


Fig. 9: Heritage inventory photograph and description of 323 Regina Street. (Source: Sleath 1989, p. 177)

3.0 Statement of Significance

The following is the Statement of Significance of Edgar House at 323 Regina Street.

3.1 Description of Historic Place

This historic place, Edgar House, is a Storybook style Cottage with a jerkinhead roof. It is a one and a half storey, stuccoed, wood-frame construction with concrete foundation. The entry porch is centred between its two cross gables and the roof over the entrance resembles a turret. The house sits on a prominent corner lot, stretching the length of 4th street from Regina Street to Sydney Street in the Queen's Park neighbourhood.

3.2 Heritage Value of Historic Place

Edgar House at 323 Regina Street has heritage value for its aesthetic and historic significance. Aesthetically, this house is an eye-catching, intact example of a Storybook style dwelling, with elements from both the French and English traditions. Its connection to the French Storybook style is seen in its various characteristic features, including: its L-shape and centred turret over its arched front entryway. Its connection to the English Storybook style is seen in elements such as its jerkinhead roof, as well as its low, sloping roof (its catslide) on its western corner. Shared elements of both Storybook styles include its rolled roofline giving it a false-thatched roof appearance, its stucco cladding, its asymmetrical design and its arched windows and doors. It was showcased in a 1928 newspaper article as a unique and attractive structure; a fact that still holds true today. Its uniqueness in the landscape contributes to this place's significance.

This house also has historic significance being among a rare stock of interwar period developments in the Queen's Park neighbourhood, being just shy of the decline that came with the Great Depression a year after its construction. It was built in 1928 with the help and input of various contractors and craftsman, named in the aforementioned article about the property. These individuals included the well-known and well-respected builder K.R. Matheson, as well as Hugh Gifford (for the plumbing and furnace), Archie Cowie (for its fireplaces and chimneys), V. Cooper and Sons (for the plastering and stucco work) and E. Hagen, (for the interior and exterior decorating). This house's namesake, Elmer Edgar, is also representative of the middle-class individuals working in New Westminster for the community, as he was the Manager of the local Tip Top Tailor's New Westminster branch. Tip Top Tailors is a Canadian company, founded in Toronto, that has been around since 1909.

3.3 Character Defining Elements

Key elements that define the heritage character of Edgar House at 323 Regina Street include:

- Its location in the Queen's Park neighbourhood.
- Its residential form, scale and massing as expressed by its one and a half storey height.
- Its jerkinhead roof and rolled shingles, imitating thatching, as well as its flared catslide on the western corner of its roof, connecting to the English Storybook style.
- Its French Storybook style elements as represented by its asymmetry and its L-shaped massing with a turret tucked in the 'L' forming a shelter over the front door.
- Its arched windows, doorways and doors.
- Its numerous wood windows featured on all sides of the house, in various sizes and configurations (some double-hung, some divided-light, some quarreled with diamond patterned panes, etc.)
- Its stuccoed exterior.

4.0 Research Findings

Neighbourhood: Queen's Park

Address & Postal Code: 323 Regina Street, V3L 1S8

Folio & PID: 08514000 & 013-593-285

Legal Description: Lot 12; Suburban Block 10 of Lot 4; New West District; Plan NWP2620

Zoning: Single Detached/RS-4

Builder & Date of completion: K.R. Matheson in 1928

Original Owner & Water Connection Connector and Year: Elmer A. Edgar & E.A. Edgar on July 14, 1928

The following tables are a consolidated summary of the residents of 323 Regina Street, as determined from the available city directories for New Westminster, as well as a list of the construction dates of the surrounding properties, illustrating the range of ages to this section of the street (visualized in Fig. 10).

Table 1: Consolidated list of the occupants of 323 Regina Street from the available city directories (Source: Vancouver Public Library, 1928 to 1955; and New Westminster Archives, 1970, 1979, 1985, 1991, 1992, 1998)

Year(s)	Name(s)	Occupation (if listed)
1928 – 1945	Elmer A. Edgar (Elverie B.)	Branch Manager, Tip Top Tailor
1946 – 1955	R. Gordon Quennell (Marion L.)	Retired
1970	Elliot E Nelles	Not listed
1979	Joyce M. Hall/Kath Hall	Not listed
1985 – 1998	R. T. Hall	Not listed

Table 2: Consolidated list of the construction dates for the properties surrounding 323 Regina Street, New Westminster, BC. (Source: BC Assessment)

Address	Year Built	Configuration
512 Third Street	1907	3 bedrooms, 2 baths
520 Third Street	1941	5 bedrooms, 3 baths
305 Regina Street	1910	3 bedrooms, 3 baths
308 Regina Street	1911	5 bedrooms, 3 baths
309 Regina Street	1936	2 bedrooms, 1 bath
310 Regina Street	1909	5 bedrooms, 2 baths
311 Regina Street	1939	2 bedrooms, 1 bath
313 Regina Street	1939	4 bedrooms, 2 baths
314 Regina Street	2000	4 bedrooms, 5 baths
316 Regina Street	1998	4 bedrooms, 3 baths
317 Regina Street	1936	4 bedrooms, 2 baths
319 Regina Street	1893	4 bedrooms, 3 baths
323 Regina Street	1928	4 bedrooms, 2 baths
514 Fourth Street	1926	4 bedrooms, 2 baths
515 Fourth Street	1940	3 bedrooms, 2 baths
516 Fourth Street	1911	4 bedrooms, 3 baths
518 Fourth Street	1973	3 bedrooms, 3 baths
520 Fourth Street	1912	5 bedrooms, 3 baths
526 Fourth Street	1913	5 bedrooms, 3 baths
528 Fourth Street	2012	3 bedrooms, 4 baths
402 Sixth Avenue	1915	4 bedrooms, 3 baths
322 Sixth Avenue	1921	6 bedrooms, 4 baths
318 Sixth Avenue	1912	4 bedrooms, 3 baths
316 Sixth Avenue	1924	3 bedrooms, 2 baths
310 Sixth Avenue	1908	4 bedrooms, 1 bath
306 Sixth Avenue	1911	2 bedrooms, 3 baths



Fig. 10: Map of the area surrounding 323 Regina Street, outlined in blue, with the construction years listed for the buildings in the immediate vicinity of the study site. Note the range of years. (Source: BC Assessment)

In summary, there are 26 houses along this section of Regina Street, 4th Street and Sixth Avenue. As a point of reference for understanding the surrounding neighbourhood and streetscape, their time periods breakdown as follows:

- 4% were built in the 1890s (1 out of 26)
- 12% were built in the 1900s (3 out of 26);
- 31% from the 1910s (8 out of 26);
- 15% from the 1920s (4 out of 26);
- 15% from the 1930s (4 out of 26);
- 8% from the 1940s (2 out of 26);
- None from the 1950s nor the 1960s;
- 4% from the 1970s (1 out of 26);
- None from the 1980s;
- 4% from the 1990s (1 out of 26); and
- 8% from the 21st century (2 out of 26).

4.1 Researcher's Note

In researching the captioned study site, Edgar House, it has been interesting and surprising to note that it is not included in the Queen's Park Heritage Conservation Area (HCA). On account of its heritage value – specifically, its aesthetic value as a somewhat rare and intact example of the whimsical Storybook style and its historical significance as an interwar pre-Great Depression development built by well-known tradesmen for a prominent Queen's Park family (in fact, already recognized in the HCA with their property at 415 Third Street (NWA 2004)) – it is unclear why this Edgar property at 323 Regina Street was omitted from the HCA. This seemed an important aspect to note amongst the site's research findings.

5.0 Archival Photographs

Unfortunately, no other historical photographs of the property were available beyond the 1928 newspaper article (Fig. 11) and the accompanying photograph of the 1989 heritage inventory description (Fig. 12). It is interesting to note the few changes to the property, such as the addition of a window box on the front window, which was apparently done shortly after the house was built in 1928 by a local ironworker. Other changes of note are the switch of the front entry staircase from being double-sided to single-sided and the addition of a chimney on the southeast corner, which has since been removed.



Fig. 11: Historical photograph of 323 Regina Street, 1928, extracted from the newspaper article on the property. (Source: The British Columbian, October 8, 1928, p. 7)



Fig. 12: Historical photograph of 323 Regina Street, 1989, taken from Volume 2 of the Heritage Resource Inventory. Note the largely similar look and condition of the property, with only minor changes, such as the addition of a window box on the front window, the change of the front entry staircase from being double-sided to being single-sided and the addition of another chimney, which has since been removed (please see the red arrows pinpointing these changed areas). (Source: Sleath 1989, p. 177)

6.0 Current Photographs



Fig. 13: Southern corner view of Edgar House at 323 Regina Street, 2020. (Source: Holisko)



Fig. 14: Eastern corner view of Edgar House at 323 Regina Street, 2020. (Source: Holisko)



Fig. 15: Northeastern side of Edgar House at 323 Regina Street, BC, 2020. (Source: Holisko)



Fig. 16: Northwestern side of Edgar House at 323 Regina Street, 2020. (Source: Holisko)

7.0 Conservation Objectives

Edgar House at 323 Regina Street will be preserved as part of a Heritage Revitalization Agreement in order to build a laneway house on their large lot and stratify their property. The proposed changes do not affect the Heritage Values nor the Character Defining Elements of this historic place.

A number of changes and some restoration work has already taken place to this historic place. For a comparison view of the work already completed, please refer to Figs. 17a and 17b below, from 2019 and 2020 respectively.



Figs. 17a and 17b: Comparative views of Edgar House at 323 Regina Street, 2019 (top) and 2020 (bottom), illustrating the various work done on site, listed in full on the following page. (Sources: Vallee (top) and Holisko)

For record purposes, work done is catalogued and summarized here, based on the information provided by the current owners:

- A similarly pitched jerkinhead roof was put over the deck on the northern side of the property, without the rolling eaves featured on the heritage building, presumably to follow Standard 11 of the Canadian *Standards and Guidelines*, to ensure its distinguishability as a new addition.
- The character-defining catslide on the western corner was repaired and restored, while being better revealed in moving the fence and installing a privacy gate.
- A small mudroom was added to the northern corner of the property, re-purposing one of the original windows that had to be removed from the kitchen.
- A deck and patio were added on the eastern corner of the property, along with a wrought iron fence, in a similar look to the window box ironwork that was added to the house shortly after it was built.
- An additional window box was also added to the south face of the property to match the one from the front.
- A set of windows from the south face of the house were re-purposed on site and replaced by wooden French doors, providing an egress point and access to the newly added south side deck and patio.
- One original window was badly water damaged and unsalvageable.
- The two small dormers along the northeastern, back side of the roof were combined into one longer one.
- Vinyl windows were installed in the two bathrooms and laundry room, along the northeastern, back side of the house with low visibility from the street. This is deemed an acceptable change on account of the minimal visual impact to the streetscape, since they are not visible from the street.
- The upper floor wood windows, facing Regina Street and Sydney Lane were replaced in-kind, with replica wood windows. The windows facing Fourth Street were not replaced and are still original.
- Areas of the stucco wall were also damaged and needed extensive patching, particularly around the front entrance and the side facing Fourth Street.
- The perimeter drain was replaced and at that time (as visible in the comparative photographs) a lot of landscaping was removed from the site, both from surrounding the house as well as from the corner portion of the hedge along Regina Street. This was to allow a clearer view of the house's front entrance, making it more accessible and visible, since the front entry largely faces Fourth Street, despite its address technically being Regina Street. The hedge was only partially removed to maintain some privacy for the new side patio on the eastern corner of the property.
- At this time, all of the drainage gutters and downspouts were replaced.
- The later-addition chimney located on the south corner of the house was removed.
- The later-addition blue awnings over the various windows were also removed.
- The house's original colour scheme (based on the 1928 newspaper article on the property) was restored.

Preservation, Restoration and Rehabilitation were and are the conservation objectives for the building. As defined by the *Standards and Guidelines for the Conservation of Historic Places in Canada* (2nd edition):

Preservation: The action or process of protecting, maintaining and/or stabilizing the existing materials, form and integrity of an historic place or of an individual component, while protecting its heritage value.

Restoration: The action or process of accurately revealing, recovering or representing the state of a historic place or of an individual component, as it appeared at a particular period in its history, while protecting its heritage value.

Rehabilitation: The action or process of making possible a continuing or compatible contemporary use of an historic place or of an individual component, through repair, alterations, and/or additions, while protecting its heritage value.

(Canada's Historic Places 2010, p. 255)

The conservation of Edgar House is focused on the preservation of the heritage house, including its various characteristic elements; restoration of its historical paint scheme; and rehabilitation of the front door and chimney. The following table summarizes the specific elements of Edgar House to be preserved, restored and rehabilitated (Table 3).

Table 3: Consolidated lists of the elements of Edgar House that are to be preserved, restored and rehabilitated. Note some have already been achieved

Preserved	Restored	Rehabilitated
Overall structure, including its form, scale and massing	Overall paint scheme	Front door
Rooflines		Chimney mortar
Stucco cladding		
All remaining original wood windows		

8.0 Building Description

Edgar House is a Storybook style Cottage, with elements from both the French and English traditions. It is a one and a half storey, stuccoed, wood-frame construction with concrete foundation. It is an L-shaped structure with a jerkinhead roof and rolled shingles, giving it a false-thatched look, as well as a flared catslide on its western corner roof. It has an elongated dormer on the northeast side of its roof (previously two dormers that have been combined). The entry porch is centred between its two cross gables and the roof over the arched entrance resembles a turret. It has numerous arched windows, doorways and doors as well as a range of wood windows on all sides of the house, in various sizes and configurations (some double-hung, some divided-light, some quarreled with diamond patterned panes, etc.). The site features a garage off of the north corner of the house in a similar look and style to the main property. The house sits on a prominent corner lot, stretching the length of 4th street from Regina Street to Sydney Street in the Queen's Park neighbourhood. It is one of the few 1920s houses remaining in the Queen's Park neighbourhood.

9.0 Condition Assessment

Overall, the exterior of Edgar House at 323 Regina Street appears to be in good to very good condition, based on the available exterior photographs. As outlined below there are just a few areas in need of minor attention.

9.1 Structure and Foundations

Overall, the condition of the walls and building envelope of Edgar House, from roof to foundation, appears to be good and having aged well. In particular, there are no major cracks visible in either the stuccoed walls or foundation. One small area of concern is the stone front steps that appear they could benefit from some minor cleaning and maintenance (Fig. 18) such as to remove moss/algae growth.



Fig. 18: Front stone steps and planter of Edgar House at 323 Regina Street, 2020, illustrating the minor maintenance concerns, such as moss growth and other plants growing between the stone slabs. (Source: Holisko)

9.2 Wood Elements

The visible, exterior wood elements, such as the doors, door frames, roof fascia and windows are, for the most part, in good condition. Any signs of deterioration are largely cosmetic, as illustrated and discussed further in the relevant sections below. Please note an internal inspection was not conducted to inspect the internal timber elements.

9.3 Roofing and Waterworks

The roof is in very good condition, overall (Figs. 19 and 20). It is difficult to determine the condition of the waterworks system from photographs, however, it is understood that these were recently replaced (with rounded aluminium ones to resemble the older more traditional style) and should therefore be in good working order. They should be checked regularly to ensure their continued efficient functioning.



Fig. 19: Front view of Edgar House at 323 Regina Street, 2020, illustrating the good condition of its roof. (Source: Holisko)



Fig. 20: Back view of Edgar House at 323 Regina Street, 2020, illustrating the good condition of its roof. (Source: Holisko)

9.4 Chimney

There is a chimney on the house, along its northwestern side (see Fig. 20 above), and it seems to be in largely good condition, with an intact chimney cap (Fig. 21a). It is worth noting that there are some signs of deterioration and loss of mortar, particularly in the areas that appear dark between the bricks (along the left side of Fig. 21b). The top of the chimney also appears that it could benefit from some cleaning and maintenance.



Figs. 21a and 21b: Fig. 21a (left) shows a detail shot of the Edgar House chimney, highlighting its largely good condition. Fig. 21b (right) shows a closer view of the chimney stack, showing some signs of deteriorating mortar and areas in need of cleaning (pinpointed by red arrows). (Sources: Holisko)

9.5 Windows and Doors

Some of the windows of the house have been replaced (or repurposed on site), although many are still original and, considering the age of the building, these intact windows and doors are in good to very good condition (as visible in Figs. 13 and 14 above and Fig. 22 below).



Fig. 22: The back deck of Edgar House at 323 Regina Street, 2020, illustrating the good condition of its original windows, with diamond patterned panes. (Source: Holisko)

Otherwise, the only other condition concern with regards to the windows and doors is with the front door, with its faded and splotchy staining (Fig. 23). It is hoped that this is simply a cosmetic concern that can be rectified by sanding and re-staining, although it should be inspected for any signs of rotting prior to any work being done on it.

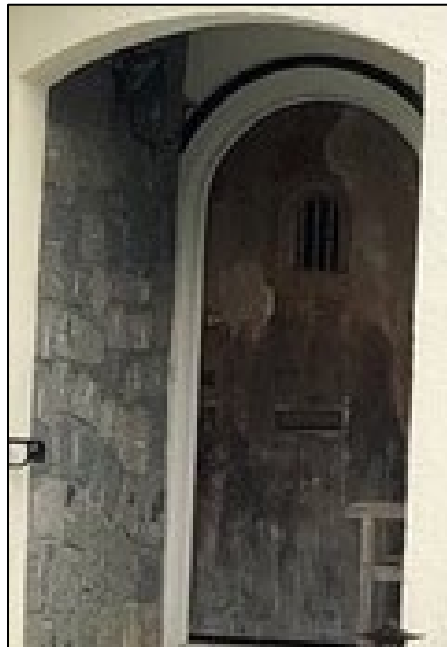


Fig. 23: Detail view of the front door of Edgar House at 323 Regina Street, 2020, illustrating its faded and splotchy staining. (Source: Holisko)

9.6 Cladding and Trimwork

As mentioned above, the stucco exterior appears to be in good condition, with no major issues identified, having been recently patched and restored. As for the trimwork, as discussed in the relevant sections above, these are also in very good shape.

9.7 Finishes

The finishes of the house are in good condition, having just recently been repainted to the historical colour scheme outlined in the 1928 newspaper article on the property and catalogued in section 10.7 below.

9.8 Landscaping

The landscaping on site is good, overall, with minimal landscaping growth near the structure and many plantings in pots, which helps to minimize the impact of roots on the building.

Despite these minor issues and concerns stated above, the overall condition of the property is good to very good. The owners should be commended for taking such good care of their property.

10.0 Recommended Conservation Procedures

10.1 Structure and Foundations – **Preservation**

- The main one and a half storey structure will be **preserved**.

10.2 Wood Elements – **Preservation**

- As addressed in greater detail in the relevant sections below, the wood elements will be **preserved**.

10.3 Roofing and Waterworks – **Preservation**

- The roofing and waterworks should be **preserved**, and regularly monitored and maintained to ensure their ongoing good condition.

10.4 Chimney – **Preservation** and **Rehabilitation**

- The chimney should be **preserved**, and **rehabilitated**, as needed. This should include regular monitoring and repointing by certified professionals, to avoid it needing to be rebuilt entirely down the road.
- Although certainly not recommended, if, overtime, it does degrade to the point of needing rebuilding, it should be dismantled to the roofline, the bricks should be cleaned and then re-used to rebuild the chimney with its original bricks, as much as possible.

10.5 Windows and Doors – **Preservation** and **Rehabilitation**

- The arched front door should be carefully **rehabilitated** (sanded down and re-stained) and **preserved**.
- All remaining original wood windows should be **preserved**.

- If there are concerns with regards to the performance of the original windows, an immediate measure to allow for better protection of them (while address heating and sound issues), is to install exterior wood storm windows on them. This would be the best conservation approach for their long-term preservation, if so desired, however, this is not a requirement.
- If this route is taken, the proposed storm windows should be traditional wood storm windows: Single pane, single light and of similar sash dimension to the window sash itself, to minimise the visual impact on the building and to allow the windows to continue to be visible on the exterior. They should be painted the same colour as the current. Dimensions should be the same as the window sash as per the proposed, historically appropriate colour scheme already used (and captured below). This is a reversible measure that would immediately benefit the building, providing greater protection to the house and improving its performance in relation to temperature control, energy efficiency and also from a noise perspective.

10.6 Cladding and Trimwork – **Preservation**

- The stucco should be **preserved**.
- The trims should be **preserved**, being monitored and maintained overtime, as needed.

10.7 Finishes – **Preservation**

- The current finish is based on the 1928 newspaper article on the house that describes its colour scheme as follows: “The walls are of cream California stucco with the arched windows and doors trimmed in black and white” (The British Columbian, October 8, 1928, p. 7). The selected colours were VC-1 Oxford Ivory for the body (from the Historical True Colours Palette; VHF 2012); Aura Low Lustre 634 for the white trim; and Regal Soft Gloss K403-80 for the black trim.
- This colour scheme should be **preserved** and maintained.
- For any eventual re-painting, follow Master’s Painters’ Institute, Repainting Manual procedures, including removing loose paint down to next sound layer, clean surface with mild TSP solution with gentlest means possible and rinse with clean water; do not use power-washing.

10.8 Landscaping

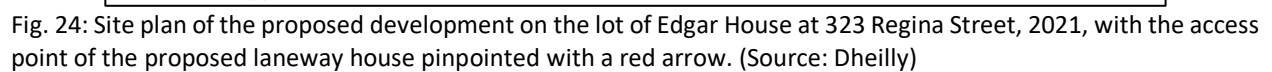
- Any additional landscaping being put in should have a minimum 2-ft clearance between the vegetation and the building face. This is preferable to ensure there is sufficient space from the structure and to remove any threat to the foundation or the building’s finishes over time.

11.0 Proposed Alterations and Future Changes

11.1 Proposed Alterations

The major proposed alterations to the property are:

- 1) Building a laneway house on the property (Figs. 24 and 25); and
- 2) Stratifying the property.



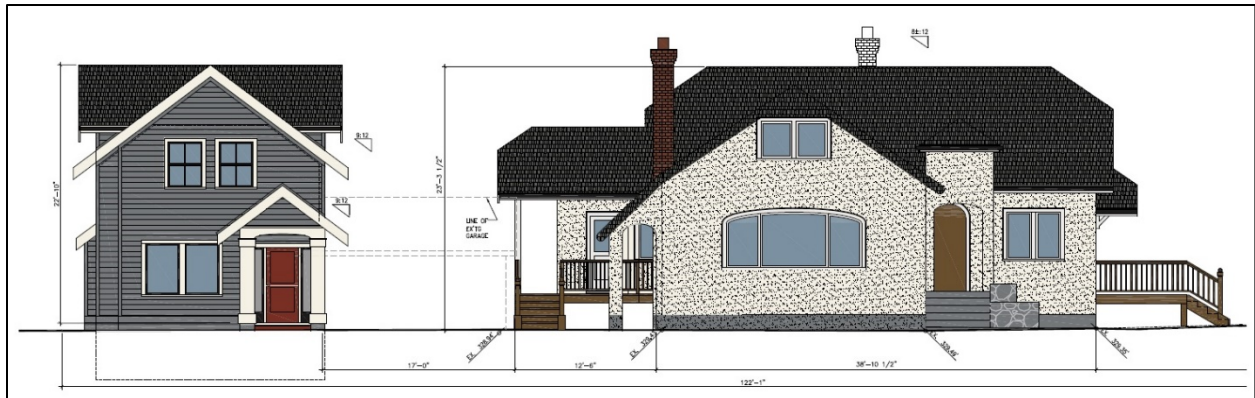


Fig. 25: Elevation from Fourth Street of Edgar House (on the right) and its proposed laneway house (on the left), 2021. (Source: Dheilly)

The proposed changes are considered a reasonable intervention given generally accepted conservation standards, rehabilitation needs and site conditions, in particular its large lot size. These proposed changes do not affect the Heritage Values and Character Defining Elements of the building.

11.2 Future Changes

Any future changes to the building's configuration, particularly any additions, should be carefully considered for minimal effect on the Heritage Values as embodied in the Character Defining Elements (CDEs) listed in the building's Statement of Significance (section 3.0 above).

12.0 Maintenance Plan

Following completion of the outlined conservation work, the owner must maintain the building and land in good repair and in accordance with generally accepted maintenance standards. All work should follow the *Standards and Guidelines for the Conservation of Historic Places in Canada (2nd Edition)*. The Local Government determines the acceptable level or condition to which the heritage building is maintained through the *Heritage Maintenance Bylaw* (CCNW 2018). As with the Heritage Conservation Plan, the maintenance standards apply only to the exterior of the building.

As general upkeep is frequently overlooked and will lead to the deterioration of heritage resources, maintenance standards warrant special attention to help to extend the physical life of a heritage asset. Any building should be kept in a reasonable condition so that it continues to function properly without incurring major expenses to repair deterioration due to neglect. The most frequent source of deterioration problems is from poorly maintained roofs, rainwater works and destructive pests.

It is important to establish a maintenance plan using the information below:

12.1 Maintenance Checklist

a. Site

- Ensure site runoff drainage is directed away from the building.
- Maintain a minimum 2-ft clearance between vegetation and building face and a 12-inch-wide gravel strip against the foundation in planted areas, if possible.
- Do not permit vegetation (such as vines) to attach to the building.

b. Foundation

- Review exterior and interior foundations, where visible, for signs of undue settlement, deformation or cracking.
- If encountered, seek advice from a professional Engineer, immediately.
- Ensure perimeter drainage piping is functional.
- Arrange a professional drainage inspection every three to five years.

c. Wood Elements

- Maintaining integrity of the exterior wood elements is critical in preventing water ingress into the building. Annual inspection of all wood elements should be conducted.
- Closely inspect highly exposed wood elements for deterioration. Anticipate replacement in kind of these elements every 10 to 15 years.
- Any signs of deterioration should be identified and corrective repair/replacement action carried out. Signs to look for include:
 - Wood in contact with ground or plantings;
 - Excessive cupping, loose knots, cracks or splits;
 - Open wood-to-wood joints or loose/missing fasteners;
 - Attack from biological growth (such as moss or moulds) or infestations (such as carpenter ants);
 - Animal damage or accumulations (such as chewed holes, nesting, or bird/rodent droppings). These should be approached using Hazardous Materials procedures; and
 - Signs of water ingress (such as rot, staining or mould).
- Paint finishes should be inspected every three to five years and expect a full repainting every seven to ten years. Signs to look for include:
 - Bubbling, cracks, crazing, wrinkles, flaking, peeling or powdering; and
 - Excessive fading of colours, especially dark tones.
- Note all repainting should be as per the recommended historic colours in section 10.7 above.

d. Windows and Doors

- Replace cracked or broken glass as it occurs.
- Check satisfactory operation of windows and doors. Poor operation can be a sign of building settlement distorting the frame or sashes or doors may be warped.
- Check condition and operation of hardware for rust or breakage. Lubricate annually.
- Inspect weather stripping for excessive wear and integrity.

e. Roofing and Rainwater Works

- Inspect roof condition every five years, in particular looking for:
 - Loose, split or missing shingles, especially at edges, ridges and hips;
 - Excessive moss growth and/or accumulation of debris from adjacent trees; and
 - Flashings functioning properly to shed water down slope, especially at the chimneys.
- Remove roof debris and moss with gentle sweeping and low-pressure hose.
- Plan for roof replacement at around 18 to 22 years.
- Annually inspect and clean gutters and flush out downspouts. Ensure gutters positively slope to downspouts to ensure there are no leaks or water splashing onto the building.

- Ensure gutter hangers and rainwater system elements are intact and secure.
- Ensure downspouts are inserted into collection piping stub-outs at grade and/or directed away from the building onto concrete splash pads.

f. General Cleaning

- The building exterior should be regularly cleaned depending on build up of atmospheric soot, biological growth and/or dirt up-splash from the ground.
- Cleaning prevents build up of deleterious materials, which can lead to premature and avoidable maintenance problems.
- Windows, doors and rainwater works should be cleaned annually.
- When cleaning always use the gentlest means possible, such as soft bristle brush and low-pressure hose. Use mild cleaner if necessary, such as diluted TSP or Simple Green ©.
- Do not use high-pressure washing as it will lead to excessive damage to finishes, seals, caulking and wood elements and it will drive water in wall assemblies and lead to larger problems.

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APPENDIX 3

CONFIRMATION OF COMMITMENT BY REGISTERED PROFESSIONAL

Date: _____

City of New Westminster

511 Royal Avenue

New Westminster, BC

V3L 1H9

Attention: Director of Climate Action, Planning and Development

Re: Heritage Revitalization Agreement for 323 Regina Street

The undersigned hereby undertakes to be responsible for field reviews of the construction carried out at the captioned address for compliance with the requirements of Appendix 2 (Conservation Plan) of the Heritage Revitalization Agreement applicable to the property, which the undersigned acknowledges having received and reviewed, and undertakes to notify the City of New Westminster in writing as soon as possible if the undersigned's contract for field review is terminated at any time during construction. This letter is not being provided in connection with Part 2 of the British Columbia Building Code, but in connection only with the requirements of the Heritage Revitalization Agreement.

Registered Professional's Name

Address

Telephone No.

Signature or Seal

APPENDIX 4

CERTIFICATION OF REGISTERED PROFESSIONAL

Date: _____

City of New Westminster

511 Royal Avenue

New Westminster, BC

V3L 1H9

Attention: Director of Climate Action, Planning and Development

Re: Heritage Revitalization Agreement for 323 Regina Street

I hereby give assurance that I have fulfilled my obligations for field review as indicated in my letter to the City of New Westminster dated _____ in relation to the captioned property, and that the architectural components of the work comply in all material respects with the requirements of Appendix 2 (Conservation Plan) of the Heritage Revitalization Agreement referred to in that letter. This letter is not being provided in connection with Part 2 of the British Columbia Building Code, but in connection only with the requirements of the Heritage Revitalization Agreement.

Registered Professional's Name

Address

Telephone No.

Signature or Seal

APPENDIX 5
APPROVED PLANS

PROJECT DATA – 323 REGINA STREET

EXT’G LEGAL DESCRIPTION:	LOT 12 OF LOT 4, SB 10, NWD, PLAN 2620	
CIVIC ADDRESS:	323 REGINA STREET, NEW WESTMINSTER, B.C.	
CURRENT ZONING/ USE:	RS–4 RESIDENTIAL DISTRICT	
	PERMITTED	EXISTING
SITE AREA:	6000 SF MIN.	66.0x122.07=8,056.62 SF
MEAN BLDG. HEIGHT (DATUM: EL: 329.47’)	25.0 FT	17.5 FT
MAXIMUM HEIGHT:	35.0 FT	23.0 FT
SITE COVERAGE:	35% (2,819.8 SF)	20.4% (1,643.4 SF)
SETBACKS OF BUILDING:		
FRONT:	19.0 FT	30.71 FT
REAR:	24.41 FT	44.0 FT
SIDE YARDS:	5.0 FT	W 13.7 FT/E 2.1 FT
FRONT PORCH ENCROACHMENT:	4.0 FT	0.0 FT
BUILDING AREAS:		
UPPER FLOOR AREA:	– SF	1,114.9 SF
MAIN FLOOR AREA:	– SF	1,643.4 SF
BSMT FLOOR AREA:	– SF	684.3 SF
TOTAL FLOOR AREA:	4028.31 SF	3,442.6 SF
FLOOR SPACE RATIO:	0.7	0.43
ATTACHED ACCESSORY:		
SIDE ENTRANCE PORCH:	– SF	33.0 SF
BACK PORCH/DECK:	– SF	219.4 SF
FRONT DECK:	– SF	72 SF
TOTAL FLOOR AREA:	805.6 SF (10%)	324.43 SF (4%)

PROJECT DATA – 571 FOURTH ST INFILL HOUSE

EXT’G LEGAL DESCRIPTION:	LOT 12 OF LOT 4, SB 10, NWD, PLAN 2620	
CIVIC ADDRESS:	571 FOURTH STREET, NEW WESTMINSTER, B.C.	
CURRENT ZONING/ USE:	RS–4 RESIDENTIAL DISTRICT	
ENERGY USAGE:	STEP CODE 3	
	PERMITTED/REQUIRED	PROPOSED
SITE AREA:	6000 SF MIN.	66.0x122.07=8,056.62 SF
MAXIMUM HEIGHT:	23.0 FT	22.5 FT
SITE COVERAGE:	10% (805.6 SF)	8% (642 SF)
SETBACKS OF BUILDING:		
LANE:	3.0 FT	3.0 FT
BETWEEN HOUSES:	16.0 FT	17.0 FT
SIDE YARDS:	6.0 FT	W 11.9 FT/E 23.5 FT
BUILDING AREAS:		
UPPER FLOOR AREA:	– SF	440.0 SF
MAIN FLOOR AREA:	– SF	610.0 SF
BSMT FLOOR AREA:	– SF	370 SF
TOTAL FLOOR AREA:	958 SF	1,420.0 SF
FLOOR SPACE RATIO:	0.12	0.18 *
ATTACHED ACCESSORY:		
FRONT PORCH:	32.0 SF	32.0 SF
CARPORT:	226 SF	222.8 SF
BIKE STORAGE:	32.0 SF	32.0 SF

OVERALL SITE PROJECT DATA

SITE AREA:	66.0 x 122.07 = 8056.62 SF
PROPOSED SITE COVERAGE:	2285.4 SF (28.4%)
PROPOSED TOTAL FSR:	0.60 (4862.6 SF)
PROPOSED EXCLUDING BSMT FSR:	0.47 (3808.3 SF)
PROPOSED PARKING:	2 SPACES PROVIDED



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PROJECT TITLE.
**HERITAGE RESTORATION
AGREEMENT,
323 REGINA ST.,
NEW WESTMINSTER, B.C.**

DRAWING TITLE.
SITE DATA

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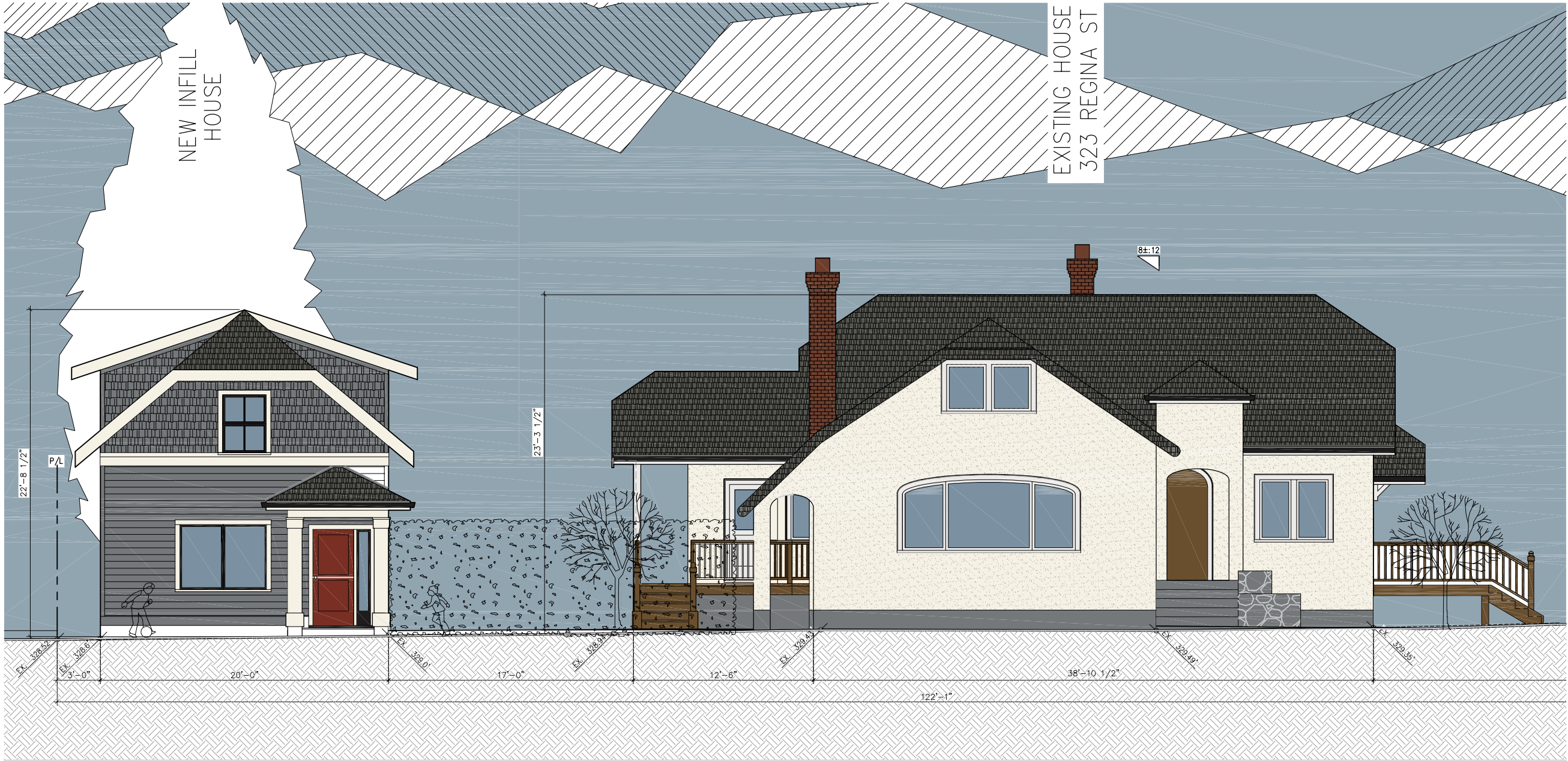
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DATE.	REVISED.
NOVEMBER 2020	AUG 6, 2021

SCALE.
NTS

DRAWING NO.

A-0.1



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PROJECT TITLE.

**INFILL HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.**

DRAWING TITLE.

**4TH STREET
ELEVATION**

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19-254

DATE. REVISED.
NOVEMBER 2020 NOV 8, 2021

SCALE.
1/8"=1'0"

DRAWING NO.

A-0.2
Page 142 of 417

1 4TH STREET ELEVATION
— SCALE: 1/8" = 1'-0"



1 4TH ST (WEST) ELEVATION
— SCALE: 3/16" = 1'-0"



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PROJECT TITLE.

**HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.**

DRAWING TITLE.

**4TH ST (WEST)
ELEVATION**

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CAD FILE NAME.
19-254

DATE. REVISED.
NOVEMBER 2020 AUG 6, 2021

SCALE.
3/16"=1'0"

DRAWING NO.

A-1.5



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PROJECT TITLE.

**HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.**

DRAWING TITLE.

**REGINA (SOUTH)
ELEVATION**

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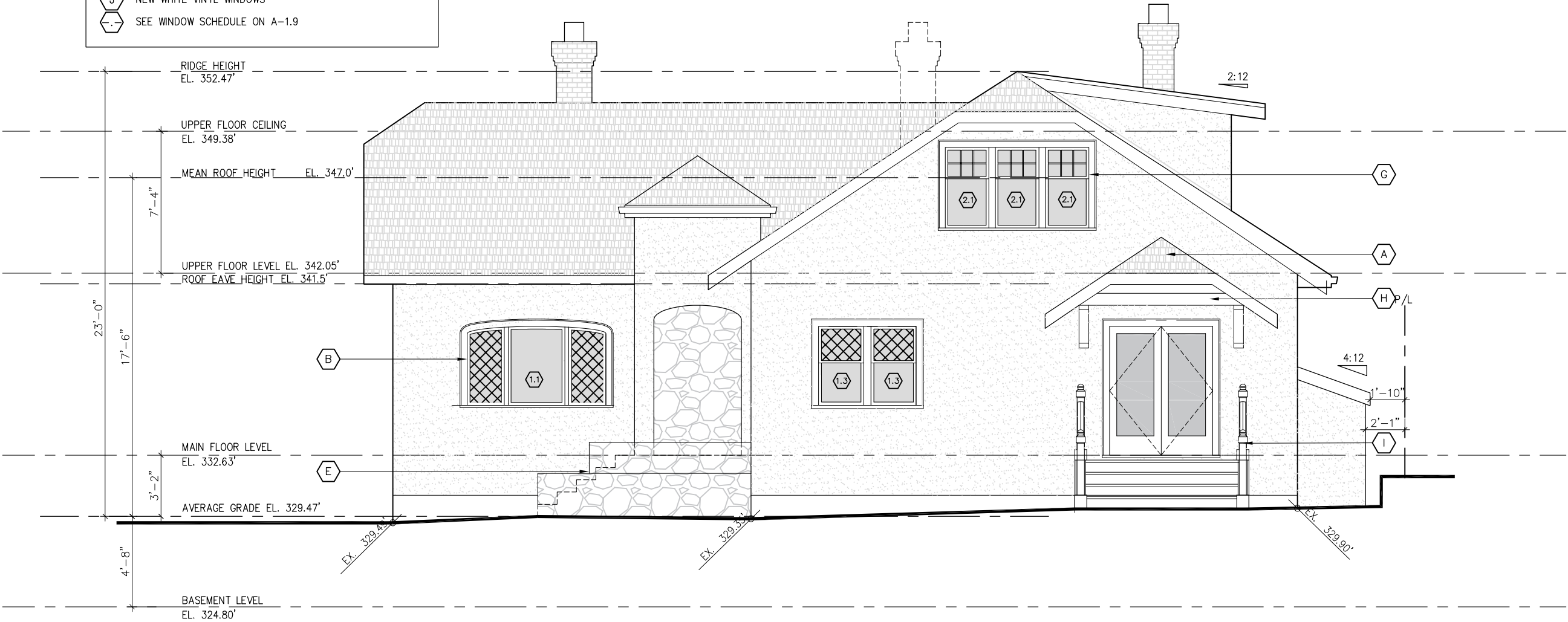
SCALE.
3/16"=1'0"

DRAWING NO.

A-1.6

MATERIALS KEY

- A ASPHALT ROOF SHINGLES
IKO HARVARD SLATE
- B RESTORED WOOD FRAME WINDOWS GLOSS BLACK
- C RESTORED ROOF AND WINDOW TRIMS & PORCH DETAILS
AURA LOW LUSTRE 634-1 WHITE
- D RESTORED STUCCO VC-1 OXFORD IVORY
- E EXISTING GRANITE
- F GUTTER AND DOWNSPOUTS BLACK
- G NEW WOOD FRAME WINDOWS GLOSS BLACK
- H NEW ROOF AND WINDOW TRIMS & PORCH DETAILS
AURA LOW LUSTRE 634-1 WHITE
- I NATURAL CEDAR
- J NEW WHITE VINYL WINDOWS
- SEE WINDOW SCHEDULE ON A-1.9



1 REGINA (SOUTH) ELEVATION
— SCALE: 3/16" = 1'-0"



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PROJECT TITLE.
HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.
**NORTH
ELEVATION**

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NOVEMBER 2020	AUG 6, 2021

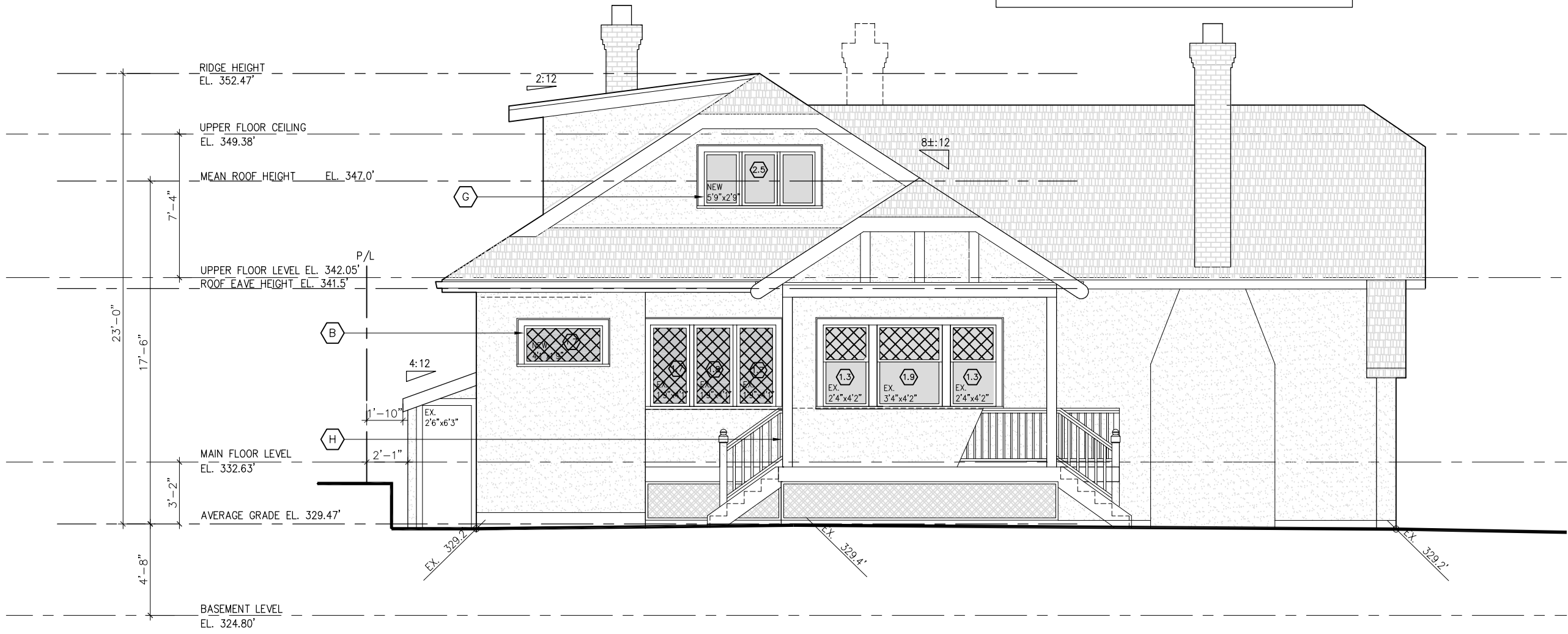
SCALE.
3/16"=1'0"

DRAWING NO.

A-1.7

MATERIALS KEY

- A ASPHALT ROOF SHINGLES
IKO HARVARD SLATE
- B RESTORED WOOD FRAME WINDOWS GLOSS BLACK
- C RESTORED ROOF AND WINDOW TRIMS & PORCH DETAILS
AURA LOW LUSTRE 634-1 WHITE
- D RESTORED STUCCO VC-1 OXFORD IVORY
- E EXISTING GRANITE
- F GUTTER AND DOWNSPOUTS BLACK
- G NEW WOOD FRAME WINDOWS GLOSS BLACK
- H NEW ROOF AND WINDOW TRIMS & PORCH DETAILS
AURA LOW LUSTRE 634-1 WHITE
- I NATURAL CEDAR
- J NEW WHITE VINYL WINDOWS
- SEE WINDOW SCHEDULE ON A-1.9



1 NORTH ELEVATION
— SCALE: 3/16" = 1'-0"



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PROJECT TITLE.
HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.
**EAST
ELEVATION**

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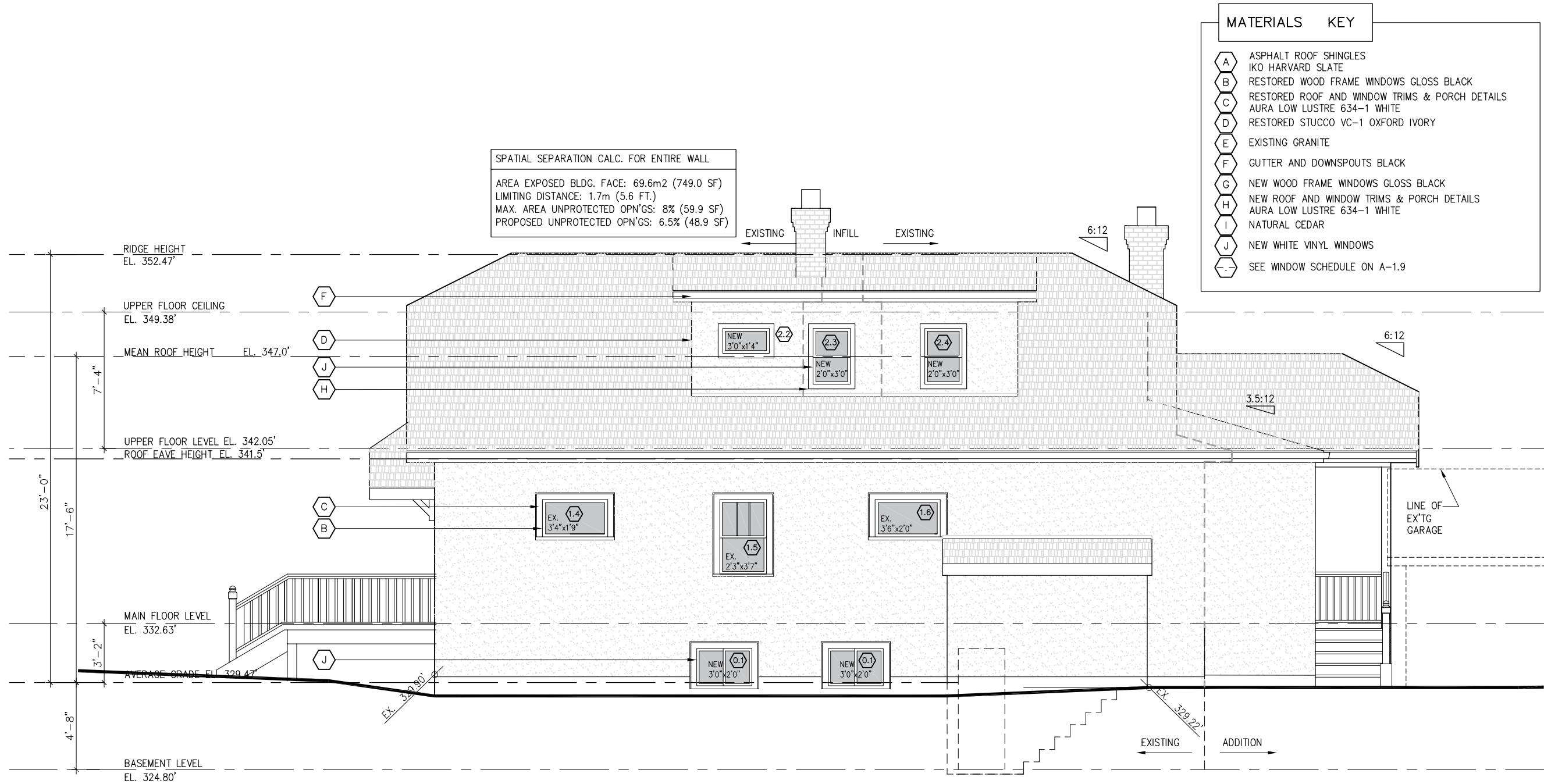
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19-254

DATE. REVISED.
NOVEMBER 2020 AUG 6, 2021

SCALE.
3/16"=1'0"

DRAWING NO.

A-1.8



1 EAST ELEVATION
— SCALE: 3/16" = 1'-0"



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PROJECT TITLE.

HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.

WINDOW
SCHEDULE

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SCALE.
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DRAWING NO.

A-1.9

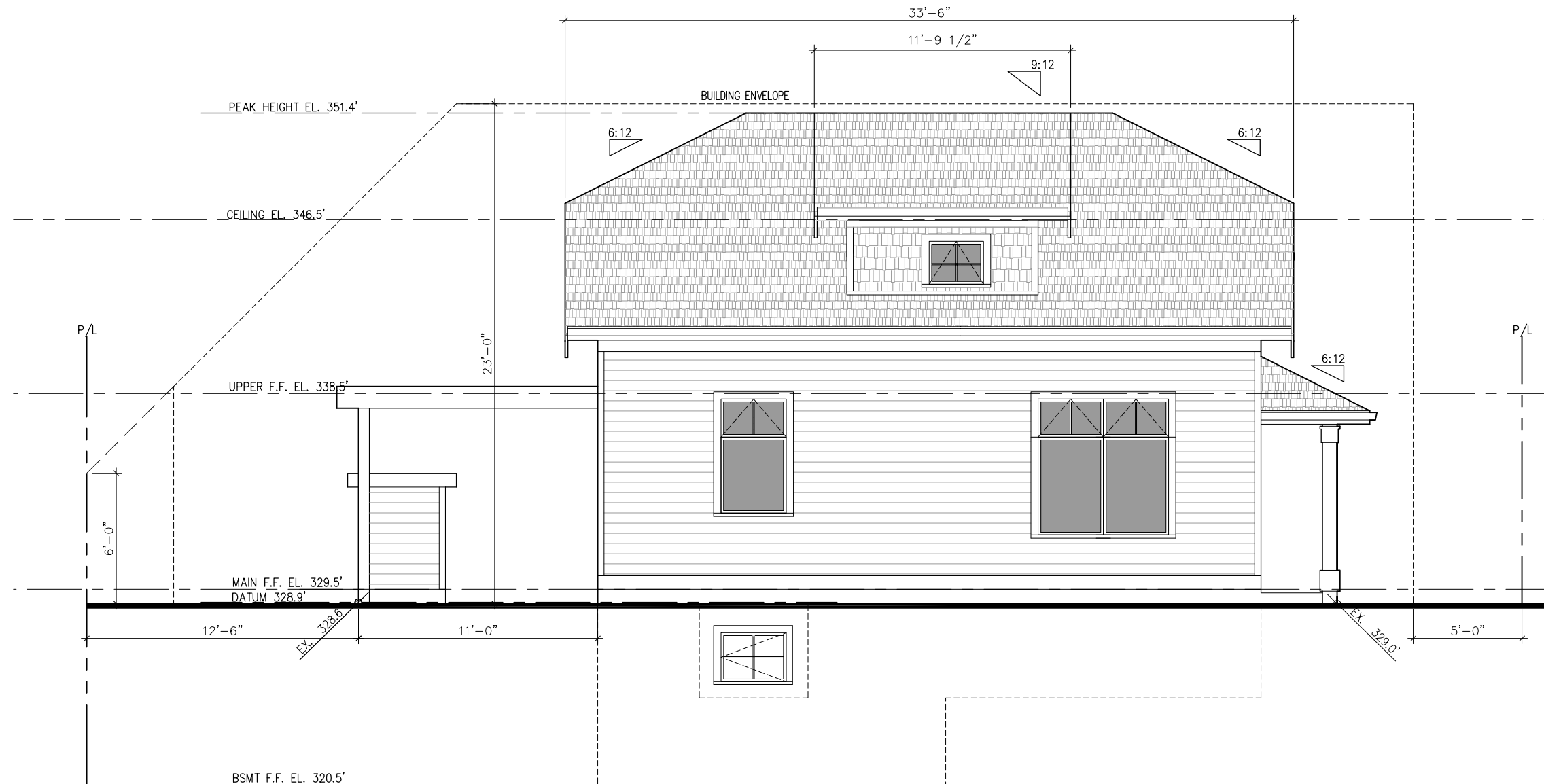
WINDOW SCHEDULE				
MARK	LOCATION	SIZE	DESCRIPTION	REMARKS
0.1	BASEMENT	3'-0"X2'-0"	SLIDING	NEW, VINYL
1.0	LIVING ROOM	10'-0"X4'-2"	FIXED	EX, WOODEN ARCHED WINDOW, W/LEADED GLASS SIDE PANELS AND CENTRAL PICTURE PANEL
1.1	LIVING ROOM	7'-3"X4'-2"	CASEMENT, FIXED, FIXED	EX, WOODEN ARCHED WINDOW, W/LEADED GLASS SIDE PANELS AND CENTRAL PICTURE PANEL
1.2	DEN	2'-4"X4'-2"	FIXED	EXISTING, WOODEN WITH LEADED GLASS
1.3	DEN, DINING ROOM	2'-4"X4'-2"	DOUBLE HUNG	EXISTING, WOODEN-LEADED GLASS UPPER PANEL (REUSE BDRM WINDOW FOR DEN)
1.4	BEDROOM	3'-4"X1'-9"	AWNING	EXISTING WOODEN WINDOW
1.5	BATHROOM	2'-3"X3'-7"	DOUBLE HUNG	EXISTING WOODEN, WITH OBSCURE GLASS IN LOWER PANEL, UPPER TRUE DIVIDED LITE.
1.6	KITCHEN	3'-6"X2'-0"	FIXED-STAINED GLASS	EXISTING WOODEN WINDOW WITH STAINED GLASS
1.7	MUDROOM, NOOK	1'-9"X4'-0"	FIXED-LEADED GLASS	EXISTING WOODEN WINDOW (REUSE NOOK WINDOW FOR MUDROOM)
1.8	NOOK	1'-9"X4'-0"	CASEMENT-LEADED GLASS	EXISTING WOODEN WINDOW
1.9	DINING ROOM	3'-4"X4'-2"	DOUBLE HUNG	EXISTING, WOODEN-LEADED GLASS UPPER PANEL
2.0	BEDROOM	2'-4"X3'-2"	DOUBLE HUNG	EXISTING, WOODEN-UPPER PANEL W/TRUE DIVIDED LITES
2.1	BEDROOM	2'-0"X4'-2"	DOUBLE HUNG	NEW, WOODEN-UPPER PANEL W/ TRUE DIVIDED LITES
2.2	BATH	3'-0"X1'-4"	AWNING	NEW, VINYL
2.3	BATH	2'-0"X3'-0"	SINGLE HUNG	NEW, VINYL, LOWER PANEL OBSCURE GLASS
2.4	LAUNDRY	2'-0"X3'-0"	SINGLE HUNG	NEW, VINYL
2.5	BEDROOM	5'-9"X2'-9"	CASEMENT, FIXED, CASEMENT	NEW, WOODEN

1

WINDOW SCHEDULE

—

SCALE: NTS



1 NORTH ELEVATION
— SCALE: 3/16" = 1'-0"



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PROJECT TITLE.

INFILL HOUSE,
571 FOURTH ST,
NEW WESTMINSTER, B.C.

DRAWING TITLE.

NORTH
ELEVATION

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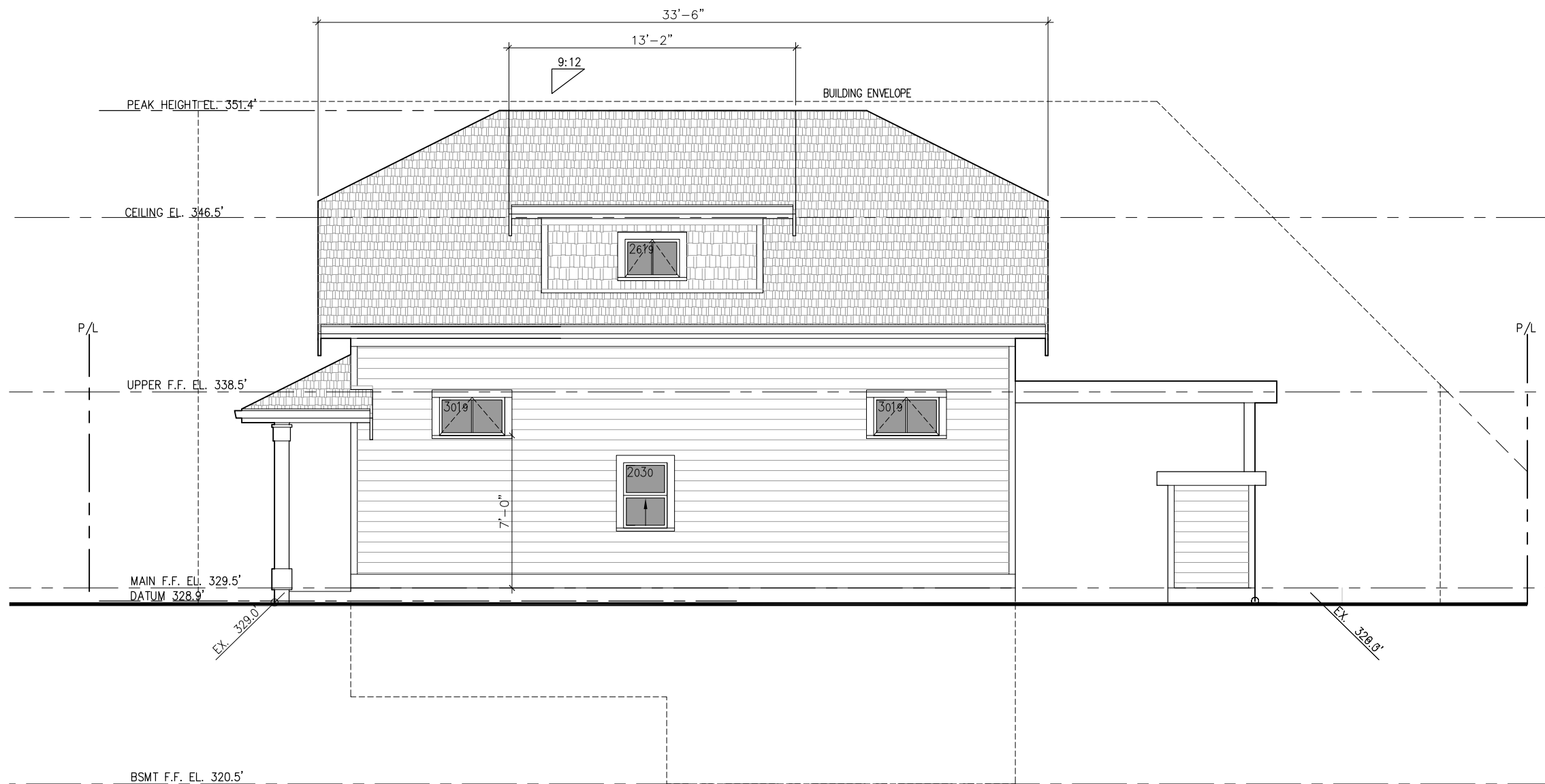
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19-254

DATE. REVISED.
NOVEMBER 2020 NOV 8, 2021

SCALE.
3/16"=1'0"

DRAWING NO.

A-2.6



1 SOUTH ELEVATION
— SCALE: 3/16" = 1'-0"



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PROJECT TITLE.

INFILL HOUSE,
571 FOURTH ST,
NEW WESTMINSTER, B.C.

DRAWING TITLE.

SOUTH
ELEVATION

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NOVEMBER 2020 NOV 8, 2021

SCALE.
3/16"=1'0"

DRAWING NO.

A-2.7

APPENDIX 6

VARIATIONS TO ZONING BYLAW NO. 6680, 2001

	Single Detached Dwelling District (RS-4) Requirement/Allowance	Heritage Building (323 Regina Street)	Infill Building (471 Fourth Street)
Maximum Detached Accessory Dwelling Floor Space Ratio*	0.1	--	0.18
Minimum Left Side Setback (north)	1.5 metres (5 feet)	--	0.9 metres (3 feet)
Minimum Right Side Setback (east)	1.5 metres (5 feet)	0.6 metres (2.1 feet)	--

** Should Step Code 3, 4 or 5 of the Energy Step Code be met, the maximum space ratio can be increased as outlined in Section 310.11.1 of Zoning Bylaw No. 6680, 2001*

Attachment 2

*Heritage Designation (323 Regina
Street) Bylaw No. 8305, 2022*

THE CORPORATION OF THE CITY OF NEW WESTMINSTER

BYLAW NO. 8305, 2022

A bylaw of the Corporation of the City of New Westminster to designate the principal building located at 323 Regina Street as protected heritage property.

WHEREAS the *Local Government Act*, RSBC 2015, c.1 provides Council with authority, by bylaw, to designate real property, in whole or in part, as protected heritage property, on terms and conditions it considers appropriate;

AND WHEREAS the registered owner of the land located at 323 Regina Street has entered into a Heritage Revitalization Agreement authorized by Bylaw No. 8304, 2022 (the "Heritage Revitalization Agreement"), which has requested that Council designate the principal building on the land as protected heritage property, and has released the City from any obligation to compensate the registered owner for the effect of such designation;

AND WHEREAS Council considers that the principal building located at 323 Regina Street has significant heritage value and character and is a prominent and valued heritage property in the City;

AND WHEREAS Council considers that designation of the principal building located at 323 Regina Street as protected heritage property under the provisions of the *Local Government Act* is necessary and desirable for its conservation;

NOW THEREFORE City Council of the Corporation of the City of New Westminster enacts as follows:

TITLE

1. This Bylaw may be cited for all purposes as "Heritage Designation Bylaw (323 Regina Street) No. 8305, 2022."

INTERPRETATION

2. In this Bylaw, the terms "heritage value", "heritage character" and "alter" have the corresponding meanings given to them in the *Local Government Act*.

DESIGNATION

3. The principal building located on that parcel of land having a civic address of 323 Regina Street, New Westminster, British Columbia, legally described as PID: 013-593-285; LOT 12 OF LOT 4 SUBURBAN BLOCK 10 PLAN 2620 and labelled "Heritage House" in Schedule A (the "Building"), is hereby designated in its entirety as protected heritage property under section 611 of the *Local Government Act* of British Columbia.

PROHIBITION

4. Except as expressly permitted by Section 5 or as authorized by a heritage alteration permit issued by the City, no person shall undertake any of the following actions, nor cause or permit any of the following actions to be undertaken in relation to the Building:
 - (a) alter the exterior of the Building;
 - (b) make a structural change to the Building including, without limitation, demolition of the Building or any structural change resulting in demolition of the Building;
 - (c) move the Building; or
 - (d) alter, excavate or build on that portion of land upon which the Building is located.

EXEMPTIONS

5. Despite Section 4, the following actions may be undertaken in relation to the Building without first obtaining a heritage alteration permit from the City:
 - (a) non-structural renovations or alterations to the interior of the Building that do not alter the exterior appearance of the Building; and
 - (b) normal repairs and maintenance that do not alter the exterior appearance of the Building.
6. For the purpose of section 5, “normal repairs” means the repair or replacement of non-structural elements, components or finishing materials of the Building with elements, components or finishing materials that are equivalent to those being replaced in terms of heritage character, material composition, colour, dimensions and quality.

MAINTENANCE

7. The Building shall be maintained in good repair in accordance with the City of New Westminster Heritage Property Maintenance Standards Bylaw No. 7971, 2018, as amended or replaced from time to time.

HERITAGE ALTERATION PERMITS

8. Where a heritage alteration permit is required under this Bylaw for a proposed action in relation to the Building, application shall be made to the City of New Westminster Development Services Department, Planning Division in the manner and on the form prescribed, and the applicant shall pay the fee imposed by the City for such permit, if any.

9. City Council, or its authorized delegate, is hereby authorized to:

- (a) issue a heritage alteration permit for situations in which the proposed action would be consistent with the heritage protection provided for the Building under this Bylaw and the Heritage Revitalization Agreement;
- (b) withhold the issue of a heritage alteration permit for an action which would not be consistent with the heritage protection provided for the Building under this Bylaw or the Heritage Revitalization Agreement;
- (c) establish and impose terms, requirements and conditions on the issue of a heritage alteration permit that are considered to be consistent with the purpose of the heritage protection of the Building provided under this Bylaw and the Heritage Revitalization Agreement; and
- (d) determine whether the terms, requirements and conditions of a heritage alteration permit have been met.

RECONSIDERATION BY COUNCIL

10. An applicant or owner whose application for a heritage alteration permit for alteration of the Building has been considered by an authorized delegate may apply for a reconsideration of the matter by Council, and such reconsideration shall be without charge to the applicant or owner.

GIVEN FIRST READING this _____ day of _____ 2022.

GIVEN SECOND READING this _____ day of _____ 2022.

PUBLIC HEARING held this _____ day of _____ 2022.

GIVEN THIRD READING this _____ day of _____ 2022.

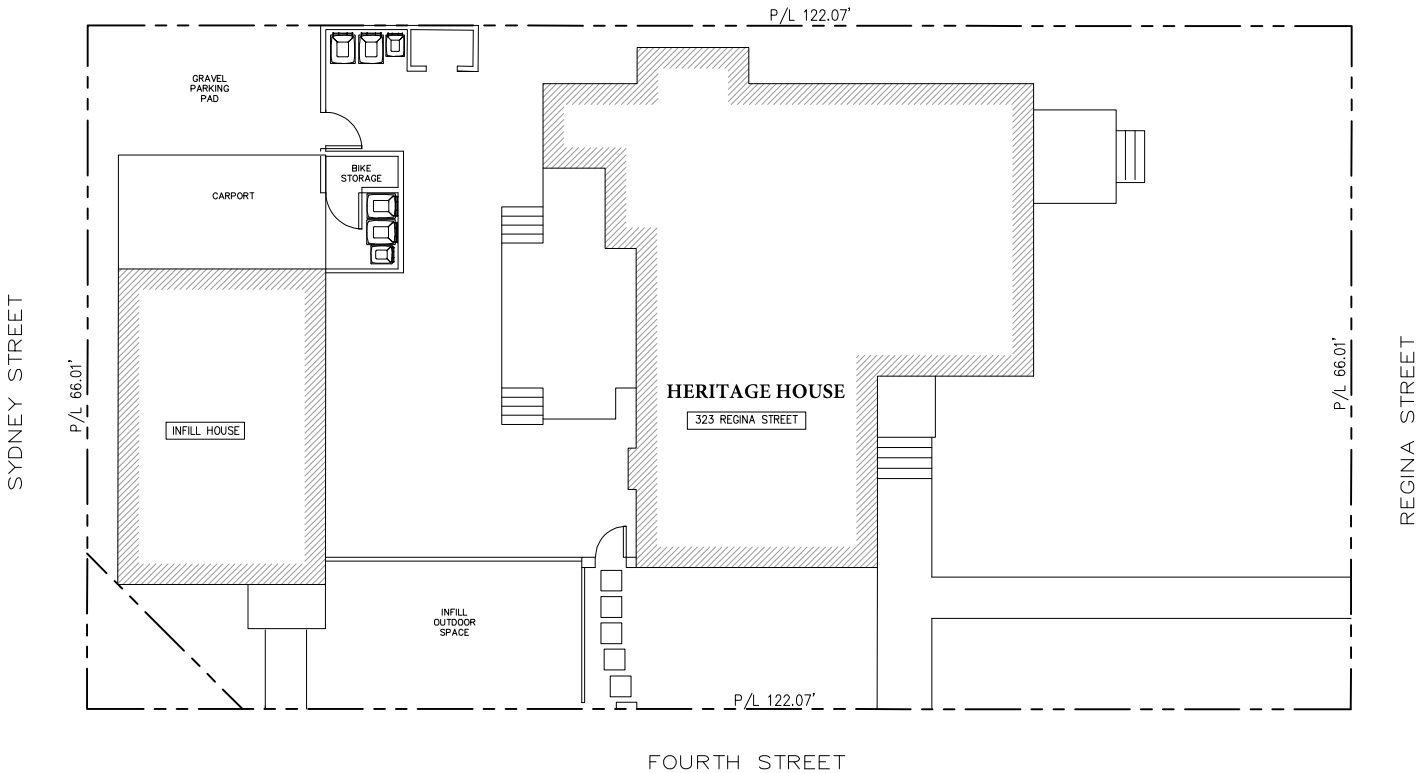
ADOPTED and the Seal of the Corporation of the City of New Westminster affixed this
_____ day of _____ 2022.

MAYOR JONATHAN X. COTE

JACQUE KILLAWEE, CITY CLERK

SCHEDULE A

SKETCH



1 SITE PLAN
— SCALE: 3/32"=1'-0"



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NEW WESTMINSTER, B.C., V3L 3M7
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E-MAIL NDHEILLY@SHAW.CA

PROJECT TITLE.
**HERITAGE RESTORATION
AGREEMENT,
323 REGINA ST.,
NEW WESTMINSTER, B.C.**

DRAWING TITLE.
**SIMPLE SITE
PLAN**

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CAD FILE NAME.
19-254

DATE. REVISED.
DECEMBER 2021 DEC 7, 2021

SCALE.
3/32"=1'-0"

DRAWING NO.

A-0.3

Attachment 3

Policy and Regulations Summary

ATTACHMENT 3: POLICY AND REGULATIONS SUMMARY

Official Community Plan (OCP) Land Use Designation

The Official Community Plan (OCP) sets out the City's anticipated land use for the future, for the purposes of guiding development applications. In the OCP, this property is designated Residential Detached and Semi-Detached Housing (RD). This designation envisions a mix of low density residential units including houses, duplexes, secondary suites, and laneway or carriage houses. The proposed application is consistent with this the RD designation.

Projects with Heritage Assets

The OCP encourages the use of Heritage Revitalization Agreements when a heritage asset on the site is appropriately incorporated into a development. Through this type of agreement, the OCP land use designation indicates that the development may be used to permit the housing forms listed in Residential – Ground oriented Infill Housing (RGO) designation. RGO is intended to allow a mix of ground oriented infill housing forms which are complementary to the existing neighbourhood character, and may include single detached dwellings, single detached dwellings on a compact lot, and other forms. The proposed application is consistent with this designation.

Queen's Park Heritage Conservation Area

The site is located in the Queen's Park Heritage Conservation Area though is not a protected property: its heritage protection was removed by Council during the Special Limited Category Study in 2018.

As a non-protected property, changes to the exterior do not require a Heritage Alteration Permit (HAP) and the property is not eligible for the Heritage Conservation Area's incentives program. The proposed Heritage Designation and HRA would provide a higher level of protection, design control, and development regulations than the Heritage Conservation Area. The additional protection and sensitive infill proposed is consistent with the goals of the Heritage Conservation Area.

Special Limited Category Study

Through the Heritage Conservation Area policy development process, approximately 80 properties were identified for further study and were categorized as Special Limited. An additional 12 protected properties were added through an Expanded Study application period. Through the three phases of the Study, the properties were reclassified as either Protected or Non-Protected, based on detailed analysis of their heritage merit and development options.

In Phase One, the City hired heritage professionals to assess the heritage value of the initial properties in the Special Limited category. In June 2018 Council removed

protection from all studied properties (33) which scored less than 60% in their assessment. This property scored 56% and as such its protection was removed. The low score was due to its lack of social-cultural value: at the time, the house is not associated with a significant person, event, tradition, or practice. The remaining properties continued to Phase Two of the Study.

Heritage Assessments were then completed for the 12 Expanded Study properties in early 2019. Based on the results of this work, six properties continued to Phase Two of the study. The other six were reclassified to Non-Protected during Phase Three.

In Phase Two, the City hired an architectural firm in mid-2019 to assess the potential of each remaining house to reach the maximum floor space permitted on that site in the Zoning Bylaw.

In Phase Three, the properties were evaluated against the Evaluation Checklist, which weighs criteria for heritage value, development potential and building integrity. Based on the results of this work, Council removed Heritage Conservation Area protection from seven properties on November 25, 2019. The remaining properties became Protected.

Queen's Park Heritage Conservation Area Design Guidelines

The Queen's Park Heritage Conservation Area Design Guidelines are the basis for assessing projects within the Queen's Park neighbourhood. The evaluation is based on an examination of the existing character of the surrounding area and the building itself. The guidelines aim to respect the integrity of historic buildings, while ensuring new construction is sympathetic to the character of the neighbourhood. The proposed application is generally consistent with these design guidelines.

Zoning Bylaw

The existing zoning for the site is RS-4 Queen's Park Single Detached Dwelling District. The intent of this district is to allow single detached dwellings with secondary suites and a laneway or carriage house. In this zone, the maximum floor space ratio (FSR) for principal houses which are protected under the Heritage Conservation Area is 0.7 and 0.5 for non-protected houses. A carriage house up to 0.1 FSR would also be permitted in either case. The proposed application would require relaxations to the Zoning Bylaw (as noted in the following sections of the report). As such, a Heritage Revitalization Agreement is proposed to permit the proposal.

Heritage Revitalization Agreement

A Heritage Revitalization Agreement (HRA) is a negotiated agreement between the City and a property owner for the purposes of heritage conservation. In exchange for long-term legal protection through a Heritage Designation Bylaw and exterior restoration, certain zoning relaxations may be considered (as noted above). An HRA is not

precedent setting, as each one is unique to a specific site. The *Policy for the Use of HRAs* lays out the process for HRAs and the relaxations which may be considered.

Heritage Related Design Guidelines

Council endorsed *The Standards and Guidelines for the Conservation of Historic Places in Canada* in 2008 as a basis for assessing heritage projects within the city. These are national guidelines for best practice in heritage conservation and design. All HRA proposals are carefully evaluated using this document to ensure conservation work on the exterior of the heritage building is in compliance. Additionally, the design of the adjacent new buildings are reviewed against the principles and guidelines in this document.

Heritage Designation Bylaw

A heritage asset which is the subject of an HRA is also protected by a Heritage Designation Bylaw. This Bylaw is a regulation that places long-term legal protection on the land title of a property. Any changes to a protected heritage property must first receive approval from City Council (or its delegate, the Director of Development Services) through a Heritage Alteration Permit (HAP). Future development is no longer entitled, but could be permitted by Council with an HAP. HAP applications are also evaluated by staff against the Standards and Guidelines and the Heritage Conservation Area guidelines, where appropriate.

The proposed Heritage Designation Bylaw would provide stronger development and design controls than the Conservation Area, and would also result in the property being added to the City's Heritage Register.

City-led Consultation

City-led consultation is not conducted on HRA projects in favour of moving the Bylaws through Public Hearing. The project was listed on Be Heard New West, the City's online community engagement platform, with a description of the project, review stages and timelines, as well as project drawings and links to various staff and committee reports. Information about the applicant-led consultation was also posted on Be Heard New West. These tools are used to gather community feedback, which staff review with the applicant and take into consideration as part of the project.

Attachment 4

Proximity to Transit Service and Other Sustainable Transportation Options

ATTACHMENT 4: PROXIMITY TO TRANSIT SERVICE AND OTHER SUSTAINABLE TRANSPORTATION OPTIONS

The site has a sidewalk on both Regina and Fourth Streets but not on Sydney Street: the installation of one would not be required as part of this development. Located nearby, within 0.3 km. (0.2 mi.), Second Street forms part of the bikeway/greenway network. Though not typically required, enclosed bike storage is being proposed for all units.

Table 3: Adjacent Transit Service to 323 Regina Street

Transit Facility	Frequency	Distance
Bus #155	Approx. 30 min	54 m. (177 ft.) to bus stop on Sixth Ave
Bus #105	Approx. 30 mins	0.3 km. (0.2 mi.) to the bus stop at Second St
Buses #106, N19	Approx. 8 mins	0.3 km (0.2 mi.) to the bus stop at Sixth St frequent transit network (FTN)
Skytrain	2-5 mins	1.3 km (0.8 mi.) to Columbia Station

Attachment 5

Proposed Project Statistics and Relaxations

ATTACHMENT 5: PROPOSED PROJECT STATISTICS AND RELAXATIONS

A summary of the proposed project statistics are outlined in Tables 4-6. Relaxations being sought through the HRA are highlighted in grey.

Table 4: Summary of Overall Proposed Project Statistics

Attributes	RS-4 Zoning	Proposed	Relaxation
Number of Dwelling Units / Tenure	One single detached dwelling (SDD) with a secondary suite and a detached accessory dwelling unit	One single detached dwelling (SDD) and a detached accessory dwelling unit	--
Minimum Site Area	557 sq. m. (6,000 sq. ft.)	749 sq. m. (8,057 sq. ft.)	--
Lot Frontage	--	20.1 m. (66 ft.)	--
Lot Depth	--	37.2 m. (122 ft.)	--
Maximum Floor Space Ratio*	0.6	0.604	--
Maximum Floor Space	449 sq. m. (4,834 sq. ft.)	452 sq. m. (4,863 sq. ft.)	--
Maximum Number of Units	3	2	--
Minimum Off-Street Parking	2 spaces	2 spaces	--
Minimum Parking Space Setback from Property Line	1.5 m. (5 ft.)	1.5 m. (5 ft.)	--

* includes 0.5 FSR for non-protected principal building and 0.1 for detached accessory dwelling unit

NOTE: grey rows indicate proposed variances, white rows meet City regulations.

Table 5: Summary of Proposed Project Statistics for 323 Regina Street (Heritage House)

Attributes	RS-4 Zoning	Proposed	Relaxation
Maximum Floor Space	374 sq. m. (4,029 sq. ft.)	320 sq. m. (3,443 sq. ft.)	54 sq. m. (586 sq. ft.)
Maximum Floor Space Ratio	0.5	0.43	--
Maximum Number of Units	2	1	--
Maximum Site Coverage	35%	21%	--
Minimum Front Setback (south)*	5.8 m. (19 ft.)	9.4 m. (30.71 ft.)	--
Minimum Rear Setback (north)*	7.4 m. (24.4 ft.)	13.4 m. (44 ft.)	--
Minimum Left Side Setback (west)*	1.5 m. (5 ft.)	4.2 m. (13.7 ft.)	--
Minimum Right Side Setback (east)*	1.5 m. (5 ft.)	0.6 m. (2.1 ft.)	0.9 m. (2.9 ft.)
Maximum Height (Roof Peak)	10.7 m. (35 ft.)	7 m. (23 ft.)	--
Maximum Height (Midpoint)	7.6 m. (25 ft.)	5.3 m. (17.5 ft.)	--
Maximum Attached Accessory Area	10%	4%	--
Minimum Off-Street Parking	1 space	1 space	--
Minimum Parking Space Setback from Property Line	1.5 m. (5 ft.)	1.5 m. (5 ft.)	--

* existing setback

NOTE: grey rows indicate proposed variances, white rows meet City regulations.

Table 6: Summary of Proposed Project Statistics for 471 Fourth Street (Infill House)

Attributes	RS-4 Zoning	Proposed	Relaxation
Maximum Floor Space Ratio	0.1	0.18	0.08
Maximum Floor Space *	74.9 sq. m. (805.7 sq. ft.)	132 sq. m. (1,420 sq. ft.)	57.1 sq. m. (614.3 sq. ft.)
Maximum Number of Units	1	1	--
Maximum Site Coverage	10%	8%	--
Fourth Street Setback (west front)	1.5 m. (5 ft.)	2.3 m. (7.75 ft.)	--
Sydney Street Setback (north side)	1.5 m. (5 ft.)	0.9 m. (3 ft.)	0.6 m. (2 ft.)
Minimum Side Setback (east rear)	1.5 m. (5 ft.)	7.2 m. (23.5 ft.)	--
Minimum Side Setback (south) <i>Separation between buildings</i>	4.9 m. (16 ft.)	5.2 m. (17 ft.)	--
Maximum Height (Peak)	7.0 m. (23 ft.)	6.9 m. (22.5 ft.)	--
Maximum Front Entry Landing Area	3 sq. m. (32 sq. ft.)	3 sq. m. (32 sq. ft.)	--
Minimum Off-Street Parking	1 space	1 space	--
Minimum Parking Space Setback from Property Line	1.5 m. (5 ft.)	1.5 m. (5 ft.)	--

* Under the Heritage Conservation Area Incentives Program, the size of the infill house for a protected property could be increased up to 89 sq. m. (958 sq. ft.) with a corresponding decrease in the size of the principal building.

NOTE: grey rows indicate proposed variances, white rows meet City regulations.

Attachment 6

Extract of July 12, 2021 Land Use and Planning Committee (LUPC) Meeting Minutes

REGULAR MEETING OF THE LAND USE AND PLANNING COMMITTEE

July 12, 2021 at 10:00 a.m.

**Meeting held electronically under Ministerial Order No. M192/2020 and
the current Order of the Provincial Health Officer - *Gatherings and Events***

MINUTES - Extract

7. 323 Regina Street: Heritage Revitalization Agreement – Preliminary Report

Kathleen Stevens, Heritage Planning Analyst, provided a PowerPoint presentation and reviewed the July, 2021 staff report, including background into the proposed Heritage Revitalization Agreement (HRA) for 323 Regina Street, and outlined the desired feedback from the Committee.

In response to questions from the Committee, Ms. Stevens and Jackie Teed, Senior Manager of Development Services, provided the following information:

- The proposed infill house would be larger than what is currently allowed under the carriage house program; and,
- Given the configuration of the lot, a small lot subdivision may be a more appropriate consideration for the application.

Discussion ensued and the Committee provided the following comments:

- This is a difficult application to consider given the current pause on HRAs and the hesitancy from Council to allow stratifications in Queen's Park;
- While increased density would be beneficial on the larger lots in Queen's Park, carriage houses are not the most desperately needed "missing middle" housing form in the City;
- The application may be contentious given the proposed massing of the carriage house, the overall density on the property, and stratification;
- The fact that this application would support inter-generational living is of benefit;
- As the property is not protected in the Heritage Conservation Area, the conversation about stratification has a different context and it would be beneficial for the application to be discussed at Council, and to receive comments from the community;

- Returning heritage protection to the house would be of benefit; and,
- A smaller infill house with no stratification could be more supportable.

MOVED and SECONDED

THAT the Land Use and Planning Committee recommend that stratification be removed as a consideration as part of the Heritage Revitalization Agreement for the 323 Regina Street application.

CARRIED.

All members of the Committee present voted in favour of the motion.

MOVED and SECONDED

THAT the Land Use and Planning Committee recommend reducing the size of the proposed carriage house as part of the Heritage Revitalization Agreement for the 323 Regina Street application.

CARRIED.

(Councillor Nakagawa opposed)

MOVED and SECONDED

THAT the Land Use and Planning Committee direct staff to refer the 323 Regina Street Heritage Revitalization Agreement application to the Community Heritage Commission to review the heritage merit.

CARRIED.

All members of the Committee present voted in favour of the motion.

MOVED and SECONDED

THAT the Land Use and Planning Committee direct staff to refer the Committee's recommendations in regards to the 323 Regina Street Heritage Revitalization Agreement application to Council for further consideration.

CARRIED.

All members of the Committee present voted in favour of the motion.

Attachment 7

*Extract of October 6, 2021
Community Heritage Commission Meeting
Minutes*

COMMUNITY HERITAGE COMMISSION**MINUTES - Extract**

Wednesday, October 6, 2021

**Meeting held electronically and open to public attendance
in Council Chamber, City Hall**

5. REPORTS AND PRESENTATIONS**5.1 Heritage Revitalization Agreement Application: 323 Regina Street**

Kathleen Stevens, Heritage Planning Analyst, reviewed the staff report dated October 6, 2021 regarding an application for a Heritage Revitalization Agreement (HRA) to construct an infill rental house at 323 Regina Street, noting that this is a non-protected property in the Queen's Park Heritage Conservation Area.

Gary Holisko, Owner/Applicant of 323 Regina Street, shared that the infill house will be for his son and fiancé to live in and clarified that major restoration of the house prior to applying for a heritage designation was undertaken due to flooding in the basement that required immediate action.

Susan Medville, Principal, Mountain Heritage, provided a PowerPoint presentation which outlined the following:

- The proposal to retain the 1928 house and build a new infill house on the property;
- The heritage values and character defining elements of the property;
- Comparative views of the existing house and outline of heritage conservation that occurred from 2019 to 2020;
- The benefits of heritage recognition; and,
- The lack of impact that a new infill house would have on the existing house.

The Commission provided the following comments:

- Most Commission members expressed general support for the proposal;
- It is refreshing to see restoration done on a house before infill housing is requested;
- If the restoration work was not done, the house may not have met HRA criteria which is a dangerous precedent to set; and,

- Concerns expressed included the use of vinyl windows, the height of the upper floor dormers, the roof over the porches not mirroring the rolled shingles on the main roof eaves, and the large size of the proposed infill house.

MOVED and SECONDED

THAT the Community Heritage Commission recommend that Council support the Heritage Revitalization Agreement for 323 Regina Street and its inclusion on the City's Heritage Register.

Carried.

Maureen Arvanitidis voted in opposition of the motion.

Attachment 8

Applicant-led Consultation Feedback and Correspondence Received

ATTACHMENT 9: APPLICANT-LED CONSULTATION FEEDBACK AND CORRESPONDENCE RECEIVED

Applicant-led Community Consultation Summary

The applicant-led consultation utilized a digital engagement platform and notification of consultation opportunities was sent to properties within 100 metres of the project site and the Queen's Park Residents Association. The project website (<https://ndheilly.wixsite.com/edgar-house>) included project details and the methods available to provide feedback to either the applicant or City staff.

An online survey, hosted on the website, was open between September 28 and October 27, 2021 and an online Open House was held on October 13, 2021 through Zoom. A total of 71 survey responses were received and approximately 19 people attended and provided feedback at the Open House.



Date: Oct 27th, 2021

Re: Edgar House 1923 Heritage Revitalization Agreement Applicant-led Public Consultation

Address: 323 Regina Street, New Westminster, B.C.

Dear Kathleen Stevens,

We are pleased to present the findings of our Applicant-led Public Consultation which is summarized below and supported by the following documents:

Survey Data

Timeline:

Website 'ndheilly.wixsite.com/edgar-house', including survey, project drawings and Heritage Conservation Report, launched September 28th, 2021.

Email sent to QPRA October 7th, 2021.

66 postcards delivered to neighbours within 100m radius between September 28 and 30th, 2021.

Survey closed at 1pm Oct 27th.

Survey Responses

The online survey included nine questions: seven requested feedback and ratings related to the projects design, location and restoration work, two pertained to the respondent's relationship to the city and Queens Park. There were 71 respondents in total.

Feedback is summarized below:

Q1 'The Edgar House is presently not protected by the Heritage Conservation Area; do you support protecting it?'

70 responses

Yes	72.86%
No	27.14%

Q2 'Tell us what you like about the project (check all that apply)?'

52 responses

Heritage Preservation	76.92%
Gentle Infill	73.08%
Intergenerational Living	88.46%
Design	76.92%
Location	59.62%



Q3 'Tell us what you would change?'

40 Responses

Location	0%
Size	27.50%
Design	2.5%
Other	70.00%

Comments in Survey Data

Themes were: No changes to the proposal, size of infill, loss of green space

Q4 'Do you like the restoration work that has been done?'

69 Responses

High	59.42%
Somewhat high	14.49%
Moderate	7.25%
No improvement	5.80%
Do not like the restoration	13.04%

Q5 'In general, do you like the proposed infill house?'

69 Responses

A great deal	42.03%
A lot	26.09%
A moderate amount	7.25%
A little	4.35%
None at all	20.29%

Q6 'Do you support infill to allow for intergenerational living?'

65 Responses

Yes	90.77%
No	9.23%

Q7 'Do you support this proposed project?'

70 Responses

Yes	68.57%
No	31.43%

Comments in Survey Data

Themes were: Support for/Concern about increased density, Heritage Preservation, Process

Q8 'Are you a New Westminster Resident?'

71 Responses

Yes	85.92%
No	14.08%



Q9 'Do you live in the Queens Park Neighbourhood?'

70 Responses

Yes	72.86%
No	27.14%

Neighbourhood Open-House

The Neighbourhood Open-House was held via Zoom on Wednesday October 13th from 6-8 pm and was well attended by predominantly opponents of the proposal. Gary had informed those he handed out the project information sheet to in the neighbourhood that it was not necessary to attend the open house, and that filling in the survey was helpful. The project team was on hand to answer questions and to hear feedback. There was a lively discussion with many questions about the project. The themes that emerged were density, process, history of the project, and green space.

There was evident confusion about the property not being included in the HCA and it now being proposed to be protected through an HRA. The previous owners had not seen the desirability of having their home included in the HCA but the new owners wishing to protect the Storybook House. A Heritage Revitalization Agreement is the best tool for them to accomplish this goal.

Many were concerned that the restoration work was completed before the HRA process had begun. To address this concern, we had the heritage professional do up a memo that addresses the restoration and renovation work to comment on whether they met the "Standards and Guidelines for the Conservation of Historic Places in Canada". She found that all these modifications met the Guidelines for historic places.

Density of the infill house was brought up as a concern, the infill house was previously reduced in size to keep the above grade square footage more in keeping with the Laneway/Carriage House guidelines. The total density on the property is proposed to be 60% with only 47% above grade.

Green Space was a concern, the existing garage will be removed, and this will help maintain greenspace on the property. Total site coverage is well below the allowable at only 28.4%.

Summary of Findings

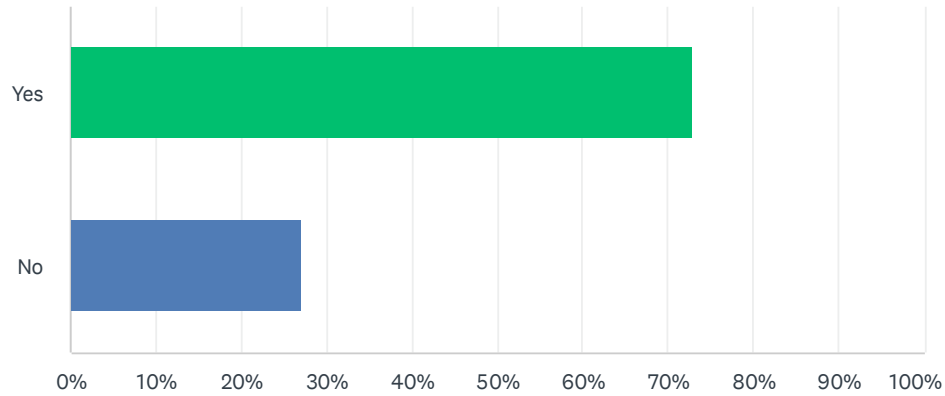
Overall, there was a mix of support for the project with the survey responses tallied 68.6% in favour and 31.4% against. Support focused on Heritage Protection, Gentle Infill, Housing Options, and Intergenerational Living. Some concerns that were brought up focused on Heritage Value, Process, and Infill House Size.

Regards,

Nancy G Dheilly, B.E.S., B.Arch.
she/her

Q1 The Edgar House is presently not protected by the Heritage Conservation Area, do you support protecting it?

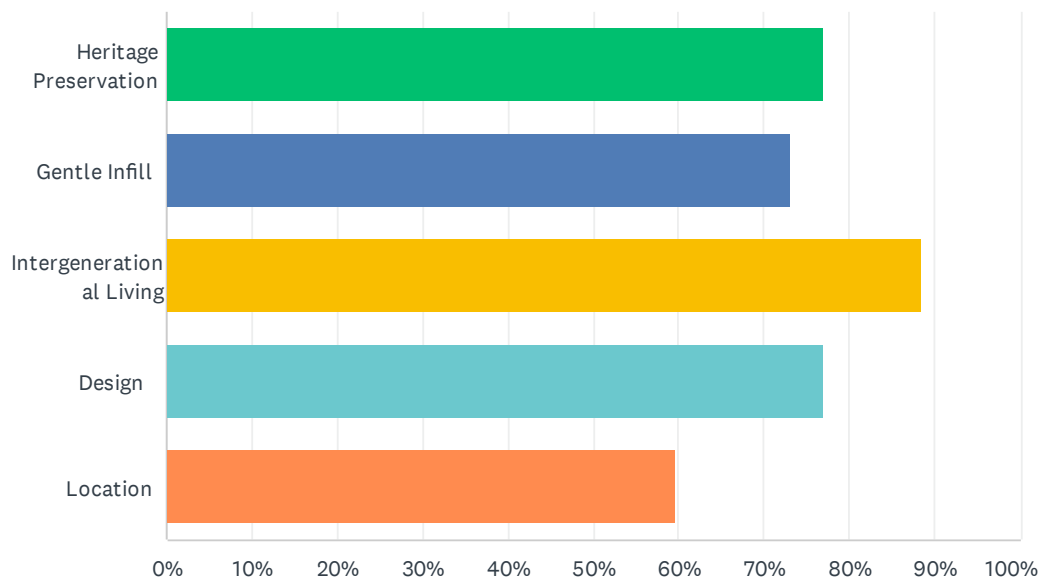
Answered: 70 Skipped: 1



ANSWER CHOICES	RESPONSES	
Yes	72.86%	51
No	27.14%	19
Total Respondents: 70		

Q2 Tell us what you like about the project (check all that apply)?

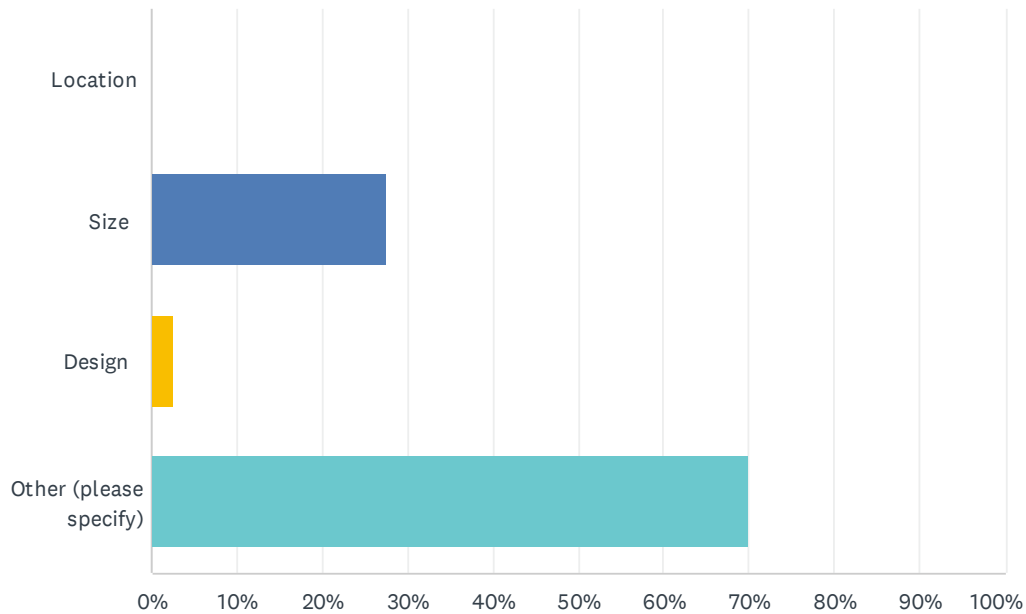
Answered: 52 Skipped: 19



ANSWER CHOICES	RESPONSES	
Heritage Preservation	76.92%	40
Gentle Infill	73.08%	38
Intergenerational Living	88.46%	46
Design	76.92%	40
Location	59.62%	31
Total Respondents: 52		

Q3 Tell us what you would change?

Answered: 40 Skipped: 31



ANSWER CHOICES	RESPONSES	
Location	0.00%	0
Size	27.50%	11
Design	2.50%	1
Other (please specify)	70.00%	28
TOTAL		40

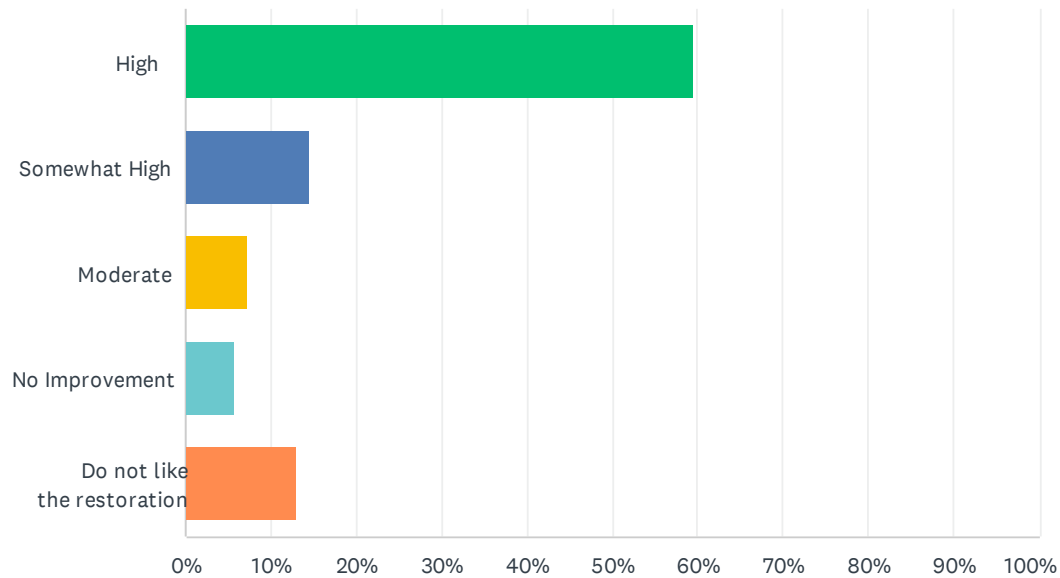
#	OTHER (PLEASE SPECIFY)	DATE
1	There is nothing I see that needs to be changed	10/25/2021 1:13 PM
2	Nothing	10/25/2021 9:43 AM
3	I would change nothing, based on what I've seen.	10/24/2021 1:53 PM
4	Nothing	10/23/2021 5:34 PM
5	Bigger suite for family	10/23/2021 4:22 PM
6	Nothing, infill laneway home plans are more than acceptable	10/23/2021 12:25 PM
7	Nothing	10/19/2021 7:05 PM
8	Nothing	10/19/2021 7:02 PM
9	Remove the infill house	10/18/2021 7:10 AM
10	All of the above	10/17/2021 11:50 AM
11	Nothing, it looks like a very well thought out plan.	10/14/2021 8:10 PM
12	All of the above, Infill is much too large, laneway house should face lane laneway house	10/13/2021 8:03 PM

should not have basement. Design is not complimentary to existing house or streetscape. If they want a house of this size, it would be better to apply for small lot subdivision

13	The primary objection is the infill house - I feel like I've give this feedback over and over. I am not against turning the primary house into a duplex.	10/13/2021 6:35 PM
14	Nothing it's perfect	10/13/2021 4:33 PM
15	no change - leave house and property as is or renovate without asking for any variances	10/12/2021 11:26 PM
16	Nothing	10/12/2021 8:17 PM
17	Nothing	10/12/2021 8:13 PM
18	Don't support infill housing.	10/12/2021 4:44 PM
19	Nothing	10/12/2021 2:47 PM
20	The infill house is great but I do think it could be slightly larger if desirable to allow for family use. The corner lot size and location would be suitable for subdivision so I don't see why not.	10/12/2021 11:08 AM
21	Nothing	10/10/2021 8:50 PM
22	Nothing	10/9/2021 8:34 PM
23	I do not support this - increasing densification as a carrot to preserve heritage houses at the cost of green space is short sighted.	10/9/2021 5:13 PM
24	nothing - more infill!!	10/8/2021 10:17 AM
25	No objections	10/7/2021 9:02 PM
26	All of the above	10/7/2021 9:52 AM
27	Size and design	10/4/2021 3:47 PM
28	The infill house should be smaller and the process is not similar to what other people need to do.	10/3/2021 8:31 PM

Q4 Do you like the restoration work that has been done?

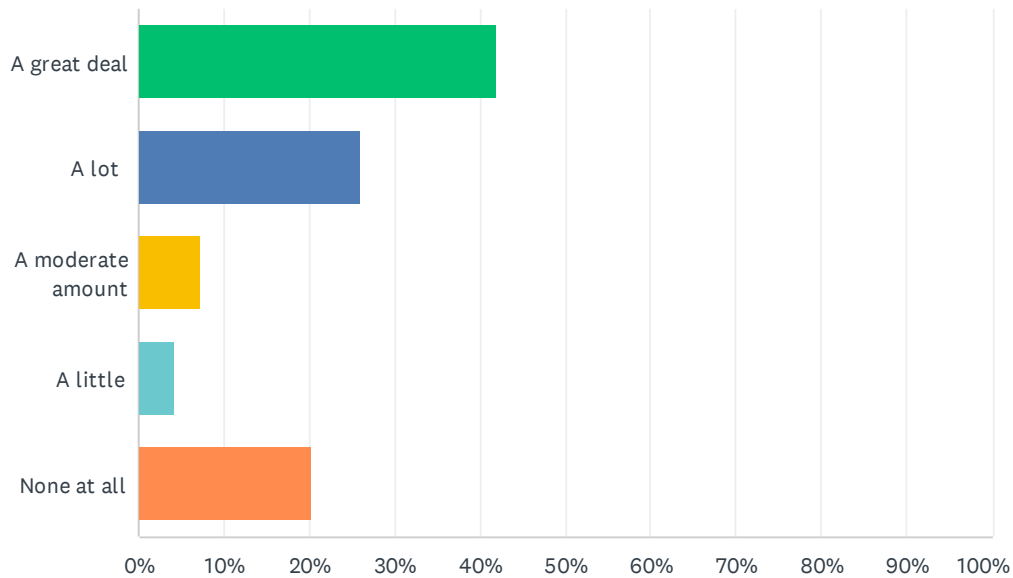
Answered: 69 Skipped: 2



ANSWER CHOICES	RESPONSES	
High	59.42%	41
Somewhat High	14.49%	10
Moderate	7.25%	5
No Improvement	5.80%	4
Do not like the restoration	13.04%	9
TOTAL		69

Q5 In general, do you like the proposed infill house design?

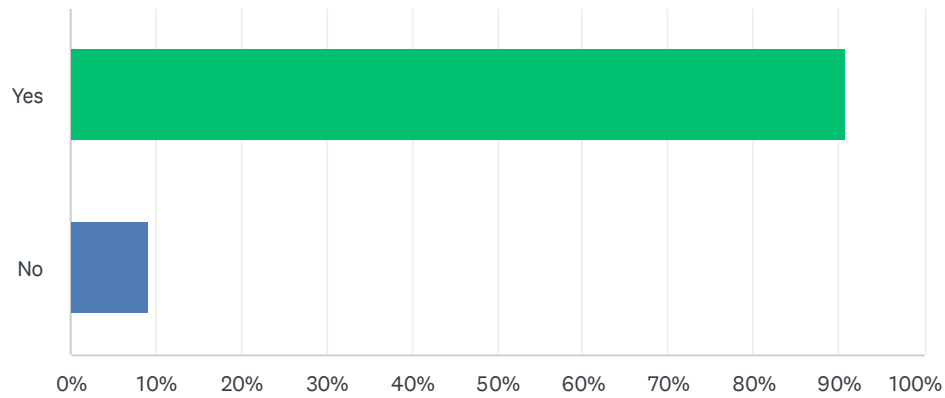
Answered: 69 Skipped: 2



ANSWER CHOICES	RESPONSES	
A great deal	42.03%	29
A lot	26.09%	18
A moderate amount	7.25%	5
A little	4.35%	3
None at all	20.29%	14
TOTAL		69

Q6 Do you support infill to allow for intergeneration living?

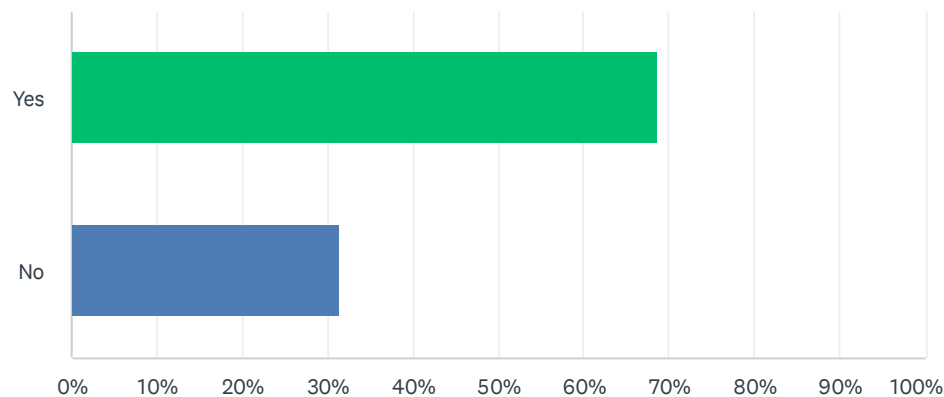
Answered: 65 Skipped: 6



ANSWER CHOICES	RESPONSES	
Yes	90.77%	59
No	9.23%	6
Total Respondents: 65		

Q7 Do you support this proposed project?

Answered: 70 Skipped: 1



ANSWER CHOICES	RESPONSES
Yes	68.57% 48
No	31.43% 22
Total Respondents: 70	

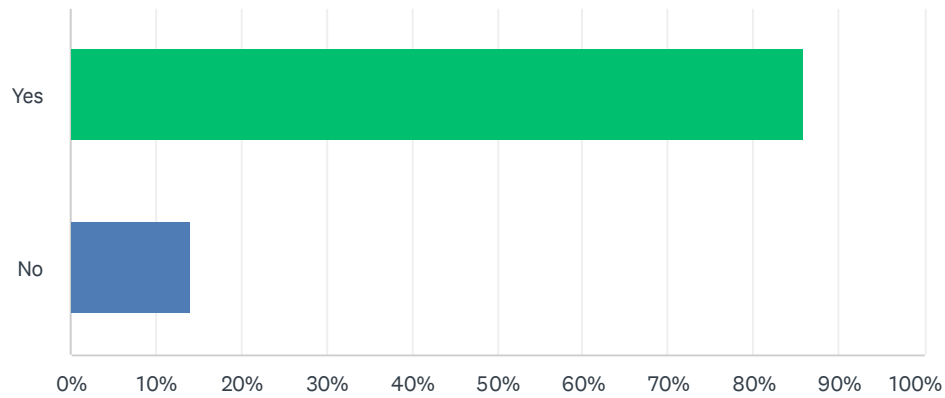
#	LET US KNOW WHY.	DATE
1	New West needs WAY more density, and the city needs to start allowing projects like this to go through.	10/25/2021 5:05 PM
2	I like the addition of the new infill house, and am a strong proponent of laneway housing in general	10/25/2021 1:13 PM
3	The original house has been beautifully restored and the infill house design matches the character of both the main house and neighbourhood. Allowing for intergenerational housing options in an impossible market helps support a vibrant community and family friendly neighbourhood.	10/25/2021 9:43 AM
4	Nice way to provide affordable, densifying housing .	10/24/2021 8:02 PM
5	It is a positive move on a number of fronts.	10/24/2021 1:53 PM
6	I think it is very progressive to encourage and support intergenerational living.	10/23/2021 5:39 PM
7	Creates more housing/density and aligns with the HRA incentives Queens Park was promised	10/23/2021 5:34 PM
8	As a frequent visitor to the neighbourhood, I'd consider it consistent with respect for neighbours and the surrounding environment & adding to its beautification, as well as being respectful of the health & well-being of the 2 generations of the family who would be living there.	10/23/2021 1:56 PM
9	Existing 1928 home should be a heritage home and we need higher density laneway homes	10/23/2021 12:25 PM
10	Size and design not in keeping with heritage.	10/21/2021 9:33 PM
11	It is an improper use of the HRA process.	10/18/2021 9:15 AM
12	We should preserve houses with character. This style is attractive and not very common. Intergenerational living is something we are considering for our family. Look at the affordability of homes these days!	10/17/2021 7:47 PM
13	Taking advantage of the HRA after making changes to the original design of the heritage home	10/17/2021 11:50 AM

14	The owners deceitfully performed renovations to a non-protected home and now want the benefits offered under the HCA	10/17/2021 8:03 AM
15	It is a win/win. Providing protection for existing beautiful heritage home while providing much needed affordable and aesthetically pleasing heritage style family home	10/14/2021 8:10 PM
16	I think laneway housing may be the only way younger generations will be able to afford their own homes	10/14/2021 8:04 PM
17	location and size of 'laneway' house with basement, fact that house has already been renovated so that it would not meet heritage guidelines and they are now seeking heritage protection after the fact in order to reap the benefits of protection.	10/13/2021 8:03 PM
18	I do not support the infill house - it is squeezed onto a small lot and does not uphold the look of the street.	10/13/2021 6:35 PM
19	Great spot for it	10/13/2021 4:33 PM
20	48% larger than is allowed. Stick to the rules - that's why they were created.	10/13/2021 1:23 PM
21	follow the rules already set out. too many exceptions are diluting the effectiveness of the already established rules and guidelines	10/12/2021 11:26 PM
22	The infill is a house that is too big for the property.	10/12/2021 8:32 PM
23	The project is a win-win—the owners will be allowed to build an infill home for <small>Personal Information Removed</small> and the house will be designated and protected. It does what HRAs are supposed to do which is balance the benefits to both the homeowner and the municipality. The design of the infill house also fits in well with the architectural context of the surrounding neighbourhood. And it adds gentle density that is sensitive to the neighbourhood and in keeping with the City's planning objectives.	10/12/2021 7:54 PM
24	Not in favour of Increased density. Site is too small. Infill will be too close to road.	10/12/2021 4:44 PM
25	It would be a Quality develop enhancing the neighbourhood.	10/12/2021 2:47 PM
26	Excellent example of gentle infill. Projects like this are a no brainer to move forward with when there is a win-win with heritage preservation as well.	10/12/2021 11:08 AM
27	Infill should be restricted to what is permitted,a 958 sq ft laneway.the current owners have stripped down even further any significant heritage by altering both the front and sides of the 1928 home.It never made the cut to be protected to begin with and the current owners have further stripped heritage value by changing the storybook winding walkways , removing a heritage front window and replacing with a new French door, putting in vinyl windows and changing roof line.This would not have been allowed if "protected", but new owners want to have it both ways.Abuse of an HRA	10/12/2021 10:36 AM
28	It will be a beautiful addition to the neighbourhood.	10/10/2021 8:50 PM
29	As above, losing green space, increasing densification in a single family residential neighbourhood destroys the safety (cars, people) and personality as surely as tearing down heritage homes does.	10/9/2021 5:13 PM
30	Too large for the lot, house should have been protected to begin with not proposed after changes are made. Feels a bit like a bargaining chip in exchange for what they want.	10/9/2021 4:33 PM
31	We need more housing, everywhere	10/8/2021 10:17 AM
32	We need to support affordable housing options	10/7/2021 9:02 PM
33	its too big and has a basement. Why give it HRA status when it is not protected heritage building?	10/7/2021 1:32 PM
34	Infill needs to be 30% smaller	10/7/2021 9:52 AM
35	It is a sensible plan that allows for intergenerational living in a time of severe housing crisis for our younger generations. Queens Park loses nothing, and only gains, from this sensible and well thought out plan that adds new younger residents, while preserving and protecting existing heritage.	10/7/2021 9:26 AM
36	We need infill and increased density (gently) in QP & NW.	10/7/2021 8:52 AM

37	https://www.gocomics.com/theflyingmccoys/2021/10/01	10/6/2021 2:49 PM
38	The original house was not restored in the storybook style in keeping with the house. It was an unsympathetic renovation which got the owners what they wanted but did nothing to enhance the character of the home. So now that they have what they want they are asking for designation on a badly altered exterior.	10/5/2021 2:01 PM
39	The house has lost pretty much any heritage value with the bad renovation so no need to protect it	10/4/2021 3:47 PM
40	The size and the way the process has been undertaken.	10/3/2021 8:31 PM
41	I do not understand the trade-off (willing to protect Edgar building in exchange for variances). All other buildings in HCA have to adhere to carriage/laneway house size limits, so should the Edgar house were it in the HCA. The restorations to the Edgar house have already been done: the requested variances are not needed to fund the already completed restorations. I do not understand why there are not the usual huge "Variance Proposal" bill-board on the property. Once again, why is this property so special that it can circumvent the usual procedures. I find it curious that the previous owners worked hard to get the Edgar house *excluded* from the Queen's Park HCA and Council agreed. Now the City is being asked for variances to put the house back in the HCA.	9/29/2021 9:21 AM

Q8 Are you a New Westminster Resident?

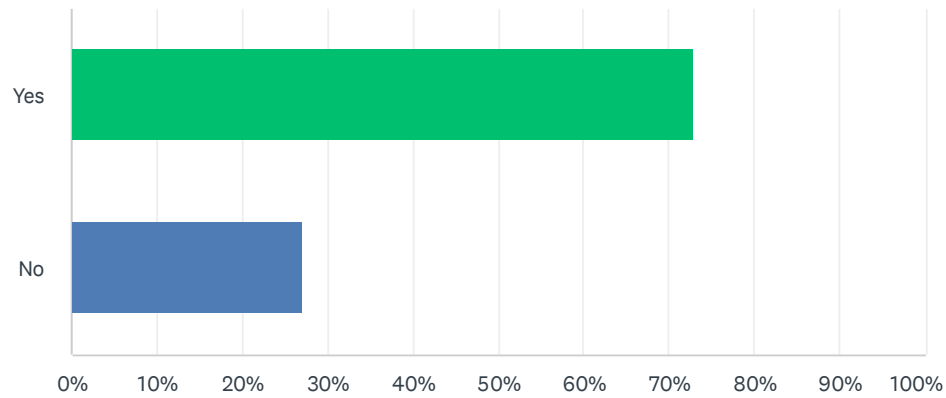
Answered: 71 Skipped: 0



ANSWER CHOICES	RESPONSES	
Yes	85.92%	61
No	14.08%	10
Total Respondents: 71		

Q9 Do you live in the Queens Park Neighbourhood?

Answered: 70 Skipped: 1



ANSWER CHOICES	RESPONSES	
Yes	72.86%	51
No	27.14%	19
Total Respondents: 70		

From: [Gillian Day](#)
To: [Lisa Spitale](#); [Emilie Adin](#); [Kathleen Stevens](#)
Cc: [Jacqueline Killawee](#); [Sophie Schreder](#); [Angela Danielisz](#); [Kathryn Beardsley](#)
Subject: FW: Preliminary Report - 323 Regina HRA application
Date: Monday, August 30, 2021 9:57:29 AM
Attachments: [image001.png](#)

Forwarded for information.

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

 City of New Westminster | Legislative Services
511 Royal Avenue, New Westminster, BC V3L 1H9
www.newwestcity.ca

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From: External-Clerks
Sent: Monday, August 30, 2021 9:56 AM
To: 'Gail QPRA' <presidentqptra@gmail.com>
Subject: RE: Preliminary Report - 323 Regina HRA application

Good morning,

I am writing to confirm receipt of your email. In addition to Council, It has been forwarded to the Chief Administrative Officer and the Director of Development Services.

Please note that your email may be included in the agenda package that is posted to the website after the Council meeting. Prior to posting, your email address with be redacted.

Yours truly,

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

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From: Gail QPRA <presidentqptra@gmail.com>
Sent: Sunday, August 29, 2021 3:39 PM
To: Chuck Puchmayr <cpuchmayr@newwestcity.ca>; Jonathan Cote <jcote@newwestcity.ca>;
Nadine Nakagawa <nnakagawa@newwestcity.ca>; Patrick Johnstone

<pjohnstone@newwestcity.ca>; Chinu Das <cdas@newwestcity.ca>; Jaimie McEvoy
<jmcevoy@newwestcity.ca>; Mary Trentadue <mtrentadue@newwestcity.ca>
Cc: External-Clerks <Clerks@newwestcity.ca>
Subject: Preliminary Report - 323 Regina HRA application

Mayor and Council:

Recognizing that it is early stages for this proposal, it is still important to give you the perspective of the neighbourhood before this moves along any further.

As the proposal states, this house did not score high enough to be one of the properties included in the Heritage Conservation Area . Whether this was the right decision on the part of the City or not, is not up for debate. When the current owners purchased the property, they said they wanted to protect the house and felt it really should have been included in the HCA. As a result of this, the Heritage Preservation Society offered to assist with the research and the report preparation that would be required to apply to the HCA and they also offered to assist with the fees that would be charged to go through the process . The owners did not take advantage of the offer, nor did they pursue this course of action despite the protection the HCA would provide to their house, as well as the incentives it could offer.

At that point they proceeded to renovate three sides of the house (not sympathetically) which would have bumped up against the guidelines of the HCA. To their credit, they also did a number of repairs as well as some much needed maintenance and a very attractive paint job, but very little that could be classified as restoration. In fact, original elements were removed from the Regina Street side of the house.

Now, and somewhat retroactively, they want to be granted an HRA for what they are presenting as restoration in order to add an oversized infill house to their property. These applicants opposed a development with similar elements directly across the street and one of their reasons was the “oversized carriage home” which, by comparison, was one third smaller than what they are putting forward. They also opposed that project because it could cause “mass redevelopment changing the entire character of the neighbourhood” and was a “backdoor misuse of an HRA”.

If adding density is more important than maintaining some integrity in the HRA process or in heritage preservation, there are other, more appropriate, ways to make that happen. Adding a laneway as per the OCP, for example, would be a welcome addition to the neighbourhood and would be closer to something affordable for someone wanting to live here. Increasing density does not increase affordability which is obvious with many of the recent proposals requesting the addition of more \$1M plus homes.

Councillor Puchmayr gave an insightful assessment of this proposal when it was reviewed at the LUPC and hopefully the upcoming refresh of the HRA may make it easier to weed out this kind of project that doesn't tick the boxes for requirements, or intent, of an HRA. It would be inappropriate to add a heritage plaque to a 2020 renovation instead of on a bona fide restoration project. This one needs to go back to the drawing board for rethinking.

Respectfully

Gail North

QPRA

Kathleen Stevens

From: Gillian Day
Sent: Monday, October 18, 2021 9:18 AM
To: Kathleen Stevens
Cc: Jacqueline Killawee; Emilie Adin; Jackie Teed
Subject: FW: 323 Regina Street Heritage Proposal

Forwarded for information.

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

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From: External-Clerks
Sent: Monday, October 18, 2021 9:17 AM
To: 'Dave Vallee' <dave@teamdavevallee.com>
Subject: RE: 323 Regina Street Heritage Proposal

Good morning,

I am writing to confirm receipt of your email. It has been forwarded to Development Services.

Yours truly,

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

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From: Dave Vallee <dave@teamdavevallee.com>
Sent: Sunday, October 17, 2021 7:23 PM
To: gpra.newwest@gmail.com
Subject: [EXTERNAL] 323 Regina Street Heritage Proposal

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To whom it may Concern,

I have looked through all the documentation and am strongly in favour of the 323 Regina Street HRA proposal. The owners have done a fabulous job restoring the existing home which leads me to believe they will do the same with the laneway home. I particularly like the landscaping and green space that will be maintained and improved.

It's a win/win with a beautiful 1928 home being protected for all time and the beautiful addition of an aesthetically pleasing, practical, and affordable and much needed in this city, laneway/carriage home.

While the size of the laneway home exceeds the maximum for non HRA laneway homes, I think the design is well thought out and the foot print of the new home is still 261 sq ft smaller than if they had built a maximize sized allowable garage, (which wouldn't be as attractive). Also the size of the home above the basement/cellar is only about 100 sq ft more than what would be allowed for a non HRA laneway home.

This type of development is what Queens Park needs more of and enhances and improves the street scape and provides much needed affordable. alternative housing needed and in this case even goes one step further, helping multi generations of a family to stay together.

Regards

Dave Vallee



Dave Vallee
Personal Real Estate Corporation
O: 604-526-2888
E: dave@TeamDaveVallee.com
W: www.TeamDaveVallee.com



Based on 2017 residential transaction sides. Source: CREA, RE/MAX

RE/MAX All Points Realty (New West)
#102 321 Sixth Street, New Westminster, BC, V3L 3A7

Kathleen Stevens

From: Gillian Day
Sent: Monday, October 18, 2021 3:44 PM
To: Jonathan Cote; Lisa Spitale; Emilie Adin; Jackie Teed; Chinu Das; Chuck Puchmayr; Chuck Puchmayr (Shaw); Jaimie McEvoy; Jaimie McEvoy (2); Mary Trentadue; Nadine Nakagawa; Patrick Johnstone
Cc: Jacqueline Killawee; Sophie Schreder; Angela Danielisz; Kathryn Beardsley; Kathleen Stevens
Subject: FW: [EXTERNAL] HRA 323 Regina Street

Forwarded for information.

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T 604.527.4612 | E gday@newwestcity.ca

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From: External-Clerks
Sent: Monday, October 18, 2021 3:43 PM
To: Personal Information Removed
Subject: RE: [EXTERNAL] HRA 323 Regina Street

Good afternoon,

I am writing to confirm receipt of your email. It has been forwarded to Mayor Cote and members of Council, the Chief Administrative Officer and the Director of Climate Action, Planning and Development.

Please note that if a member of Council raises this matter at a meeting, your email may be included in the agenda package that is posted to the City's website. Prior to posting, your email address, house number and phone number will be redacted.

Yours truly,

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

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From: Personal Information Removed
Sent: Monday, October 18, 2021 2:06 PM
To: qpra.newwest@gmail.com; External-Clerks <Clerks@newwestcity.ca>
Cc: Personal Information Removed
Subject: [EXTERNAL] HRA 323 Regina Street

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To the President of the QPRA, Mayor Cote and members of Council.

I writing in support of the proposed HRA project at 323 Regina Street.

Here are my concerns about the QPRA's one sided, opposition to this project.

1. **The Queens Park Residents Association is not a residents association.** While it is called a "residents association" it more properly should be termed an HOA (Home Owners Association) or a POA (Property Owners Association). Since no effort is made to include groups such as renters, people of colour, religious groups and younger age groups - by the ongoing nature of its activities, the current QPRA is a HOA/POA that does not operate as a legitimate residents association.
2. **The QPRA is not a professional Heritage Association.** While the residents of Queens Park are heritage enthusiasts, their expertise in heritage is limited and often diverges from the professional heritage community. Much the same as the pandemic, we all talk about vaccines but it is the experts in that area that society relies on for advice. If the city and other heritage experts believe the project has merit and the house meets heritage standards; then it does. To act upon opinions by the QPRA undermines the legitimacy of the heritage process and professionals.
3. **Climate change is a real challenge for the world and cities are a primary source of emissions.** The gentle densification of inner city single family neighbourhoods such as Queens Park will make a significant difference due to their ready access to transit; the existing infrastructure already available and the fact more families can live closer to work and other amenities. It is a concern to citizens to see densification projects in QP turned down time after time.
4. **The Housing Crisis is another real challenge.** Twenty percent of people in Vancouver live on 80% of the land due to single family neighbourhoods. At the present time, Queens Park is unaffordable to upcoming generations. Subdivision, stratification, secondary suites, infills and laneways will help to maintain the look and feel of our heritage community if we chose to embrace it, not stick our collective heads in the sand and hope somehow the housing crisis goes away. This is NIMBYism.
5. **Finally, why does the QPRA focus so much about laneway houses and infills?** Garages with identical massing and footprints are going up all the time in Queens Park, yet their construction passes without notice. Why is it the QPRA clutches their pearls over this project? Shouldn't any building built in a backyard be subject to the same scrutiny? My guess is QPRA's true motivation is not about the so called 'heritage deficiencies' of the 323 Regina Street Project but the fact that the house is a laneway.

Thank you,
Personal Information Removed

Kathleen Stevens

From: Gillian Day
Sent: Tuesday, October 19, 2021 8:54 AM
To: Jonathan Cote; Lisa Spitale; Emilie Adin; Jackie Teed; Kathleen Stevens; Chinu Das; Chuck Puchmayr; Chuck Puchmayr (Shaw); Jaimie McEvoy; Jaimie McEvoy (2); Mary Trentadue; Nadine Nakagawa; Patrick Johnstone
Cc: Jacqueline Killawee; Sophie Schreder; Angela Danielisz; Kathryn Beardsley
Subject: FW: [EXTERNAL] Yes to new Laneway Proposal at 323 Regina St.

Forwarded for information.

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

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From: External-Clerks
Sent: Tuesday, October 19, 2021 8:53 AM
To: 'Jon Holisko' Personal Information Removed
Subject: RE: [EXTERNAL] Yes to new Laneway Proposal at 323 Regina St.

Good morning

I am writing to confirm receipt of your email. It has been forwarded to Mayor Cote and members of Council, the Chief Administrative Officer and the Director of Climate Action, Planning and Development.

Please note that if a member of Council raises this matter at a meeting, your email may be included in the agenda package that is posted to the City's website. Prior to posting, your email address will be redacted.

Yours truly,

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From: Jon Holisko Personal Information Removed
Sent: Tuesday, October 19, 2021 8:28 AM
To: gpra.newwest@gmail.com

Cc: External-Clerks <Clerks@newwestcity.ca>

Subject: [EXTERNAL] Yes to new Laneway Proposal at 323 Regina St.

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Dear Mayor and Councilors,

I'm the youngest son of ^{Personal Information Removed}, and I am in favour of the proposed laneway home at 323 Regina.

I support the proposal for the following reasons:

I believe the housing crisis we are currently in requires that we need more affordable housing for young families, and we need to be more open to ways of curbing this crisis. Currently it's next to impossible for anyone around my age to buy property in the lower mainland. At this point in our lives, we've outgrown our 500 square foot apartment in Surrey.

My parents have done an incredible job of restoring the Edgar House. Stand outside the house for 20 minutes on a nice summer day and see the amount of compliments it gets from passers-by. I've heard nothing but positivity about the restoration - even from the very same people who are now claiming it doesn't fit the storybook character - which raises concerns. My parents didn't have to restore this house - It was not protected. They chose to spend their money to restore it. It was not cheap. They've more than proven their commitment to heritage conservation and to the historic character of Queens Park.

The laneway house will not have a visible basement, so I'm not sure I understand why there is opposition to it. It makes 0 difference from outside of the house - you can't see the basement. No one would even know a basement is there.

My fiancé and I currently live in a 500 sq ft apartment in Surrey. We've been here for 4 years and as much as we love our neighborhood we have grown out of our space and we'd like to start a family. All we are asking for is a basement for our home, to provide extra space for work since we both will be working from home often. 958 sq ft just doesn't seem big enough to raise an entire family in, and again, the basement would not be seen from outside anyways.

There is a small but loud minority of Queens Park residents who fear change and fear that we will tear down their neighborhood. On the contrary, nothing has been torn down, and nothing will be. We are adding to the neighborhood: diversity, family values, community, and we are indeed keeping the character of Queens Park with the design of the laneway house.

I ask that should this proposal come to a public hearing, you vote yes to it.

Sincerely,

Jonathan Holisko

Kathleen Stevens

From: Gillian Day
Sent: Tuesday, October 19, 2021 4:08 PM
To: Jonathan Cote; Lisa Spitale; Emilie Adin; Jackie Teed; Kathleen Stevens; Chinu Das; Chuck Puchmayr; Chuck Puchmayr (Shaw); Jaimie McEvoy; Jaimie McEvoy (2); Mary Trentadue; Nadine Nakagawa; Patrick Johnstone
Cc: Jacqueline Killawee; Sophie Schreder; Angela Danielisz; Kathryn Beardsley
Subject: FW: Support for Infill Proposal at 323 Regina Street

Forwarded for information.

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

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From: External-Clerks
Sent: Tuesday, October 19, 2021 4:07 PM
To: 'Liz Iseli' Personal Information Removed
Subject: RE: Support for Infill Proposal at 323 Regina Street

Good afternoon,

I am writing to confirm receipt of your email. It has been forwarded to Mayor Cote and members of Council, the Chief Administrative Officer and the Director of Climate Action, Planning and Development.

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Yours truly,

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From: Liz Iseli Personal Information Removed
Sent: Tuesday, October 19, 2021 11:33 AM

To: gpra.newwest@gmail.com

Cc: External-Clerks <Clerks@newwestcity.ca>

Subject: [EXTERNAL] Support for Infill Proposal at 323 Regina Street

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To Whom It May Concern,

I reside at Regina Street in Quees Park. I support the proposal for the infill planned for 323 Regina Street. I believe the design and plan of the infill house is in keeping with the neighbourhood style. It's clear that making the home visually appealing was important to the homeowner and taken into account by the architect is coming up with the design. The relaxations requested are not at all unreasonable and in fact make a lot of sense, allowing a small amount of additional space for a family.

Sincerely,
Liz Iseli

Kathleen Stevens

From: Gillian Day
Sent: Tuesday, October 19, 2021 8:49 AM
To: Emilie Adin; Jackie Teed; Kathleen Stevens
Cc: Jacqueline Killawee; Kathryn Beardsley
Subject: FW: General Meeting - HRA 323 Regina St discussion/poll

Forwarded for information.

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T 604.527.4612 | E gday@newwestcity.ca

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From: External-Clerks
Sent: Tuesday, October 19, 2021 8:49 AM
To: Personal Information Removed
Subject: RE: General Meeting - HRA 323 Regina St discussion/poll

Good morning,

I am writing to confirm receipt of your email. It has been forwarded to the Climate Action, Planning and Development Department.

Yours truly,

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

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From: Personal Information Removed
Sent: Monday, October 18, 2021 8:13 PM
To: gpra.newwest@gmail.com
Cc: External-Clerks <Clerks@newwestcity.ca>; Personal Information Removed
Subject: [EXTERNAL] General Meeting - HRA 323 Regina St discussion/poll

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Good Evening,

I live at Regina Street, New Westminster and I would like to register our support of the proposal for infill housing at 323 Regina Street. The design of the in-fill house is tasteful unlike other approved projects I have witnessed in the city. We also all know there is a need for additional housing in New Westminster not to mention adding another home to the protected HRA. Sounds like a win-win to me.

Thanks,

Personal Information Removed

Kathleen Stevens

From: Gillian Day
Sent: Tuesday, October 19, 2021 8:57 AM
To: Jonathan Cote; Lisa Spitale; Emilie Adin; Jackie Teed; Kathleen Stevens; Chinu Das; Chuck Puchmayr; Chuck Puchmayr (Shaw); Jaimie McEvoy; Jaimie McEvoy (2); Mary Trentadue; Nadine Nakagawa; Patrick Johnstone
Cc: Jacqueline Killawee; Sophie Schreder; Angela Danielisz; Kathryn Beardsley
Subject: FW: [EXTERNAL] HRA 323 Regina Street

Forwarded for information.

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From: External-Clerks
Sent: Tuesday, October 19, 2021 8:57 AM
To: 'Maxine Llewellyn' Personal Information Removed
Subject: RE: [EXTERNAL] HRA 323 Regina Street

Good morning

I am writing to confirm receipt of your email. It has been forwarded to Mayor Cote and members of Council, the Chief Administrative Officer and the Director of Climate Action, Planning and Development.

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Yours truly,

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From: Maxine Llewellyn Personal Information Removed
Sent: Tuesday, October 19, 2021 8:32 AM
To: External-Clerks <Clerks@newwestcity.ca>

Cc: gpra.newwest@gmail.com

Subject: [EXTERNAL] HRA 323 Regina Street

CAUTION: This email originated from outside of the City of New Westminster's network. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mayor and City Councillors,

My name is Maxine Llewellyn. I am the fiancé of Jonathan Holisko, the son of ^{Personal Information Removed} I'm writing in support of the proposed HRA application for 323 Regina St, which would provide heritage protection for the historic Edgar House, as well as allow for an infill home to be built for Jonathan and myself to raise a family.

Queens Park is one of the oldest, most well preserved heritage neighborhoods in Canada. It's current and former residents have done a wonderful job of conserving and maintaining the neighborhood and that should be commended. However there is a need for affordable housing and I feel the conversation surrounding this issue tends to be quite contentious, and often becomes secondary to Heritage preservation.

I support this application for the following reasons. Queens Park has very low density, most of the neighborhood is comprised of single family detached homes. It's no secret that many young people are not in a position to purchase these types of homes, even getting into a condo is becoming a challenge for many. The proposed infill house adds modest density, while providing affordable housing for Jonathan and myself to start a family of our own, the design of the home is respectful and complimentary to the overall existing aesthetic of the neighborhood.

^{Personal Information Removed} are very passionate about heritage conservation, Edgar house is the second home they've restored since moving to the neighborhood in 2014. Although there have been some minor alterations, they've taken great care and great pains to be respectful of the historical design aspects right down to finishing the home in it's original exterior color scheme. I don't feel the restoration has caused the home to lose any character, and the minor alterations do not take away from the Storybook design. It would be a heritage win having this unique home protected for future generations.

It's not lost on us that it's a huge privilege to live in a neighborhood such as Queens Park, and we are beyond thankful for the opportunity. We appreciate you taking the time to review and consider our proposal, we hope that we have your support.

Kindest Regards,


Maxine Llewellyn

Kathleen Stevens

From: Gillian Day
Sent: Friday, October 22, 2021 11:43 AM
To: Emilie Adin; Jackie Teed; Kathleen Stevens
Cc: Jacqueline Killawee; Kathryn Beardsley
Subject: FW: [EXTERNAL] HRA for 323 Regina Street

Forwarded for information.

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

 City of New Westminster | Legislative Services
511 Royal Avenue, New Westminster, BC V3L 1H9
www.newwestcity.ca

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From: External-Clerks
Sent: Friday, October 22, 2021 11:41 AM
To: Personal Information Removed
Subject: RE: [EXTERNAL] HRA for 323 Regina Street

Good morning,

I am writing to confirm receipt of your email. It has been forwarded to the Climate Action, Planning and Development department.

Yours truly,

Gillian Day (she/her) | Agenda Secretary
T 604.527.4612 | E gday@newwestcity.ca

 City of New Westminster | Legislative Services
511 Royal Avenue, New Westminster, BC V3L 1H9
www.newwestcity.ca

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From: Personal Information Removed
Sent: Friday, October 22, 2021 11:02 AM
To: gpra.newwest@gmail.com
Cc: External-Clerks <Clerks@newwestcity.ca>
Subject: [EXTERNAL] HRA for 323 Regina Street

Dear fellow members of QPRA:

We are writing in support of the proposal for infill housing at 323 Regina Street. New Westminster has a housing affordability problem and this is a positive solution at a local level. This home is for the Personal Information Removed . We think this is a perfect example of where an HRA designation of a rare storybook style home provides a win for the City by preserving this house; while providing needed affordable housing for a young family in our city. The design of the in-fill house is attractive and consistent with the traditional housing stock in Queens Park.

Yours faithfully,

Personal Information Removed

Kathleen Stevens

From: Personal Information Removed
Sent: Monday, October 25, 2021 8:42 AM
To: Kathleen Stevens
Subject: [EXTERNAL] Support for 323 Regina Street HRA

CAUTION: This email originated from outside of the City of New Westminster's network. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Kathleen and Council,

As a former member of the Queens Park Heritage Study Working Group and Queens Park Heritage Control Period Desing committee, I am writing to support the HRA for 323 Regina Street enthusiastically.

First, as a later build than many of the homes in Queens Park, protecting the house would ensure the exemplary diversity of housing that exists in Queens park. Queens park is not a neighbourhood of Victorian and Edwardian homes. On the contrary, it is a neighbourhood filled with brilliant examples of housing from every decade. 323 is one of those and has been renovated and maintained to ensure it will contribute to the area for decades to come.

Second, the working group supported a Heritage Conservation Area for Queens Park because it was the best way to provide opportunities for sensitive infill and maintain the beloved structures in the neighbourhood. The HCA was recommended because it gave the city the most flexible control to support homeowners' current love of heritage and encourage increasingly diverse and densified housing in the future. It was created for precisely the kinds of projects proposed at 323 Regina.

Third, as someone who has been forced out of Queens Park because of the Lower Mainland's housing crisis, secondary units like the one proposed at 323 Regina are the only way I might be able to return to the neighbourhood. I would love to live in such a structure. We need more to be built, and the HRA tool is one of the ways to push back against the loud voices often heard in the Queens Park matters. It is a trade-off that supports the ideas of the HCA and supports next-generation housing needs.

The massing of the secondary structure is sensitive to the surrounding buildings and meets the design requirements for the HCA. It checks all the boxes. It also offers a missing middle diversity of housing that contributes rather than detracts from the area. Once it is built and landscaping matures, it will be like many of the diverse little lanes in the neighbourhood.

HRA's are a tool to protect examples of particular architecture and should be seen as another tool in the toolbox for addressing the housing crisis. The City of New Westminster should support 323 Regina and similar projects to contribute sensitive infill and support a unique structure.

I hope to see the HRA for 323 Regina Street approved.
Thank you

Personal Information Removed

From: [Carilyn Cook](#)
To: [Mayor & Councillors](#); [Lisa Spitale](#); [Emilie Adin](#); [Jackie Teed](#); [Kathleen Stevens](#)
Cc: [Kathryn Beardsley](#); [Jacqueline Killawee](#); [Sophie Schreder](#); [Angela Danielisz](#)
Subject: FW: [EXTERNAL] Re: HRA 323 Regina Street
Date: Thursday, October 28, 2021 2:51:30 PM
Attachments: [image001.png](#)

Forwarded for information.

Carilyn Cook (she/her) | Committee Clerk
T 604.515.3782 | E ccook@newwestcity.ca
🏛️ City of New Westminster | Legislative Services
511 Royal Avenue, New Westminster, BC V3L 1H9
www.newwestcity.ca

In Office: Mondays/Tuesdays & alternate Wednesdays
Remote: Alternate Wednesdays & Thursdays/Fridays

From: External-Clerks
Sent: Thursday, October 28, 2021 2:49 PM
To: 'Queens Park Residents' Association' <qpra.newwest@gmail.com>
Subject: RE: [EXTERNAL] Re: HRA 323 Regina Street

Good afternoon,

I am writing to confirm receipt of your email. It has been forwarded to Mayor Côté, members of Council, the Chief Administrative Officer, the Director of Climate Action, Planning and Development, and the Senior Manager of Climate Action, Planning and Development.

Please note: if a Council member raises this matter at an open meeting, your email will be added to the agenda and posted to the City's website as part of the agenda package. Prior to posting, your contact and identifying information will be redacted.

Yours truly,

Carilyn

Carilyn Cook (she/her) | Committee Clerk
T 604.515.3782 | E ccook@newwestcity.ca
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511 Royal Avenue, New Westminster, BC V3L 1H9
www.newwestcity.ca

In Office: Mondays/Tuesdays & alternate Wednesdays
Remote: Alternate Wednesdays & Thursdays/Fridays

From: Queens Park Residents' Association <qpra.newwest@gmail.com>
Sent: Tuesday, October 26, 2021 7:10 PM
To: Personal Information Removed

Cc: External-Clerks <Clerks@newwestcity.ca>; Personal Information Removed

Subject: [EXTERNAL] Re: HRA 323 Regina Street

CAUTION: This email originated from outside of the City of New Westminster's network. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Personal Information Removed

I have no desire to get into a war of words, but I feel compelled to provide you with some clarification on some of the assumptions made in your recent email to me along with Mayor and Council.

I do take exception to you saying the QPRA does not operate as a legitimate residents association. Yes, most of our directors are home owners but, as you are aware, that is primarily who lives in Queen's Park and therefore who should be represented. The requirements to be on the board are only two: (1) live in Queen's Park, and (2) care about Queen's Park. As you can appreciate, finding younger folk to be executive members is difficult as there is a time commitment (both in reading materials and attending monthly meetings) and they find themselves having to split their valuable volunteer time between activities involving their children and other community participation. We are fortunate that, along with us old retired people on the board, we are happy to have a Personal Information Removed as a director. In addition, we are grateful to add an apartment renter to the mix who will bring a different perspective to issues. We do not ask any questions about, nor actively recruit new members based on colour, ethnicity, or religion but welcome whoever wishes to take on a volunteer role.

The QPRA does not claim to be a professional heritage association. We have many knowledgeable members who know a great deal about history and heritage and we do draw on them for information on a regular basis. But to be clear, with respect to the project you are referencing, it was the City's heritage expert who deemed the house to have insufficient heritage merit to be included in the HCA and it was heritage enthusiasts who were encouraging the owners to go through the process to have their house put back in.

I think you will find that the majority of Queen's Park residents support gentle densification but there is some disparity in how that translates. Adding a house to every piece of green space would not meet the litmus test for 'gentle', nor having them overbuilt in size hardly qualifies as gentle. No amount of subdivision, stratification, or lot splitting will render anything that is 'affordable' and it is not rational or logical to think that it will. Densification does not equate to affordability. A quick example is the property directly across from the HRA in question where the developer wanted to change the property from a single family home to three dwellings. Each of these would have been priced over \$1M so the neighbourhood would have ended up with maximum densification on one lot and three more market priced houses - nothing affordable there. Queen's Park as a location is not 'affordable' in and of itself and that is not an issue created by the residents who live there. When Burnaby got too expensive, people moved to New Westminster to buy because you could still find cheaper properties and when those climbed to market pricing, new buyers then went to Coquitlam, Maple Ridge and even Mission. Jamming in as many houses as possible in Queen's Park or ANY area does not bring down the price and has never been the case.

Lastly, to your point on garages going up everywhere so why does the QPRA care so much about laneways. Garage construction is strictly regulated and ‘passes without notice’ for just that reason. There are no relaxations to be bartered for and no there is skirting the guidelines for what is allowable. We only wish laneways and infills had the same regulations and scrutiny that garages do. If the current application was for a laneway as laid out under the OCP, rather than trying to use an HRA to gain an oversized infill, there would be no push back and there would be the prospect of a more modest rental property that met the ‘gentle’ densification description.

Regards,
Gail North
President, QPRA

On Mon, Oct 18, 2021 at 2:05 PM Personal Information Removed

wrote:

To the President of the QPRA, Mayor Cote and members of Council.

I writing in support of the proposed HRA project at 323 Regina Street.

Here are my concerns about the QPRA’s one sided, opposition to this project.

1. The Queens Park Residents Association is not a residents association. While it is called a “residents association” it more properly should be termed an HOA (Home Owners Association) or a POA (Property Owners Association). Since no effort is made to include groups such as renters, people of colour, religious groups and younger age groups - by the ongoing nature of its activities, the current QPRA is a HOA/POA that does not operate as a legitimate residents association.

2. The QPRA is not a professional Heritage Association. While the residents of Queens Park are heritage enthusiasts, their expertise in heritage is limited and often diverges from the professional heritage community. Much the same as the pandemic, we all talk about vaccines but it is the experts in that area that society relies on for advice. If the city and other heritage experts believe the project has merit and the house meets heritage standards; then it does. To act upon opinions by the QPRA undermines the legitimacy of the heritage process and professionals.

3. Climate change is a real challenge for the world and cities are a primary source of emissions. The gentle densification of inner city single family neighbourhoods such as Queens Park will make a significant difference due to their ready access to transit; the existing infrastructure already available and the fact more families can live closer to work and other amenities. It is a concern to citizens to see densification projects in QP turned down time after time.

4. The Housing Crisis is another real challenge. Twenty percent of people in Vancouver live on 80% of the land due to single family neighbourhoods. At the present time, Queens Park is unaffordable to upcoming generations. Subdivision, stratification, secondary suites, infills and laneways will help to maintain the look and feel of our heritage community if we chose to embrace it, not stick our collective heads in the sand and hope somehow the housing crisis goes away. This is NIMBYism.

5. Finally, why does the QPRA focus so much about laneway houses and infills?

Garages with identical massing and footprints are going up all the time in Queens Park, yet their construction passes without notice. Why is it the QPRA clutches their pearls over this project? Shouldn't any building built in a backyard be subject to the same scrutiny? My guess is QPRA's true motivation is not about the so called 'heritage deficiencies' of the 323 Regina Street Project but the fact that the house is a laneway.

Thank you,

Personal Information Removed

From: [Carilyn Cook](#)
To: [Mayor & Councillors](#); [Lisa Spitale](#); [Emilie Adin](#); [Jackie Teed](#); [Kathleen Stevens](#)
Cc: [Kathryn Beardsley](#); [Jacqueline Killawee](#); [Sophie Schreder](#); [Angela Danielisz](#)
Subject: FW: [EXTERNAL] Letter of Support for HRA at 323 Regina Street
Date: Thursday, October 28, 2021 2:58:39 PM
Attachments: [image001.png](#)

Forwarded for information.

Carilyn Cook (she/her) | Committee Clerk
T 604.515.3782 | E ccook@newwestcity.ca
🏛️ City of New Westminster | Legislative Services
511 Royal Avenue, New Westminster, BC V3L 1H9
www.newwestcity.ca

In Office: Mondays/Tuesdays & alternate Wednesdays
Remote: Alternate Wednesdays & Thursdays/Fridays

From: External-Clerks
Sent: Thursday, October 28, 2021 2:57 PM
To: Personal Information Removed
Subject: RE: [EXTERNAL] Letter of Support for HRA at 323 Regina Street

Good afternoon,

I am writing to confirm receipt of your email. It has been forwarded to Mayor Coté, members of Council, the Chief Administrative Officer, the Director of Climate Action, Planning and Development, and the Senior Manager of Climate Action, Planning and Development.

Please note: if a Council member raises this matter at an open meeting, your email will be added to the agenda and posted to the City's website as part of the agenda package. Prior to posting, your contact and identifying information will be redacted.

Yours truly,

Carilyn

Carilyn Cook (she/her) | Committee Clerk
T 604.515.3782 | E ccook@newwestcity.ca
🏛️ City of New Westminster | Legislative Services
511 Royal Avenue, New Westminster, BC V3L 1H9
www.newwestcity.ca

In Office: Mondays/Tuesdays & alternate Wednesdays
Remote: Alternate Wednesdays & Thursdays/Fridays

From: Personal Information Removed
Sent: Wednesday, October 27, 2021 5:59 PM

To: External-Clerks <Clerks@newwestcity.ca>

Cc: Britney Dack <bdack@newwestcity.ca>

Subject: [EXTERNAL] Letter of Support for HRA at 323 Regina Street

CAUTION: This email originated from outside of the City of New Westminster's network. Do not click links or open attachments unless you recognize the sender and know the content is safe.

Dear Mayor & Council,

I write in support of the above captioned HRA and make the following points:

- I find the additional FSR requested to be reasonable, particularly as a significant portion of the laneway home's FSR will be below grade.
- I find the proposed heritage designation of the existing home to be laudable and significant
- The "Storybook" style that this home represents were common on the west side of Vancouver, where they are being demolished in large numbers and as such I think it's a good thing that this one is being saved.
- I do not see anything controversial in this application and urge Council to support the proponent's application.

It seems the debate around this project has regrettably become a flashpoint for the larger community discussion around density and affordability. Much of the negative commentary I've heard about this HRA relates to perceived density trends and not the project specifics.

On the broader policy discussion, my view is that heritage policy and densification planning are not, or at least should not, be in competition with each other. However, there is a growing *public perception* that heritage is somehow standing in the way of density. Heritage and densification can be *complimentary*.

As a general policy direction, and in particular as the HRA guidelines are being revisited, I think New Westminster has an opportunity to differentiate itself from the Metro region by pursuing as strategy of *density through the window of heritage*.

New Westminster cannot ignore the pressure of a growing population, but it is also unwise, in my opinion, to put the City's unique heritage identity in jeopardy. The City has distinguished itself as a national leader in heritage preservation, and losing that reputation risks becoming further subsumed (and ultimately lost) under the regional identity. Heritage is New Westminster's sustainable competitive advantage in the region. It's a pathway to remaining relevant in a region dominated by Vancouver and its identity as the most beautiful city in the world.

Policies that link density and *relative* affordability to heritage retention should be explored. There is an opportunity for a uniquely "made in new West" solution to densification of single family neighbourhoods.

I am advocating for this kind of approach in the community, including with the QPRA. I do not understand the ferocity of the opposition to this project or others with marginal FSR

variances.

So far I'm seen to be somewhat of a heretic to many within the QPRA & the NWHPS. I keep telling them "this is the way."

I think they'll come around....

Thank you for your time and attention.

Sincerely,

Personal Information Removed

From: [Carilyn Cook](#)
To: [Mayor & Councillors](#); [Lisa Spitale](#); [Emilie Adin](#); [Jackie Teed](#); [Kathleen Stevens](#)
Cc: [Kathryn Beardsley](#); [Jacqueline Killawee](#); [Sophie Schreder](#); [Angela Danielisz](#)
Subject: FW: [EXTERNAL] Re: HRA 323 Regina Street
Date: Thursday, October 28, 2021 2:54:46 PM
Attachments: [image001.png](#)

Forwarded for information.

Carilyn Cook (she/her) | Committee Clerk
T 604.515.3782 | E ccook@newwestcity.ca
🏛 City of New Westminster | Legislative Services
511 Royal Avenue, New Westminster, BC V3L 1H9
www.newwestcity.ca

In Office: Mondays/Tuesdays & alternate Wednesdays
Remote: Alternate Wednesdays & Thursdays/Fridays

From: External-Clerks
Sent: Thursday, October 28, 2021 2:53 PM
To: Personal Information Removed
Subject: RE: [EXTERNAL] Re: HRA 323 Regina Street

Good afternoon,

I am writing to confirm receipt of your email. It has been forwarded to Mayor Côté, members of Council, the Chief Administrative Officer, the Director of Climate Action, Planning and Development, and the Senior Manager of Climate Action, Planning and Development.

Please note: if a Council member raises this matter at an open meeting, your email will be added to the agenda and posted to the City's website as part of the agenda package. Prior to posting, your contact and identifying information will be redacted.

Yours truly,

Carilyn

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🏛 City of New Westminster | Legislative Services
511 Royal Avenue, New Westminster, BC V3L 1H9
www.newwestcity.ca

In Office: Mondays/Tuesdays & alternate Wednesdays
Remote: Alternate Wednesdays & Thursdays/Fridays

From: Personal Information Removed
Sent: Wednesday, October 27, 2021 12:00 AM
To: Queens Park Residents' Association <qpra.newwest@gmail.com>

Cc: External-Clerks <Clerks@newwestcity.ca>; Personal Information Removed

Subject: [EXTERNAL] Re: HRA 323 Regina Street

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Ms. North, Mayor Cote, Council and Personal Information Removed

The federal government for New Zealand just passed legislation to overturn single family zoning throughout the country. Now New Zealanders can develop up to 50 percent of their land and build up to three storeys - without requiring consent from municipal authorities. The reforms also unleash landowners to build up to three homes per lot in areas previously restricted to one or two homes. (<https://nationalpost.com/news/canada/could-new-zealands-radical-new-housing-law-help-canada-curb-its-skyrocketing-real-estate-prices>).

Why is New Zealand doing this? They are doing this for the climate crisis and the housing crisis. There's nothing governments can do to help those two issues more than densification. It just makes sense everywhere and New Westminster is no exception.

What you describe as gentle isn't densification at all; it is dead stop opposition to anything but support for single family properties as well as more and more and more garages. Despite your very vigorous attempts to assert that garages are held to a certain standard; secondary housing is held to a much higher standard in this city. You believe building a garage is hard? Well sister, you aint seen nothing yet until you build a laneway. It is time for your association to fact check your many claims and talk about these issues honestly.

Ms North, this is not just happening in New Zealand - this is also happening in Europe, California and yes, Vancouver. What is my beef with the Queens Park Residents Association? It is that the stated purpose of the QPRA is (straight from your website) *"The purpose of the Queens Park Residents' Association (the "QPRA") is to bring neighbours together to keep the Queens Park neighbourhood a green, safe, beautiful, historic, livable, single family neighbourhood."* You state that you are promoting a single family neighbourhood. There is nothing about secondary suites, laneways, carriage houses or infills. There is no reference to gentle densification.

Forty four percent of New West residents are renters. We own a house in Queens Park. Our laneway house is housing for Personal Information Removed. Our secondary suite of 940 sf has a family of 3. So of the 9 people who live on this property, 7 of them are renters and only two are home owners. I don't know the exact number of renters in Queens Park, but I bet it approaches the same percentage as the rest of the city. After all, many of the large houses are home to only one or two people. Their families have grown up years ago and moved away. I have nothing against these older owners (I am one of them); but they must start to realize that this kind of privileged existence in inner cities means their children and grandchildren can't buy a house or begin to solve the very serious problem of climate change.

While you state that renters can join the QPRA; the QPRA's past track record shows that very few ever do. Why is this large group under represented? Its because of your stated purpose is to promote single family homes. Have you ever addressed the issues of renters? Affordable housing? Don't you think you should? The email that you sent out about the HRA at 323 Regina Street illustrated your bias clearly. While the QPRA email pointed out all the so called deficits of the project; it was completely silent on any of the advantages. You used your position as president of one of the premier, city sponsored, residents associations in New Westminster, to promote a singular political viewpoint. If you want to join a political party to lobby against laneway housing, by all means do so. But that's not the purpose of a residents' association.

We could both go on and on about this subject I'm sure. I'll end it here for now.

Regards

Personal Information
Removed

On Oct 26, 2021, at 7:10 PM, Queens Park Residents' Association
<qp.ra.newwest@gmail.com> wrote:

Personal Information
Removed

I have no desire to get into a war of words, but I feel compelled to provide you with some clarification on some of the assumptions made in your recent email to me along with Mayor and Council.

I do take exception to you saying the QPRA does not operate as a legitimate residents association. Yes, most of our directors are home owners but, as you are aware, that is primarily who lives in Queen's Park and therefore who should be represented. The requirements to be on the board are only two: (1) live in Queen's Park, and (2) care about Queen's Park. As you can appreciate, finding younger folk to be executive members is difficult as there is a time commitment (both in reading materials and attending monthly meetings) and they find themselves having to split their valuable volunteer time between activities involving their children and other community participation. We are fortunate that, along with us old retired people on the board, we are happy to have a Personal Information Removed as a director. In addition, we are grateful to add an apartment renter to the mix who will bring a different perspective to issues. We do not ask any questions about, nor actively recruit new members based on colour, ethnicity, or religion but welcome whoever wishes to take on a volunteer role.

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respect to the project you are referencing, it was the City's heritage expert who deemed the house to have insufficient heritage merit to be included in the HCA and it was heritage enthusiasts who were encouraging the owners to go through the process to have their house put back in.

I think you will find that the majority of Queen's Park residents support gentle densification but there is some disparity in how that translates. Adding a house to every piece of green space would not meet the litmus test for 'gentle', nor having them overbuilt in size hardly qualifies as gentle. No amount of subdivision, stratification, or lot splitting will render anything that is 'affordable' and it is not rational or logical to think that it will. Densification does not equate to affordability. A quick example is the property directly across from the HRA in question where the developer wanted to change the property from a single family home to three dwellings. Each of these would have been priced over \$1M so the neighbourhood would have ended up with maximum densification on one lot and three more market priced houses - nothing affordable there. Queen's Park as a location is not 'affordable' in and of itself and that is not an issue created by the residents who live there. When Burnaby got too expensive, people moved to New Westminster to buy because you could still find cheaper properties and when those climbed to market pricing, new buyers then went to Coquitlam, Maple Ridge and even Mission. Jamming in as many houses as possible in Queen's Park or ANY area does not bring down the price and has never been the case.

Lastly, to your point on garages going up everywhere so why does the QPRA care so much about laneways. Garage construction is strictly regulated and 'passes without notice' for just that reason. There are no relaxations to be bartered for and no there is skirting the guidelines for what is allowable. We only wish laneways and infills had the same regulations and scrutiny that garages do. If the current application was for a laneway as laid out under the OCP, rather than trying to use an HRA to gain an oversized infill, there would be no push back and there would be the prospect of a more modest rental property that met the 'gentle' densification description.

Regards,
Gail North
President, QPRA

On Mon, Oct 18, 2021 at 2:05 PM ^{Personal Information Removed}
wrote:

To the President of the QPRA, Mayor Cote and members of Council.

I writing in support of the proposed HRA project at 323 Regina Street.

Here are my concerns about the QPRA's one sided, opposition to this project.

1. The Queens Park Residents Association is not a residents association.

While it is called a "residents association" it more properly should be termed an HOA (Home Owners Association) or a POA (Property Owners Association). Since no effort is made to include groups such as renters, people of colour,

religious groups and younger age groups - by the ongoing nature of its activities, the current QPRA is a HOA/POA that does not operate as a legitimate residents association.

2. The QPRA is not a professional Heritage Association. While the residents of Queens Park are heritage enthusiasts, their expertise in heritage is limited and often diverges from the professional heritage community. Much the same as the pandemic, we all talk about vaccines but it is the experts in that area that society relies on for advice. If the city and other heritage experts believe the project has merit and the house meets heritage standards; then it does. To act upon opinions by the QPRA undermines the legitimacy of the heritage process and professionals.

3. Climate change is a real challenge for the world and cities are a primary source of emissions. The gentle densification of inner city single family neighbourhoods such as Queens Park will make a significant difference due to their ready access to transit; the existing infrastructure already available and the fact more families can live closer to work and other amenities. It is a concern to citizens to see densification projects in QP turned down time after time.

4. The Housing Crisis is another real challenge. Twenty percent of people in Vancouver live on 80% of the land due to single family neighbourhoods. At the present time, Queens Park is unaffordable to upcoming generations. Subdivision, stratification, secondary suites, infills and laneways will help to maintain the look and feel of our heritage community if we chose to embrace it, not stick our collective heads in the sand and hope somehow the housing crisis goes away. This is NIMBYism.

5. Finally, why does the QPRA focus so much about laneway houses and infills? Garages with identical massing and footprints are going up all the time in Queens Park, yet their construction passes without notice. Why is it the QPRA clutches their pearls over this project? Shouldn't any building built in a backyard be subject to the same scrutiny? My guess is QPRA's true motivation is not about the so called 'heritage deficiencies' of the 323 Regina Street Project but the fact that the house is a laneway.

Thank you,

Personal Information Removed

Attachment 9

Engineering Servicing Memo

Memorandum

To: Kathleen Stevens, Heritage Planning Analyst

Date: December 21, 2021

From: Roger Chang, Engineering Technologist

File: PRJ-009878

Subject: WORKS AND SERVICES REQUIREMENTS FOR 323 REGINA STREET – HER00810

We are responding to the updated Heritage Revitalization Agreement (HRA) application as referenced above dated November 10, 2021 for the proposed renovation of the existing single detached dwelling, construction of new infill detached dwelling, 132 sq. m (1,420 sq. ft.).

Please be advised that staff have completed a final review of this application with the applicant (Gray Holisko). We identified and agreed the following details is to be addressed as part of this application:

1. Discuss all onsite rainwater management and paving materials with Development Services, Planning Division at 604-527-4532. The landscaping design is strongly encouraged to incorporate rainwater retention, infiltration and harvesting including rain gardens, permeable surfaces, rain barrels and swales.
2. Discuss all onsite service details with Development Services, Building Division at 604-527-4580 (Plumbing Permit). The on-site sanitary and stormwater systems, perimeter drainage and roof leaders, will need to be fully separated.
3. Discuss all City communication servicing details with Phil Kotyk, Fiber Network Operations Manager, at 604-527-4641. City communication conduit may be provided in accordance with the City's Intelligent City Design requirements, as it pertains to the Fiber Optic Network and Street Lighting Design.
4. Discuss all costs associated with the design and replacement of the existing overhead electrical and telecommunication utilities with an underground system to service the property. For further information please contact Marc Rutishauser, Acting Manager Electrical Engineering Design and Planning, Electrical Operations Department at 604-527-4533.
5. Discuss all costs associated with telecommunication and gas companies (Shaw, Telus & Fortis BC) directly regarding the provision of their services for the proposed development, noting all works will need to be underground and completed at the owner's expense. We recommend that this consultation be made as soon as possible to establish requirements and avoid conflicts.

Provided the applicant is successful in obtaining a Development Permit, the Engineering Department requirements include but may not necessarily be limited to the following:

6. Payment of a **\$7,500.00** deposit towards the estimated cost of upgrading, by the City, of the existing water service connection with meter setter and Brooks box at property line. City records indicate the existing water service connection is older than 40 years, as per the *City of New Westminster Water Works Bylaw No. 7631, 2013* requires the connection to be replaced. If a fire sprinkler system is proposed for the property, the owner will need to retain a mechanical engineer to determine the appropriate size of the service connection required.

7. Payment of a **\$15,000.00** deposit towards the estimated cost of reconstructing, by the City, of the full lane width with rollover curb and gutter along the development frontage. When you are ready to have the lane reconstructed, please contact Gabe Beliveau, Superintendent Streets, Sidewalks, Signs and Towing at (604) 517-5417.
8. Payment of a **\$4,000.00** deposit towards the estimated cost of reconstructing, by the City, the damage sections of the curb along the development frontage on Fourth Street. When you are ready to have the curb reconstructed, please contact Gabe Beliveau, Superintendent Streets, Sidewalks, Signs and Towing at (604) 517-5417.
9. Apply for a Street Occupancy Permit (SOP) for all works within City street right-of-ways. An approved traffic management plan may be required five (5) business days prior to issuance of a SOP. For more information on the requirements, see our webpage at <https://www.newwestcity.ca/services/streets-and-sidewalks/street-occupancy-permit>.
10. Ensure that the implementation of the Erosion and Sediment control best practices meet the requirements outlined in the *City of New Westminster Erosion and Sediment Control Bylaw 7754, 2016*. For more information on the requirements, see our webpage at <https://www.newwestcity.ca/services/environment-and-sustainability/water-protection-and-conservation#erosion-sediment-control>

Deposits shall be received by the City prior to Building Permit issuance. Should the cost to complete the works be less than the deposits collected, the remaining funds will be returned to you. If the costs exceed the deposits, you will be invoiced the difference.

Should you have any further questions, please contact the undersigned at 604-527-4633 or rchang@newwestcity.ca.

Regards,



Roger Chang, ASCT
Engineering Technologist

REPORT

Climate Action, Planning and Development

To: Mayor Cote and Members of Council **Date:** January 10, 2022

From: Emilie K. Adin, MCIP **File:** REZ00221
Director, Climate Action, Planning and
Development

Item #: 2022-15

Subject: Rezoning Application for Duplex: 122 Eighth Avenue – Preliminary
Report

RECOMMENDATION

THAT Council direct staff to process the rezoning application for a duplex at 122 Eighth Avenue, as outlined in the “Consultation and Review Process” section of this report.

PURPOSE

To seek Council’s approval to process the rezoning application for a duplex at 122 Eighth Avenue as outlined within this report.

EXECUTIVE SUMMARY

A rezoning application has been received to allow construction of a duplex at 122 Eighth Avenue. The proposed side-by-side, ground-oriented residential units would be stratified, and drawings indicate that an overall Floor Space Ratio (FSR) of 0.61 is proposed. Both of the proposed units would be family-friendly, containing three bedrooms. Secondary suites would not be permitted. Two off-street parking spaces are proposed at the rear of the site, accessed via the lane. The proposed rezoning of the property from Single Detached Residential Districts (RS-1) to Comprehensive Development District (CD) to allow for a duplex use is consistent with the subject property’s Official Community Plan (OCP) land use designation.

BACKGROUND

Policy and Regulations

The Official Community Plan (OCP) land use designation for the subject property is Residential – Detached and Semi-Detached Housing, which allows for duplexes. The property is zoned single-detached residential (RS-1) and requires rezoning to allow for a duplex development. The *Duplex, Triplex and Quadruplex: Interim Review Policy* provides policy context for rezoning and design review for a duplex. Although the existing Duplex Districts (RT-1) zone would be used for general reference, a Comprehensive Development District (CD) would be created for this site as per the Interim Review Policy.

As the subject site is located within the Laneway and Carriage Houses Development Permit Area (DPA 1.1), a Development Permit is not required. However, design review of the form and character of the proposed development would be conducted as a condition of the rezoning process. A summary of relevant City policies and regulations is included in Attachment 1.

Site Characteristics and Context

The subject site is located in the Glenbrooke North neighbourhood on Eighth Avenue, near Second Street, in an area of primarily single detached dwellings. The property has an approximate area of 6,956 sq. ft. (646 sq. m.) with a 49.5 ft. (15.1 m.) frontage on Eighth Avenue. The property slopes slightly northeast along Eighth Avenue and towards the rear lane, from which vehicle access would be taken. It is also flanked by a lane and both are undersized. The property currently contains a single detached dwelling.

The property is one block southwest of Royal Square Mall, two blocks west of Glenbrook Middle School and Terry Hughes Park, and is less than 1,310 ft. (400 m.) from Herbert Spencer Elementary School. A site context map and aerial image is provided below:

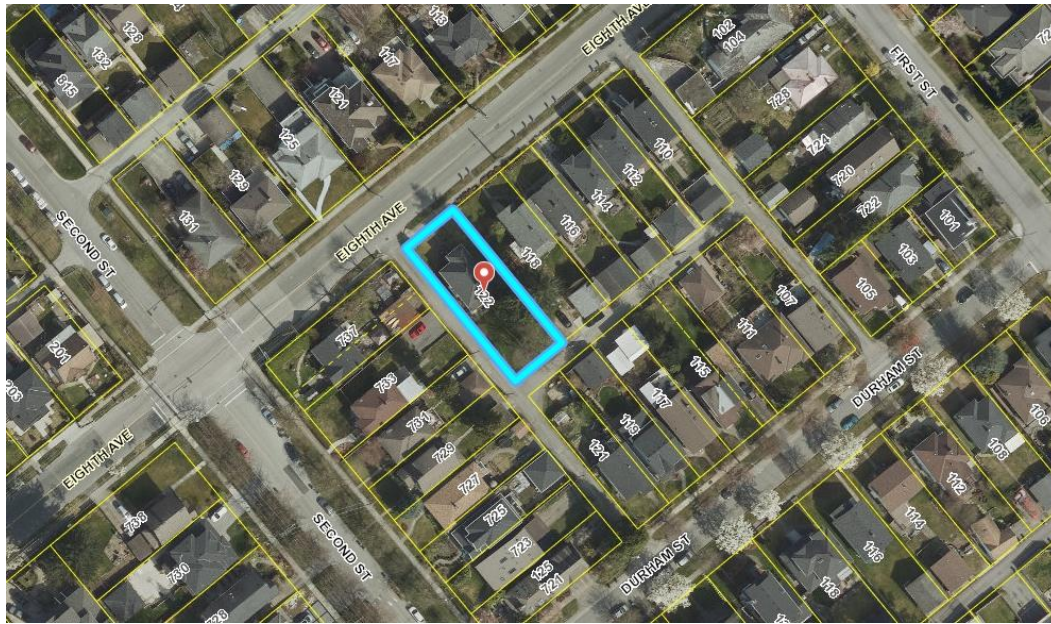


Figure 1: Site Context Map with 122 Eighth Avenue highlighted in blue

Proximity to Transit Service and Other Sustainable Transportation Options

Eighth Avenue is classified as a collector road, while Second Street is a local road. The flanking and rear lanes are undersized at 16 ft. (4.88 m.). The Central Valley Greenway is located approximately two blocks south of the site. The sidewalk network surrounding the site is complete, including an accessible curb letdown at the intersection of Eighth Avenue and the flanking lane and Eighth Avenue and Second Street. Transit service is proximate, as shown on the table below:

Transit Facility	Approx. Frequency	Approx. Distance
#105 / #128	20 minutes	155 ft. (47 m.) to Eighth Ave. and Second St.

Demolition of the Existing Single Detached House

Prior to submission of the rezoning application, the applicant sought approval for the demolition of the existing 1911 house. As per the City's policy for buildings that are 100 years or older, a Heritage Assessment for this house was completed. The Heritage Assessment indicated that the building was in poor condition and that numerous interventions had been made. The demolition permit application was forwarded to the Community Heritage Commission (CHC) on June 2, 2021 and the CHC recommended that staff discuss the option of a Heritage Revitalization Agreement with the property owner, as the original form was distinguishable. The owner has chosen not to pursue this option. The Director of Climate Action, Planning and Development has not yet issued the demolition permit.

PROJECT DESCRIPTION

The proposed development consists of a side-by-side, ground-oriented stratified duplex, consistent with the form anticipated by the *Duplex, Triplex and Quadruplex: Interim Review Policy*. Both units would be oriented to Eighth Avenue. Plans indicate an overall density of 0.61 FSR, with an above-grade FSR of 0.41. Units are proposed to be approximately 2,121 sq. ft. (197 sq. m.) each and family-friendly, containing three bedrooms located on the upper storey. Each unit would have access to outdoor space in the form of ground level open space, rear decks and front verandahs. The duplex would be built to Step 3 of the Energy Step Code, as required by City bylaw.

The applicant has proposed a total of two off-street parking spaces, consistent with Zoning Bylaw requirements for duplex developments. Access would be taken from the rear lane and parking for both units accommodated in carports. Weather-protected, secure bike parking is proposed for both units.

The proposed Eighth Avenue streetscape elevation is provided in Figure 2 below. Design drawings and rationale are included in Attachments 2 and 3. A project statistics table is included in Attachment 4.



Figure 2: Proposed Eighth Avenue streetscape elevation

DISCUSSION

Overall Evaluation

The proposed duplex would help increase opportunities for family-friendly, ground-oriented infill housing, which was identified as a key objective during development of the Official Community Plan. As the proposed project also satisfies the interim requirements

for duplex developments with regard to density, parcel size, design form, parking and access, staff consider this to be an appropriate pilot project for consideration under the *Duplex, Triplex and Quadruplex: Interim Review Policy*.

Interim Policy Guidelines

The Interim Review Policy includes a set of guidelines for the building massing and unit entries. Staff considers the overall density and form to be generally consistent with the neighbourhood context and the design to be generally consistent with the design guidelines. Additional design work is needed, specifically with regard to the building massing, private open space design, and screening of waste bins. As part of the design review process, staff would work with the applicant to address these items based on the guidelines included as per DPA 1.1 (Laneway and Carriage Houses) and 1.3 (Townhouses and Rowhouses).

Lane Width

The Interim Review Policy requires that duplex pilot projects be located on properties that have a minimum 16 ft. (4.88 m.) lane, with some exceptions. The subject site satisfies this requirement. Further review is necessary to determine if a land dedication would be required to facilitate future lane widening to the City's standard lane width of 19.69 ft. (6.0 m.).

Trees

As part of this rezoning application, the applicant has submitted an arborist report in support of a Tree Permit application. The report identifies three on-site trees and two off-site trees. Of these, two are specimen-sized and located on-site. The current design proposes the removal of two on-site trees, due to their poor health and location, and protection of one on-site specimen tree and all off-site trees. Further review of the project's tree retention and replacement strategy would be undertaken by staff during the application review process.

CONSULTATION AND REVIEW PROCESS

The City has formalized the interim development review process. As per this process, the anticipated review steps for this application are:

1. Preliminary report to Council (**WE ARE HERE**);
1. Applicant-led public consultation, including dissemination of information through the local Residents Association;
2. City-led public consultation, including the creation of a Be Heard New West webpage and survey;
3. Council consideration of First and Second Readings of the project's Bylaws and issue notice of waiving Public Hearing;

4. A Public Hearing (if Council does not waive) followed by Council's consideration of Third Reading and Adoption of the rezoning Bylaw.

As there are fewer than six units proposed, and the form of development is consistent with the Official Community Plan, the application would not be forwarded to the New Westminster Design Panel nor the Advisory Planning Commission for review and comment.

INTERDEPARTMENTAL LIAISON

The City has a project-based team approach for reviewing development applications, which facilitates interdepartmental review, providing comments to the applicant throughout the development review process.

OPTIONS

The following options are available for Council's consideration:

1. That Council direct staff to proceed with processing the proposed rezoning at 122 Eighth Avenue, as outlined in the "Consultation and Review Process" section of this report;
2. That Council provide staff with alternative direction.

Staff recommend Option 1.

ATTACHMENTS

Attachment 1 – Policy and Regulations Summary
Attachment 2 – Drawing Package
Attachment 3 – Applicant's Design Rationale
Attachment 4 – Project Statistics Summary

APPROVALS

This report was prepared by:
Wendee Lang, Planning Analyst

This report was reviewed by:
Rupinder Basi, Supervisor of Development Planning
Lynn Roxburgh, Acting Supervisor of Land Use Planning and Climate Action

This report was approved by:
Emilie K. Adin, Director, Climate Action, Planning and Development
Lisa Spitale, Chief Administrative Officer

Attachment 1
Policy and Regulations
Summary

POLICIES AND REGULATIONS SUMMARY

Official Community Plan

The subject property is designated (RD) Residential – Detached and Semi-Detached, which is described, in part, as follows:

Purpose: To allow low density ground oriented residential uses including gentle infill which increases housing choice and retains existing neighbourhood character.

Principal Forms and Uses: Single detached dwellings and duplexes. Single detached dwellings may also include a secondary suite and/or a detached accessory dwelling unit (e.g. laneway house, carriage house).

The proposed development is consistent with the intent of the RD land use designation and therefore, no Official Community Plan (OCP) amendment would be required for this proposed development.

Development Permit Area

The subject site is located within the Laneway and Carriage Houses Development Permit Area (DPA 1.1), and as such, a Development Permit is not required for the proposed development. However, as per the *Duplex, Triplex and Quadruplex: Interim Review Policy*, design review of the form and character of the proposed development would be conducted as a condition of the rezoning process.

Duplex, Triplex and Quadruplex: Interim Development Review Policy

Council has endorsed an interim development review policy for duplex, triplex and quadruplex applications in order to facilitate pilot projects, such as this application. The interim requirements relate to density, parcel size, design form and character, parking, and access. These interim requirements are intended to address that there is limited policy direction currently in place, such as updated duplex zoning regulations.

The Interim Review Policy also identifies that a duplex on an RD designated property cannot include secondary suites, given that the OCP does not contemplate this building form.

Zoning Bylaw

The subject properties are currently zoned Single Detached Residential (RS-1), and would need to be rezoned to support the proposed development. It is understood that the existing Duplex (RT-1) zone would be used for general reference, except where other guidelines have established a new approach. As such, a Comprehensive

Development (CD) zone would be created for this parcel, should the proposed development be supported.

Family-Friendly Housing Policy

The proposed development has fewer than 10 units and therefore is not subject to the Family-Friendly Housing Policy requirements of the City; however, three bedrooms are proposed for each unit, which is in keeping with the Policy.

Attachment 2

Drawing Package

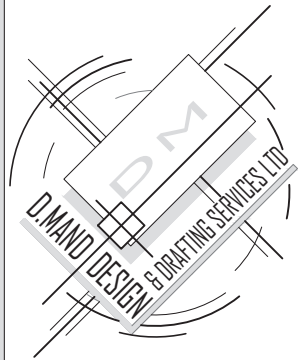
CALCULATIONS :

RT-1 ZONING

LOT SIZE:	6,955 sq.ft.
PROPOSED FLOOR AREAS:	
PRINCIPAL BUILDING:	
MAIN FLOOR :	1,440 sq.ft.
UPPER FLOOR:	1,388 sq.ft.
BASEMENT FLOOR:	1,414 sq.ft.
PROPOSED GROSS FAR :	4,242 sq.ft.
PERMITTED GROSS FAR:	4,242 sq.ft. [61.00%]
(STEP CODE 3)	
ATTACHED ACCESSORY STRUCTURE AREA:	
PERMITTED:	696 sq.ft. [10%]
PROPOSED:	695 sq.ft.
DETACHED ACCESSORY STRUCTURE AREA:	
PERMITTED:	696 sq.ft. [10%]
PROPOSED:	682 sq.ft.
LOT COVERAGE:	
PERMITTED:	2,782 sq.ft. [40.00%]
PROPOSED:	1,440 sq.ft.



SITE PLAN
SCALE: 1/8" = 1'-0"



14658 - 84 AVENUE
SURREY, BC
V3S 9K7
t: 6045971838
f: 6045971350
dmand@telus.net
www.dmanddesign.com

THESE PLANS CONFORM TO BCBC 2018
CONSTRUCTION SHALL COMPLY WITH
THESE PLANS AND LOCAL BUILDING
BY-LAWS.
CONTRACTOR SHALL CONFIRM ALL
DIMENSIONS PRIOR TO START OF
CONSTRUCTION.
THE DESIGNER ASSUMES NO LIABILITY
FOR ANY ERRORS OR OMISSIONS IN
THESE PLANS. IT IS THE BUILDER/OWNER
RESPONSIBILITY TO REVIEW AND VERIFY
ALL LEVELS, DIMENSIONS AND
STRUCTURAL ADEQUACIES PRIOR TO
CONSTRUCTION
DO NOT SCALE DRAWINGS
**BUILDERS NOTES:
THE OWNER/BUYER IS AWARE THAT
DOORS, WINDOWS AND BUILDING ELEVATIONS
MAY VARY DUE TO SITE CONDITIONS.
SQUARE FOOTAGE SHOWN ARE APPROXIMATE
THE BUILDER RESERVES RIGHT TO MAKE
ON GOING CHANGES AND ADJUSTMENTS
FINAL HOME DESIGN AND MEASUREMENTS
MAY NOT MATCH AS PER PLANS
IN ORDER TO COMPLY WITH BYLAWS,
BUILDING CODES AND SITE CONDITIONS:
THESE PLANS ARE SUBJECT TO CHANGE
WITHOUT NOTICE TO CLIENT/BUILDER.

JOB DESCRIPTION:
**PROPOSED DUPLEX
DWELLING**

ADDRESS:
**122 EIGHTH AVENUE
NEW WESTMINSTER**
LEGAL DESCRIPTION:
**LOT E
SUBURBAN BLOCK 13
PLAN 9424**

P.I.D. 011-390-964
CLIENT:
**JAZZ BHATIA
(604) 897-5220
libranz2003@yahoo.com**

DATE:
DEC. 01, 2021
SCALE:
1/4"=1'-0" (UNO)
DRAWN:
D.M. CHECKED:
D.M.



EXISTING HOUSE AT FRONT STREET



EXISTING HOUSE AT SIDE LANE



EXISTING HOUSE AT REAR LANE



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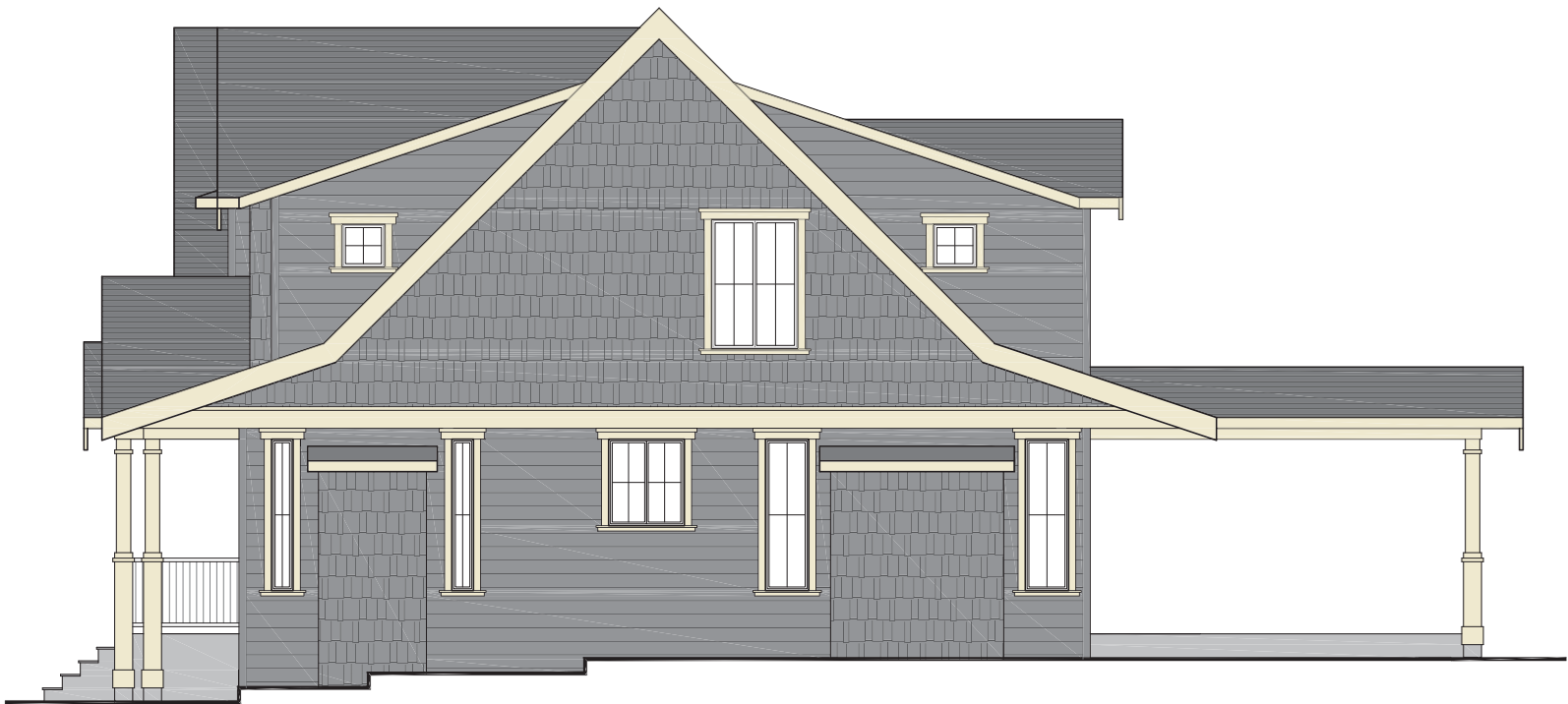
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DATE:
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1/4"=1'-0" (UNO)
DRAWN:
D.M.
CHECKED:
D.M.
5
OF 13



EXTERIOR MATERIAL SCHEDULE			
NO.	DESCRIPTION	MANUFACTURER	ITEM NUMBER
A	ASPHALT SHINGLE	IKO - DYNASTY	GRANITE BLACK
B	LAP SIDING	JAMES HARDIE HARDIEPLANK	BENJAMIN MOORE VC-13 BUTE TAUPE
C	1" X 10" BARGE BOARD		BENJAMIN MOORE VC-1 OXFORD IVORY
D	1" X 6" FASCIA BOARD		BENJAMIN MOORE VC-1 OXFORD IVORY
E	1" X 6" WINDOW/DOOR TRIM		BENJAMIN MOORE VC-1 OXFORD IVORY



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JOB DESCRIPTION:
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DWELLING**

ADDRESS:
**122 EIGHTH AVENUE
NEW WESTMINSTER**

LEGAL DESCRIPTION:
**LOT E
SUBURBAN BLOCK 13
PLAN 9424**

P.I.D. 011-390-964
CLIENT:
**JAZZ BHATIA
(604) 897-5220
libranz2003@yahoo.com**

DATE: DEC. 01, 2021
SCALE: 1/4"=1'-0" (UNO)
DRAWN: D.M. CHECKED: D.M.
6
OF 13

Attachment 3
*Applicant's Design
Rationale*

City of New Westminster
511 Royal Avenue
New Westminster, BC V3L 1H9

December 16, 2021

RE: PROJECT SUMMARY LETTER

122 Eighth Avenue, New Westminster Property is located in the Glenbrooke North neighborhood. The property is walking distance to Terry Hughes park and Queen's park. Both levels of schools are in close vicinity. Shopping is easily accessible.

The current site is sloping up from North to South and neighboring area is moderately landscaped, mostly with large trees in rear yards.

Neighborhood consists of significantly older homes with some newer homes built from the 1990's and onwards. The general architecture is traditional or craftsman.

Proposed is a duplex on a site which has flanking and rear lanes. The front of each proposed unit faces Eighth Avenue. Main indoor and outdoor living space is on the ground level and majority of bedrooms are on the second level. The cellar is for recreational purposes. Proposal also includes Carports for each unit, accessed from the rear lane.

The proposed design compliments the current streetscape, continuing with the traditional craftsman style. Landscaping includes keeping a large tree in the rear yard and introduced planting will enhance the neighborhood.

The proposed project would provide family-friendly, ground-oriented housing, which is an important City goal to provide ground-oriented housing for our community. Ground oriented housing is critical for health, security, and overall well-being and encompasses choice, supply, security, affordability, and suitability. Also pursue creative approaches to housing policy and on-the-ground projects to transform the way housing is provided in New Westminster.

Also Facilitate development of a range of ground-oriented infill housing and provision of other choices in housing under a range of tenures and ownership models.

Also, will explore strategies to reduce homelessness, including developing opportunities for supportive housing.

Warm Regards,

Jass Bhatia
604-897-5220

Attachment 4
Project Statistics
Summary

PROJECT STATISTICS

	Permitted / Required if Under Interim Policy	Proposed
Lot Area	6,000 sq. ft. (557.4 sq. m.) with lane access	6,955.7 sq. ft. (646.2 sq. m.)
Site Frontage	-	49.5 ft. (15.1 m.)
Average Lot Depth	-	140.5 ft. (42.8 m.)
Front Setback	19.0 ft. (5.79 m.)	19.0 ft. (5.79 m.)
Floor Space Ratio	0.61 FSR (Step 3)	0.61 FSR (Step 3)
Above-grade FSR	-	0.41 FSR
Building Height Mid-point	25.0 ft. (7.62 m.)	25.0 ft. (7.62 m.)
Building Height Roof Peak	35.0 ft. (10.67 m.)	29.4 ft. (9.0 m.)
Site Coverage (Principal Building)	35%	30%
Off-Street Parking	2 spaces (1.0 per dwelling unit)	2 spaces (1.0 per dwelling unit)
Bicycle Parking	No requirement	1 space per unit (2 total)

REPORT

Climate Action, Planning and Development

To: Mayor Cote and Members of Council **Date:** January 10, 2022

From: Emilie K. Adin, MCIP **File:** REZ00217
Director of Climate Action, Planning and
Development

Item #: 2022-14

Subject: Rezoning Application for Infill Townhouse: 337 and 339 Keary Street –
Preliminary Report to Council

RECOMMENDATION

THAT Council direct staff to proceed with processing the proposed rezoning at 337-339 Keary Street, as outlined in the “Consultation and Review Process” section of this report.

PURPOSE

To seek Council’s approval to proceed with processing the proposed rezoning at 337 and 339 Keary Street.

EXECUTIVE SUMMARY

Rezoning and development permit applications have been received for 337 and 339 Keary Street. A total of nine side-by-side, ground-oriented residential units are proposed across two buildings, and plans indicate an overall Floor Space Ratio (FSR) of 1.0. All of the proposed units would be family-friendly, containing three bedrooms, and stratified. The development proposes 10 off-street parking spaces in accordance with Zoning Bylaw requirements. Long-term bicycle storage would be provided for each unit, as well as six short-term bicycle spaces, as per Zoning Bylaw requirements.

A Preliminary Application Review for this project was completed on November 12, 2020 and the applicant has incorporated the majority of this feedback into their proposed design.

BACKGROUND

Preliminary Application Review

The applicant previously submitted a Pre-Application Review (PAR) inquiry for this proposal, completed on November 12, 2020. Based on the COVID-19 Interim Development Review Process, the pre-application was not required to be presented to the Land Use and Planning Committee (LUPC). Key items brought forward to the applicant included provision of bike storage for all units, request for additional information on proposed basement areas, a directive to reduce tree canopy loss, and integration of a children's play area.

Policy and Regulations

The Official Community Plan (OCP) land use designation for the subject properties is Residential – Ground Oriented Infill Housing (RGO), which allows townhouses. The properties are zoned Single Detached Residential Districts (RS-1) and require rezoning to allow for townhouse development. While the existing Infill Townhouse (RT) zone will guide the proposed rezoning, a Comprehensive Development (CD) zone would be created for this parcel to accommodate site constraints created by the properties' slope.

The subject site is located within the Ground Oriented Housing Development Permit Area (DPA 1.2) and a Development Permit (DP) is required for the form and character of the proposed development. The proposal would be evaluated against the Infill Townhouse and Rowhouse Development Permit Area guidelines concurrently with the review of the rezoning application. Should Council adopt the proposed rezoning, the Development Permit would be issued by the Director of Climate Action, Planning and Development. The proposal additionally meets the Family Friendly Housing Policy, though this is not a requirement as fewer than 10 units are proposed.

A summary of relevant City policies and regulations is included in Attachment 1.

Site Characteristics and Context

The subject site, which includes two properties, is located in the Sapperton neighbourhood, in an area composed of single-detached dwellings and mid-rise apartment buildings. The lots have an approximate combined area of 17,296 sq. ft. (1,606.9 sq. m.) and slope down Keary Street from west to east with a grade change of approximately 10%. There is no rear lane access to the property. One single-detached dwelling is currently situated on each lot.

The existing single detached properties to the east and west of the subject site are also designated (RGO) Residential – Ground Oriented Infill Housing. The existing mid-rise apartment building abutting the site to the north is designated (RM) Residential – Multiple Unit Buildings. The site is located approximately one block west of Royal

Columbian Hospital and the surrounding Special Employment Area, one block south of Sapperton Park, and one block east of Skwo:wech Elementary School. A site context map and aerial image is provided below:



Figure 1: Site Context Map with 337 and 339 Keary Street highlighted in blue

Proximity to Transit Service and Other Sustainable Transportation Options

Keary Street is classified as a local road and serves as a portion of the Crosstown Greenway, while nearby E. Columbia Street is a collector road and a designated Great Street. The sidewalk network surrounding the site is complete, including an accessible curb letdown at the intersection of Keary Street and E. Columbia. The site is well-served by transit, as shown on the table below:

Table 1: Site Proximity to Transit Service

Transit Facility	Approx. Frequency	Approx. Distance
#109	30 minutes	575 ft. (175 m.) to E. Columbia Street at Keary Street
#155	20 minutes	400 ft. (122 m.) to Keary Street at E. Columbia Street
Sapperton SkyTrain Station	6 to 7 minutes	1,310 ft. (400 m.) to Sapperton SkyTrain Station

Demolition of the Existing Single Detached Houses

Prior to submission of their rezoning and development permit applications, the applicant sought approvals for the demolition of the existing 1906 and 1907 houses. As per the City's policy for buildings that are 100 years or older, a Heritage Assessment for these

houses was submitted. The Heritage Assessment indicated that the buildings contain low to moderate heritage significance. The demolition permit application was forwarded to the Community Heritage Commission (CHC) on April 7, 2021 and the CHC supported the staff recommendation for issuance. At this meeting, the CHC also recommended that the applicant consider advertising the 337 Keary Street house as available for relocation, and that they consider deconstruction as an alternative to demolition waste. The applicant intends to explore both ideas. The Director of Climate Action, Planning and Development has not yet issued the demolition permit.

PROJECT DESCRIPTION

The proposed development consists of two buildings containing nine side-by-side, ground-oriented residential townhouse units, consistent with the form anticipated by the Infill Townhouse and Rowhouse Development Permit Area guidelines. Building 1 is proposed to be oriented parallel to Keary Street, while Building 2 would be oriented towards the central drive aisle. Design drawings and rationale are included in Attachments 2 and 3.

Drawings indicate an overall density of 1.0 FSR. Due to the slope of the site, the buildings range from between two to three storeys above grade. The drawings indicate that the proposed unit sizes would range from approximately 1,200 sq. ft. (111 sq. m.) to 2,100 sq. ft. (195 sq. m.) inclusive of garage areas and 1,200 sq. ft. (111 sq. m.) to 1,800 sq. ft. (167 sq. m.) exclusive of garage areas. All units are proposed to be family-friendly, containing three bedrooms located on the upper storey. Each unit would have access to outdoor space in the form of ground level, deck, and balcony areas. A project statistics table is included in Attachment 4.

As no rear lane is present, vehicle access and parking would be via a central drive aisle. The applicant has proposed a total of 10 off-street parking spaces inclusive of nine resident stalls and one visitor/loading stall. Parking for eight units would be accommodated in attached garages, while parking for one unit would be provided via a parking pad located at the rear of the visitor parking pad. The proposed parking complies with the Zoning Bylaw requirements for infill townhouse developments. Consistent with policy expectations, long-term bicycle parking spaces would be provided for each unit, as well as six short-term bicycle parking spaces. A rendering of the proposed development, viewed from Keary Street, is provided in Figure 2 below.



Figure 2: Proposed Project Rendering (view from Keary Street)

DISCUSSION

Building Massing and Transition

As the subject site is located within the Ground Oriented Housing DPA (DPA 1.2), the proposal would be evaluated against the Infill Townhouse and Rowhouse DPA guidelines and principles. The applicant has proposed a contemporary form with gable roof lines, reflective of the architectural character of Keary Street. Building 1 is proposed to transition to neighbouring single-detached houses by stepping down the building from west to east and integrating a two storey end unit adjacent to the vehicular access point. Staff considers the overall density and form to be generally consistent with the neighbourhood's context, and the design to be generally consistent with the guidelines.

The proposed design would be further reviewed by staff through the application review process, with input from the New Westminster Design Panel.

Off-Street Parking and Vehicular Access

As no rear lane is present at this site, staff have indicated that attached resident garages, accessed via a central drive aisle, may be supported. Though the Infill Townhouse and Rowhouse DPA guidelines do not permit enclosed parking structures, this relaxation is being considered due to the site's constraints. The proposed garage areas are included in the project's density calculations.

Basement Level

Below-grade spaces are proposed for all units. However, due to the sloping nature of the site and the maximum grade to which the central drive aisle can be lowered, these basement areas do not meet the "basement" definition contained in the RT zone. As a result, a Comprehensive Development (CD) zone would be created for this parcel to accommodate this constraint.

Trees

As part of this rezoning application, the applicant has submitted an arborist report in support of a Tree Permit application. The site is fairly heavily treed, with six trees present on-site and eight trees, including two City trees, present off-site. Of these, five are specimen-sized. The current design proposes the removal of four on-site trees, to facilitate development, and protection of two on-site trees as well as all (eight) off-site trees. Replacement trees are proposed in excess of that required. Further review of the project's tree retention and replacement strategy would be undertaken by staff during the application review process.

CONSULTATION AND REVIEW PROCESS

The City has formalized the interim development review process. As per this process, the anticipated review steps for this application are:

1. Preliminary report to Council (**WE ARE HERE**);
2. Applicant-led public consultation, including dissemination of information through the local Residents Association;
3. Presentation to the New Westminster Design Panel;
4. City-led public consultation, including the creation of a Be Heard New West webpage and survey;
5. Council consideration of First and Second Readings of the project's Bylaws and issue notice of waiving Public Hearing;
6. A Public Hearing (if scheduled) followed by Council's consideration of Third Reading and Adoption of the rezoning Bylaw
7. Issuance of the Development Permit by the Director of Climate Action, Planning and Development.

As the form of development is consistent with the Official Community Plan, the application would not be forwarded to the Advisory Planning Commission for review.

INTERDEPARTMENTAL LIAISON

The City has a project-based team approach for reviewing development applications, which facilitates interdepartmental review, providing comments to the applicant throughout the development review process.

OPTIONS

The following options are available for Council's consideration:

1. That Council direct staff to proceed with processing the proposed rezoning at 337 and 339 Keary Street, as outlined in the "Consultation and Review Process" section of this report;

2. That Council provide staff with alternative direction.

Staff recommend Option 1.

ATTACHMENTS

Attachment 1 – Policy and Regulations Summary

Attachment 2 – Drawing Package

Attachment 3 – Applicant's Design Rationale

Attachment 4 – Project Statistics Summary

This report was prepared by:

Wendee Lang, Planning Analyst

This report was reviewed by:

Rupinder Basi, Supervisor of Development Planning

Lynn Roxburgh, Acting Supervisor of Land Use Planning and Climate Action

This report was approved by:

Emilie K. Adin, Director, Climate Action, Planning and Development

Lisa Spitale, Chief Administrative Officer

Attachment 1

Policy and Regulations Summary

POLICY AND REGULATIONS SUMMARY

Official Community Plan

The subject properties are designated (RGO) Residential – Ground Oriented Infill Housing, which is described, in part, as follows:

Purpose: To allow a mix of ground oriented infill housing forms which are complementary to the existing neighbourhood character. Generally forms with a higher number of units are expected to be located on larger properties. Units can be attached, detached or a combination of the two.

Principal Forms and Uses: Single detached dwellings, single detached dwellings on a compact lot, duplexes, triplexes, quadraplexes, cluster houses, townhouses, rowhouses and other equivalent ground oriented housing forms. Lots with single detached dwellings may also include a secondary suite and/or a detached accessory dwelling unit.

The proposed development is consistent with the intent of the RGO land use designation and therefore, no Official Community Plan (OCP) amendment would be required for this proposed development.

Development Permit Area

The subject site is located within the Ground Oriented Housing Development Permit Area (DPA 1.2) and, as such, a Development Permit is required for the proposed development. This DPA refers development of infill townhouse properties to DPA 1.3 (Infill Townhouses and Rowhouses). The intent of this DPA is to encourage small scale infill projects, such as infill townhouses and rowhouses that are complementary to the existing single detached dwelling context through appropriate building form, scale, and location. Small projects, with a low number of units, are anticipated.

A Development Permit is required for the proposed development, which will be reviewed concurrently with the Rezoning application. Development Permits have been delegated to the Director of Climate Action, Planning and Development for issuance.

Zoning Bylaw

The subject properties are currently zoned Single Detached Residential (RS-1), and would need to be rezoned and consolidated to support the proposed development.

As the basements proposed as part of the development do not satisfy the “basement” definition contained in the Infill Townhouse and Residential Districts (RT), the applicant is proposing to rezone to a Comprehensive Development (CD) zone. The intent of the RT district is to, in combination with the design guidelines for infill townhouses and rowhouses contained within the Official Community Plan, allow infill townhouses and

rowhouses that integrate well into existing single detached residential neighbourhoods. The proposal meets this intent, which would be reflected in the CD zone.

Family-Friendly Housing Policy

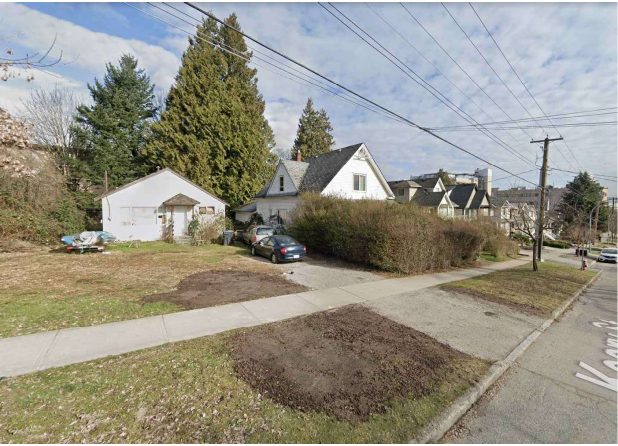
The proposed development has fewer than 10 units and therefore is not subject to the Family-Friendly Housing Policy requirements of the City; however, three bedrooms are proposed for each unit, which is in keeping with the Policy.

Attachment 2

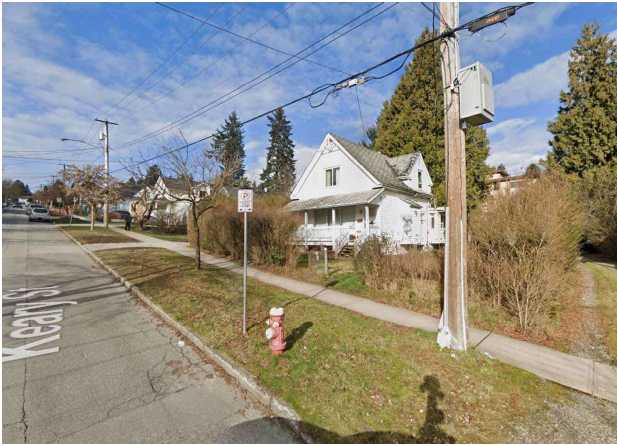
Drawing Package



 SUBJECT SITE  SKYTRAIN  BUS STOPS - - - 400m WALKING CIRCLE (5 MINUTES)  BIKE ROUTES



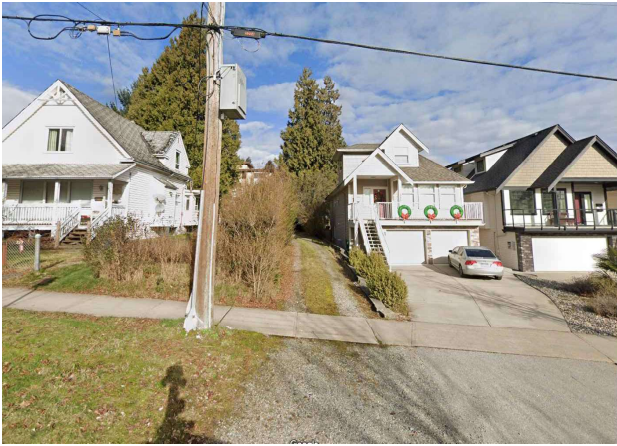
1 VIEW FROM SOUTH WEST ACROSS SITE



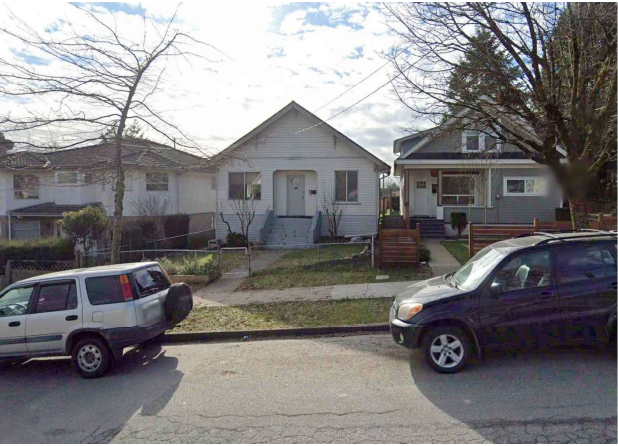
2 VIEW FROM SOUTH EAST ACROSS SITE



3 NEIGHBOUR TO THE WEST - 345 KEARY



4 NEIGHBOUR TO THE EAST - 333 KEARY



5 VIEW SOUTH FROM SITE - 340 KEARY



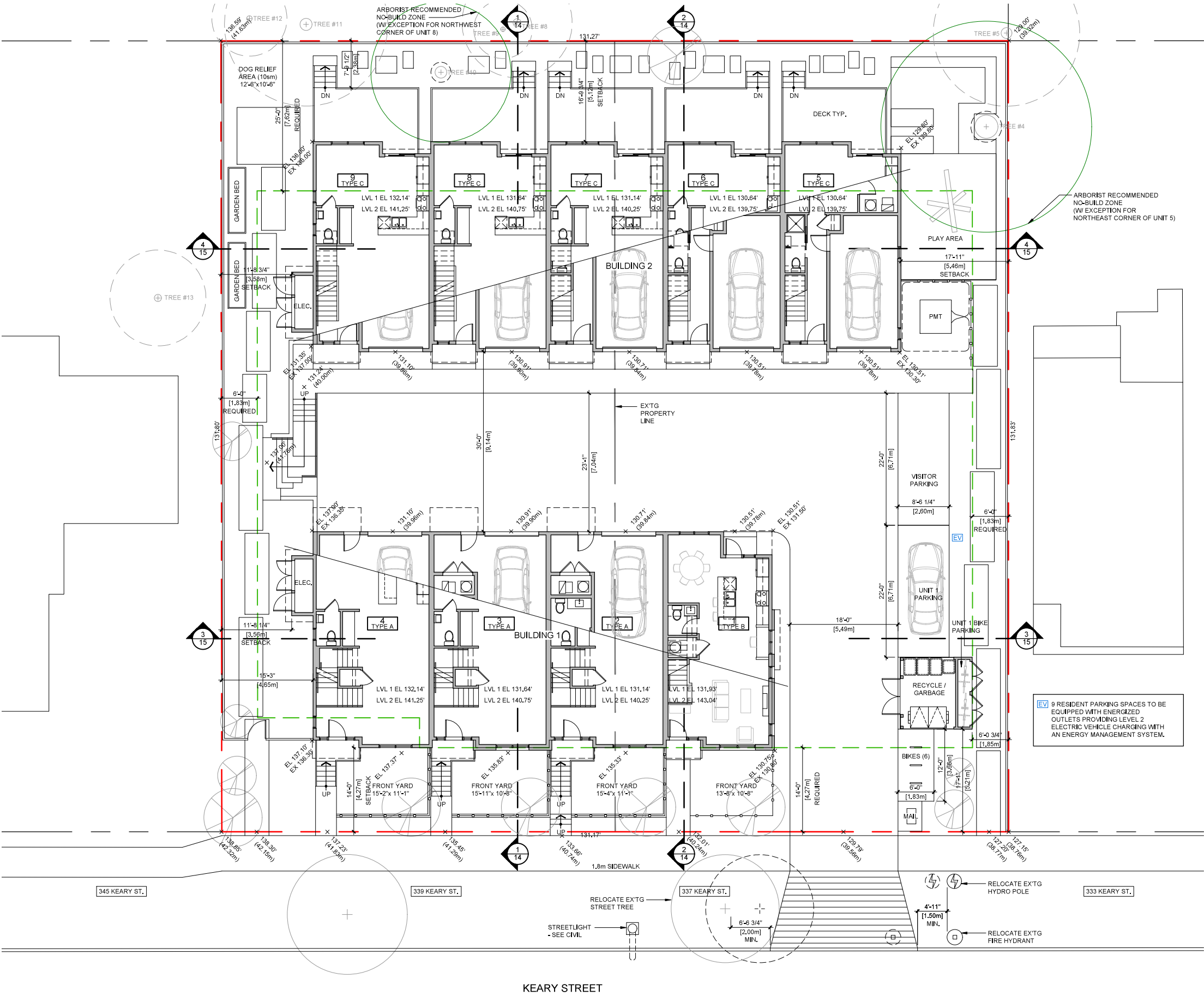
KEY PLAN

CONTEXT PLAN - SITE PHOTOS

KEARY TOWNHOMES

DECEMBER 13, 2021
337 & 339 KEARY STREET, NEW WESTMINSTER

GRIMWOOD
ARCHITECTURE



PROJECT STATISTICS		
CIVIC ADDRESS:	337 & 339 KEARY STREET, NEW WESTMINSTER, BC	
LEGAL DESCRIPTION:	LOTS 30 AND 35, BOTH OF LOTS 8 AND 9 SUBURBAN BLOCK 3, NWD, PLAN 2620	
ZONING:	PROPOSED: CD - BASED ON RT (INFILL TOWNHOUSE AND ROWHOUSE RESIDENTIAL DISTRICTS) EXISTING: RS-1	
LOT AREA:	1,606.9sm (17,296sf)	
LOT COVERAGE:	ALLOWED N/A	PROPOSED 36.6% (6,328sf)
BUILDING FLOOR AREA: (EXCL. GARAGES):	N/A N/A	1,621.1sm (17,449.4sf) 1,413.0sm (15,209.4sf)
FSR: (EXCL. GARAGES):	N/A N/A	1.01 0.88
BUILDING SETBACKS:	ALLOWED	PROPOSED
NORTH:	7.62m (25.0ft)	5.12m (16.8ft)
EAST:	1.83m (6.0ft)	5.46m (17.9ft)
WEST:	1.83m (6.0ft)	3.56m (11.7ft)
SOUTH (KEARY ST):	4.27m (14.0ft)	4.27m (14.0ft)
SOUTH (END UNIT):	5.79m (19.0ft)	4.27m (14.0ft)
BUILDING HEIGHT:	ALLOWED	PROPOSED
BUILDING 1:	10.67m (35.0ft)	10.34m (33.9ft)
BUILDING 2:	10.67m (35.0ft)	10.48m (34.4ft)
PARKING:	REQUIRED	PROPOSED
3+ BED (1.0/ UNIT)	9 (1.0 x 9)	9
VISITOR (0.1/ UNIT)	1 (0.1 x 9)	1
TOTAL PARKING	10	10
EV (ENERGIZED LEVEL 2 OUTLET)	9	9
BICYCLE PARKING:	REQUIRED	PROPOSED
LONG TERM (1.25/ UNIT)	12 (1.25 x 9)	12
SHORT TERM	6	6
RECYCLING:	3 - 360L/ 12 UNITS	3 - 360L BINS
FOOD SCRAPS:	1 - 240L/ 49 UNITS	1 - 240L BIN
GARBAGE:	1 - 240L/ 2 UNITS	1 - 3 CUBIC YARD BIN

FLOOR AREA CALCULATIONS:						
UNITS	LOWER FLOOR AREA	GARAGE AREA	MAIN FLOOR AREA	UPPER FLOOR AREA	UNIT AREA INCL. GARAGE	UNIT AREA EXCL. GARAGE
BUILDING 1						
1	600.4		638.4		1,238.8sf	1,238.8sf
2	692.9	302.3	678.4	712.6	2,083.9sf	1,781.6sf
3	692.9	302.3	678.4	708.3	2,079.6sf	1,777.3sf
4	701.6	301.8	686.6	716.9	2,105.1sf	1,803.3sf
BUILDING 2						
5	665.7	271.2	665.7	671.8	2,003.2sf	1,732.0sf
6	657.6	265.6	657.6	663.5	1,978.7sf	1,713.1sf
7	657.6	265.6	657.6	663.5	1,978.7sf	1,713.1sf
8	657.6	265.6	657.6	663.5	1,978.7sf	1,713.1sf
9	665.5	265.6	665.5	671.7	2,002.7sf	1,737.1sf
TOTAL:	5,991.8sf	2,240.0sf	5,985.8sf	5,471.8sf	17,449.4sf	15,209.4sf



BUILDING 1 SOUTH ELEVATION



BUILDING 1 NORTH ELEVATION



BUILDING 2 SOUTH ELEVATION



BUILDING 2 NORTH ELEVATION

COLOUR ELEVATIONS

KEARY TOWNHOMES

DECEMBER 13, 2021

337 & 339 KEARY STREET, NEW WESTMINSTER

GRIMWOOD
ARCHITECTURE

Page 255 of 417

DP RESUBMISSION



BUILDING 1 FROM SOUTHWEST



BUILDING 1 FROM SOUTHEAST



BUILDING 1 FROM NORTHEAST



BUILDING 2 FROM SOUTHEAST

COLOUR PERSPECTIVES KEARY TOWNHOMES

DECEMBER 13, 2021

337 & 339 KEARY STREET, NEW WESTMINSTER

GRIMWOOD
ARCHITECTURE

Attachment 3
*Applicant's Design
Rationale*

GRIMWOOD

Architecture + Urban Design

Project Name: Keary Street Townhomes

Project Address: 337 & 339 Keary Street. New Westminster, BC

Date: 2021.12.17

DESIGN RATIONALE

Sapperton Heights Holdings Inc. is proposing to redevelop 2 lots along Keary Street, which currently contain two older single-family homes, with a 9-unit townhouse project. The proposed townhouse units are all of three-bedroom configuration.

PROJECT DESCRIPTION AND DESIGN RATIONALE

The 9-townhomes will be contained in two buildings. One building will front Keary Street and one will sit behind a central drive aisle/courtyard.

Buildings facing Keary Street will be 2.5 storey's in height, while the units behind will be 3 storey's above grade. All units will have private and usable outdoor space. Along the Keary frontage is a landscaped front yard that incorporates a classic 'stoop' condition that activates the street condition, puts eyes on the street and provides a buffer from traffic. The rear town homes have generous elevated decks on the rear that connect directly from the main level living space.

SITE CONSTRAINTS

It is our understanding the intent of the RT Townhouse Rowhouse guideline has to do with the perception of scale and sensitive integration into existing single-family neighbourhoods. The requirement of having 0.75 FSR above grade and 0.15 below grading ensures the impact of the built form to the surrounding single family homes is minimized, as the units are substantially pushed into the ground.

There are several constraints unique to this site that make meeting these requirements challenging:

Because the site is without access from a lane on the north, vehicle access must come directly off Keary Street. Due to the size and scale of the site an underground parkade is not economically feasible, therefore we must utilize slab on grade construction. With this type of construction the elevation of the lower floor level is tied directly to the elevation of the garages, which are in turn set by the elevation of the central drive aisle. Driveway grades have been designed to a minimum elevation so the buildings can sit as low as possible on the site while meeting the Overland flow path requirement. We are thereby limited in our ability to sink the units further into the site, which precludes us from qualifying our lower level as 'basement' as per the CNW definition. We are therefore request that we proceed with the application under a CD zone and have more of our allowable FSR 'above grade'.

In an effort to meet the intent of the RT guidelines we have manipulated the built form so as to minimize the impact to the single family homes to the east and west. 3 storey units are located on the west 'high' side of the site, pushed down as low as possible and buried into the high side of the site. The resultant elevation on the west property line is a 2 storey elevation. As the site falls dramatically to the east, we introduced a 2 storey unit on south-east corner, to present a similar 2 storey elevation and successfully transition to the single family home to the east.

FORM + CHARACTER

(a) Building Materials

- We've proposed an elevated material palette that is residential in character (Walls: Brick / horizontal siding, cementitious panels. Roof: Asphalt Shingle. Windows: Vinyl, Softs: Natural Wood);
- Exposed concrete to be concealed with landscaping.

(b) BUILDING COLOURS

- Proposed colours reflect a common palette of muted grey and white exterior, with accented front doors and windows.

(c) COMPATIBLE ELEVATIONS AND TRANSITIONS

- All building faces visible from adjacent streets have been designed to compliment the existing elevations of adjacent properties.
- All homes are ground oriented with individual front entries that are clearly defined.
- Front facing doors along Keary will contribute to the enhancement and increased activity along this street.

(d) FACADES

- Each unit is clearly legible and delineated as a private home providing articulation across the main elevation.

(e) ROOF LINES

- Proposed roof lines are complimentary to existing character nature of the neighbourhood.

(f) NATURAL SYSTEMS

- All units have large and operational windows at fronts and backs of homes which will allow for improved internal ventilation and natural light penetration. Where possible, windows were also included on sides of building to further increase the light and air circulation.

(g) PARKING AREAS

- Private off-street parking is provided in private at-grade garages accessed from a central driveway.
- Visitor parking area will be clearly marked as visitor parking.

(h) UTILITY AND GARBAGE AREAS

- Garbage/Recycling containers will be provided in a designated screened enclosure
- Individual hot water utility rooms will be inside each home;

NEIGHBOURHOOD CHARACTER

- The proposed design offers a gentle, yet defined transition from the adjacent residential lot and complements the surrounding community amenities
- The proposed design echoes the existing materials and character of the neighbourhood;
- The ratio of windows/doors to solid wall is compatible with existing buildings in the neighbourhood;
- Lighting features will include porch lights, lights in private outdoor areas and path lighting.

LANDSCAPING

- All areas not covered by hard surface will be landscaped;
- Landscaping will reflect local planting species;
- Landscaping will be used to enhance separation of public and private spaces.

LIVABILITY

- The buildings have been designed to minimize shadow impacts on adjacent sites with use of gable roof forms which allow light to filter through between gables roofs;
- Light penetration has been maximized by offering large and operational windows along front and rear of units and providing side windows where possible;
- All units will contain a private front yard and a private rear deck
- Bicycle storage will be provided inside private garages.

ENVIRONMENTAL

The immediate environmental benefit this project offers, will be the replacement of two existing houses that have extremely poor energy performance.

As mentioned, the site is close to major public transportation routes. This minimizes the need for dependency on cars and encourages walking to a wide range of essential amenities. Landscaping has been designed to allow for the retention of storm water on site to redistribute to local ground water or to compliment landscape irrigation. Light pollution is minimal because the development is compact.

AFFORDABILITY

Multiple families can be accommodated in three-bedroom townhomes at a price which is significantly lower than buying single family homes in the area. 9 families have the opportunity to own where currently only two families can be accommodated. Multi-family developments also help reduce the cost of living by splitting the costs of maintaining the land and buildings.

CPTED

Crime prevention has been accommodated with well-lit open side yards and spaces. Yards are open and landscaped with low vegetation to minimize opportunities to be undetected by residents. Another strength of this proposal, is its close proximity to shops and services. This includes, major public transit, close proximity to shopping, close proximity to public services, including the Hospital, Police, Ambulance, Fire services, Elementary and High Schools, and City Hall.

VARIANCES

Due to the lack of rear lane access we have no choice but to provide vehicle access/driveway off Keary Street and access units through a central drive aisle. This creates significant spatial constraints on the site and we would request relaxations in the following areas:

- Height envelope on the west property line of approx. 3'-10.5".
- Rear yard setback.

FAMILY-FRIENDLY HOUSING POLICY

The project seeks to provide a variety of townhouse sizes including (9) three bedroom townhomes.

The townhomes will feature exclusive outdoor space at grade, spacious decks, both juliette and walk out balconies.

The complex will encircle a central drive aisle which will double as a communal social area/courtyard.

SITE CHARACTERISTICS

The site sits between Richmond Street to the west and East Columbia to the east, with an 11.7 ft fall in grade to the east.

Access to the site is from Keary St only (there is no lane access)

Two single family homes currently occupy the lots. of which were assessed to have little heritage value and approved for demolition by the Heritage Advisory Committee.

PROJECT DETAILS

ZONING: Proposed: CD referencing infill townhouse and rowhouse residential districts. Existing: RS-1

LOT AREA: 1,606.9sm (17,296sf)

LOT COVERAGE: proposed 36.6% (6,328sf)

TOTAL FSR INCLUSIVE OF BASEMENT:

allowed: 1.01

proposed: 1,621.1sm (17,449.4sf)

TOTAL FSR EXCLUDING BASEMENT:

allowed 0.85

proposed 1,413.0sm (15,209.4sf) 0.88

BUILDING SETBACKS:

NORTH:

allowed 7.62m (25.0ft)

proposed 5.12m (16.8ft)

EAST:

allowed 1.83m (6.0ft)

proposed 5.46m (17.9ft)

WEST:

allowed 1.83m (6.0ft)

proposed 3.56m (11.7ft)

SOUTH (KEARY ST):

allowed 4.27m (14.0ft)
proposed 4.27m (14.0ft)

SOUTH (END UNIT):

allowed 5.79m (19.0ft)
proposed 4.27m (14.0ft)

BUILDING HEIGHT:

building 1: 10.67m (35.0ft)
proposed 10.34 m (33.9ft)
building 2: 10.67m (35.0ft)
proposed 10.48m (34.4ft)

-PARKING:

3+ BED (1.0/ UNIT)

required: 9 (1.0 x 9) 9
proposed: 9

VISITOR

required (0.1/ unit) 1 (0.1 x 9)
proposed 1

TOTAL PARKING

required 10
proposed 10

EV (ENERGIZED

required 9
proposed 9

BICYCLE PARKING:

LONG TERM (1.25/ UNIT)

required 12 (1.25 x 9)
proposed 12

SHORT TERM

proposed 6

RECYCLING: 3 - 360 litre bins

GARBAGE: 1 - 3 cubic yard bin

FOOD SCRAPS:1-240L BIN

Attachment 4
Project Statistics
Summary

PROJECT STATISTICS

	Permitted / Required Under RT Zoning	Proposed
Lot Area	-	17,296.8 sq. ft. (1,606.9 sq. m.)
Site Frontage	-	131.2 ft. (40.0 m.)
Average Lot Depth	-	131.8 ft. (40.2 m.)
Total FSR Inclusive of Basement	1.00 FSR	1.01 FSR
Total FSR Excluding Basement	0.85 FSR	1.01 FSR <i>Proposed basements do not meet RT zone definition</i>
Maximum Basement Height Above Existing Grade	3.28 ft. (1.0 m.)	<i>Proposed basements do not meet RT zone definition</i>
Building Height	35 ft. (10.67 m.)	Building 1: 33.9 ft. (10.3 m.) Building 2: 34.4 ft. (10.5 m.)
Residential Units	-	9 units
Off-Street Parking Resident Visitor/Loading Total	9 spaces 1 spaces 10 spaces	9 spaces 1 spaces 10 spaces
Bicycle Parking	For all uses permitted in the RT Zone, off-street bicycle parking is required in accordance with the Laneway and Carriage House Design Guidelines within the City of New Westminster Official Community Plan.	12 spaces (long term) 6 spaces (short term)

REPORT

Office of the Chief Administrative Officer

To: Mayor Cote and Members of Council **Date:** January 10, 2022

From: Lisa Spitale
Chief Administrative Officer **File:**

Item #: 2022-31

Subject: Update regarding Downtown Livability Strategy

RECOMMENDATION

THAT Council endorse the actions underway.

PURPOSE

To provide an update on the status of the immediate and short term actions from the Downtown Livability Strategy, previously presented to Council on October 18, 2021.

SUMMARY

A number of issues such as fires, vacant properties, the COVID-19 pandemic, and increased homelessness have impacted livability in New Westminister's downtown. On September 13, 2021, Council passed a motion to support livability, called the *Downtown Recovery Strategy*.

In response, staff developed a Downtown Livability Strategy that included immediate and short term actions, endorsed by Council on October 18, 2021. The approach is interdepartmental and is based on the City's vision that New Westminister is a vibrant, compassionate, sustainable city that includes everyone.

This report provides an update on the immediate and short term actions outlined in that Strategy, as well as information on possible longer term actions that can be taken for the Downtown and in other neighbourhoods of the city.

BACKGROUND

On October 13, 2021, Council passed a motion called the Downtown Livability Strategy.

Whereas Downtown is the densest and most rapidly-growing residential neighbourhood of New Westminster, representing a commitment to regional Transit-Oriented Mixed-Use development goals concentrated in identified Regional City Centres; and

Whereas the initial revitalization since the 2010 Downtown Community Plan was developed has suffered a series of more recent setbacks, including the loss of several historic buildings to fires and the loss of a major anchor retailer, while a recent loss of park space and ongoing construction serve to challenge livability goals for the downtown community; and

Whereas despite robust growth, several properties in the key blocks of Columbia Street have been derelict or vacant for many years, impairing neighborhood revitalization efforts and challenging the impression of Columbia Street as a vibrant commercial district for both residents and existing businesses;

Therefore be it resolved:

That staff review strategies and regulatory tools available to Council to support the rapid revitalization of underperforming, derelict, and vacant properties on Columbia Street in the historic Downtown, including but not limited to powers under the New Westminster Redevelopment Act (1989), and

That Staff provide recommendations for rapid and medium-term actions to support the vibrancy of business, the activation of the streets, and improving the amenity value of the historic Downtown for all residents of New Westminster.

In response, staff developed a Downtown Livability Strategy that included immediate and short term actions, endorsed by Council on October 18, 2021. The approach is interdepartmental and is based on the City's vision that New Westminster is a vibrant, compassionate, sustainable city that includes everyone.

Through an interdepartmental team that includes Engineering, Police, Fire, Economic Development, Social Planning and Integrated Services, five areas of focus for the immediate and short term actions were determined:

1. Cleanliness and 24-hour public toilets
2. Homeless outreach and added emergency shelter capacity
3. Opioid epidemic and illicit drug response
4. Business support and engagement
5. Work with Fraser Health to address mental health issues

ANALYSIS

Staff determined that there were five areas of focus for the immediate and short term actions and have been working to implement actions to address the issues.

Immediate and Short Term Tactics:

Many of the identified tactics have been implemented and are completed or in progress. The following table provides an update on those tactics.

Issue:

Need to improve general cleanliness and the provision for 24 hour, public toilets in the Downtown

Category	Action Identified in October 18th, 2021 Report	Notes:
Immediate	The Engineering Department has added larger garbage receptacles throughout the Downtown and increased collection frequency.	Waste receptacles installed.
	Expression of Interest has been developed for specialized waste management in the Downtown.	Services secured.
	The City is adding one portable toilet in Hyack Square.	This toilet was installed November 22.
Short Term	Investigate resource requirements to support late night garbage collection (refer to 2022 Budget process).	Two additional Auxiliary Labour staff working 4PM – 12:30AM, 7 days per week, beginning November 22nd.
	Continue work plan to provide 24/7 washrooms in City facilities, and report back to Council.	Work continues as staff coordinate internally on strategies for operations, maintenance, and public communications.

	Investigate with TransLink the provision of public washrooms at SkyTrain stations.	Initial conversations have occurred.
	Investigate additional fee for service with faith-based or non-profit service providers as washroom attendants/outreach service providers.	In the new year, a more fulsome discussion will occur as part of a larger discussion as part of the other public toilet initiatives.
	Prepare to purchase and install a prefabricated stand-alone public toilet in the Downtown and report back to Council (already identified in the 2022 Budget).	A project management consultant has been retained to forward this work.

Issue:**Homeless outreach and added emergency shelter capacity**

Category	Action Identified in October 18th, 2021 Report	Notes:
Immediate	Police, Bylaws, Engineering and Fire Departments are creating a coordinated multi-Departmental Team.	The team has been created, and protocols and procedures established. A second Vulnerable Persons Officer has been temporarily added.
	The multi-Departmental Team is working on a protocol to check the well-being of homeless individuals and attend to any safety or environmental hazards on a daily basis.	
Immediate	flu vaccines	Seasonal flu vaccines were provided to the vulnerable populations that attended the "Don't Go Hungry" Program in Queensborough.

Short Term	<p>In collaboration with the Lower Mainland Purpose Society, a temporary Warming Centre was established at 40 Begbie Street.</p>	<p>The City and the Lower Mainland Purpose Society operationalized a Warming Centre at the Health Contact Centre, located at 40 Begbie Street. The Warming Centre operated from 11:00 p.m. to 8:00 a.m. between December 26 and 30, 2021, and averaged about 15 unsheltered persons per night.</p> <p>With the opening of the Extreme Weather Response Program (EWRP) shelter on the lower floor of the former Army and Navy Department Store, which opened on December 30, 2021, the Warming Centre was decommissioned.</p> <p>City staff will be debriefing about the Warming Centre, and it could form one of the recommendations as contained in the City's Homelessness Action Strategy, which is under development.</p>
	<p>In collaboration with BC Housing and the Lower Mainland Purpose Society, up to 50 Extreme Weather Response Program beds were opened on the lower floor of the former Army and Navy Department Store.</p>	<p>New Westminster now has up to 65 Extreme Weather Response Program (EWRP) beds. Of this number, up to 50 are located on the lower floor of the former Army and Navy Department Store, located at 502 Columbia Street, and up to 15 are located on the lower floor of the Russell Housing Centre, located at 740 Carnarvon Street.</p> <p>The EWRP beds are activated during extreme weather events and are operational until March 31, 2022. Given the opening of the EWRP shelter at the former Army and Navy Department Store, the City is determining the status of and need for the EWRP shelter at the Russell Housing Centre.</p> <p>To date, the EWRP shelter at the former Army and Navy Department Store is averaging about 35 unsheltered persons per night, thus there is still some excess capacity. The City, with the Lower</p>

		<p>Mainland Purpose Society, are advocating for the EWRP shelter at the former Army and Navy Department Store to transition to a Winter Shelter, which would be operational on a nightly basis until March 31, 2022, and preferably a 24/7 shelter with support services, which would be operation for 14 or 18 months or until new supportive housing is ready for occupancy at 60 to 68 Sixth Street.</p>
	<p>City of New Westminster Inter-Departmental Working Group on Homelessness continues to meet in its efforts to address business and resident concerns related to homelessness and other social issues.</p>	<p>The inter-Departmental Working Group on Homelessness, which is mandated to address business and resident concerns related to homelessness and other social issues, and facilitate inter-Departmental communication and collaboration, continues to meet. The Working Group includes a standing agenda item related to the Downtown Livability Initiatives but is also focused on addressing concerns in other neighbourhoods of the city.</p> <p>Recently, it scheduled a walkabout with Downtown businesses to discuss their perspectives and to solicit their ideas for action; however, this walkabout had to be rescheduled due to inclement weather.</p>

Issue:**Opioid epidemic and illicit drug response**

Category	Action Identified in October 18th, 2021 Report	Notes:
Immediate	Outreach and training between Fire and Police first responders, Bylaw staff and Social Planners	<p>Bylaw staff oriented Fire staff with a driving tour of areas that are frequented by people experiencing homelessness.</p> <p>Bylaw and Fire staff attended a virtual training session "Harm Reduction Training for Local Government" on Dec. 10, 2021 hosted by Opioid Emergency Response Center (OERC), Community Action Initiative (CAI) and the British Columbia Centre for Disease Control (BCCDC).</p> <p>Outreach training between New West Fire and Police has begun with introductory ride-alongs for fire personnel.</p>

Issue:**Business support and engagement**

Category	Action Identified in October 18th, 2021 Report	Notes:
Immediate	Create a dashboard for Economic Development to understand the current situation from other divisions when businesses reach out with who to contact for further information.	<p>Although a dashboard has not been created, a communications pipeline has been established where staff report weekly to one person, who collects the information and puts public updates on newwestcity.ca/downtownlivability and develops an update for council on items that are confidential.</p> <p>A one page PDF was created early November 2021 to summarize the actions, and a follow up</p>

	Formalized plan and materials for business outreach that cohesively relays information coming from City Departments.	one page PDF will be created in early January. Additionally, business education training has been planned as a complement to the VENN program for non-profits, to provide education on overdose, homelessness, crime prevention, etc. These are being planned by Police, and Economic Development, with input from Bylaws, and Fire. The first one is tentatively scheduled for early February, and additional topics will be presented every second month.
	Targeted overdose awareness and education for trades, temporary labour, and construction companies by refocusing existing work with the BIA, WorkSafe and Purpose Society.	Planning has been started on this educational opportunity and it is tentatively scheduled for February 2022.

Issue:

Need to work with Fraser Health in addressing mental health issues which are contributing to increasing homelessness and illicit drug use

Category	Action Identified in October 18 th , 2021 Report	Notes:
Immediate	City staff will work closely with Fraser Health's new Integrated Response Team which will support sheltered and unsheltered homeless persons with mental health issues.	The Fraser Health Integrated Response Team will support sheltered and unsheltered vulnerable persons with mental health issues in New Westminster and the Tri-Cities. A new manager is being hired to operate the service, which will be comprised of a mix of nursing, allied health professionals, and clinical and peer support workers. The service will operate between 8:30 a.m. and 4:30 p.m., seven days per week. Fraser Health funds the team.

	City staff will clarify the role of the Assertive Community Treatment (ACT) Team, which includes the City's Mental Health Police Liaison Officer, and serves adults living with serious mental illness.	City staff will request that the Assertive Community Treatment (ACT) Team make a presentation to Council. As part of this presentation, it will be requested to clarify its role and mandate, including related to the new Fraser Health Integrated Response Team; the number of clients it is serving, including in New Westminster and the Tri-Cities; and how it is working with City staff, including in Fire, Integrated Services and Police, and Homeless Outreach.
Short Term	The City to provide mental health education, information and training to staff on the front lines, including in Engineering Operations, Integrated Services, and Parks and Recreation.	Training is underway with staff in a variety of divisions.

Medium and Longer Term Tactics:

Some tactics identified in the motion and the plan require additional analysis and/or planning to implement them:

- Develop a work plan to reduce timelines for building permit applications and business license applications that require building permits
 - Staff are continuing to work to reduce permitting times through increased inter-departmental collaboration, implementation of enhanced electronic review, streamlining internal and inter-departmental processing procedures and additional staff training to increase technical capacity. We anticipate this will positively impact the workflow such that the backlog is reduced before the end of 2022. An additional challenge is provincial regulations, adopted in February 2021, requiring BOABC certification of plan reviewers and inspectors. This continues to impact staffing capacity throughout the province as demand far exceeds supply.
- *Sidewalk Café Bylaw (1997)* revisions based on learning from Temporary Patio Program
 - Staff have conducted analysis and are developing proposed changes informed by: the temporary patio program, the proposed changes by the Cannabis and Liquor Regulation Branch, and what other municipalities are doing. Council should expect a report in early 2022 with recommendations.

- *Streetscape improvements*
 - To support vibrancy and livability, streetscape improvements will be considered as budget allows, and to capitalize on opportunities as they come up.
- *Public washroom plan for new and existing permanent, accessible public washrooms under analysis*
 - A consultant has been retained to support potential procurement of a new, permanent, free-standing pre-fabricated washroom facility. Specific location has not been determined. It would be selected based on siting requirements and user preferences. Preliminary targeted community engagement is underway with additional planned for 2022. This work will also inform future to next steps.
 - The utilization of existing infrastructure is also being considered by staff working towards implementation.

INTERDEPARTMENTAL LIAISON

This is a high priority initiative. Several City departments are collaborating on the Downtown Livability Strategy.

OPTIONS

There are two options for Council's consideration; they are:

Option 1 – Council endorse the actions underway; or

Option 2 – Provide staff with alternate direction.

Staff recommend Option 1

This report was written by: Jen Arbo, Economic Development Coordinator, Office of the CAO with input from:

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This report was approved by:

Lisa Spitale, Chief Administrative Officer

REPORT

Engineering Services

To: Mayor Côté and Members of Council **Date:** January 10, 2022

From: Lisa Leblanc **File:** 05.1035.10
Director of Engineering Services (DOC #1978321)

Item #: 2022-12

Subject: **Uptown Active Transportation Improvements Projects - Design and Engagement Update**

RECOMMENDATIONS

THAT Council receive the Uptown Active Transportation Improvements Engagement Summary Report and Multiple Account Evaluation memo for information.

THAT Council endorse configuration Option 2 for the Rotary Crosstown Greenway Upgrades.

THAT Council endorse routing Option 2 for the New Westminster Secondary School Cycling Connector.

THAT Council direct staff to implement an interim New Westminster Secondary School Cycling Connector along Sixth Street (routing Option 2) using high quality lower-cost materials, to test the feasibility and evaluate impacts of the routing option on transit and traffic operations.

PURPOSE

To seek Council's endorsement to move forward to conceptual design and the next round of public engagement with the recommended configuration option for Rotary Crosstown Greenway upgrades and the recommended routing option for the New Westminster Secondary School Cycling Connector, based on results of the first round of engagement as well as recommendations from the findings of a multiple account evaluation for the Uptown Active Transportation Improvements projects.

BACKGROUND

The Uptown Active Transportation Improvements include two projects that will reimagine some streets to make it easier for people of all ages and abilities to cycle, wheel, or walk comfortably and safely in Uptown. These two projects include:

- The **Rotary Crosstown Greenway (RCG) Upgrades** on Seventh Avenue, which will improve safety, comfort, and accessibility on the section of the existing RCG along Seventh Avenue, between Eighth Street and Fifth Street. These upgrades will replace the interim bike lanes installed on Seventh Avenue in 2017.
- The **New Westminster Secondary School (NWSS) Cycling Connector** project, which will link the RCG on Seventh Avenue to the east entrance of the new school at Sixth Street and the new bike lane on the NWSS property, enabling students, staff and visitors to conveniently and safely cycle, wheel or walk to the school from the east and south.

These projects take direction from the vision set in the Council-approved Uptown Streetscape Vision, to create “a vibrant commercial district with a distinct identity, accessible and sustainable transportation, and inviting and engaging public spaces that are welcoming to all people.”

Additional goals include enhancing the public realm and tree canopy, seeking opportunities for green infrastructure, and reducing the impacts of motor vehicles and through traffic in this neighbourhood, while limiting the impacts to people who rely on on-street parking and loading.

EXISTING POLICY AND PRACTICE

The projects align with the City’s Master Transportation Plan goals, policies and actions, 2019-2022 Council Strategic Plan, Uptown Streetscape Vision objectives, and Seven Bold Steps for Climate Action – specifically Bold Steps 2 and 7, Car-Light Community and Quality People-Centered Public Realm.

ANALYSIS

Rotary Crosstown Greenway (Fifth Street – Eighth Street)

Three configuration options for this section of the RCG were presented to the public:

- **Option 1** involves providing narrow protected bicycle lanes and retaining on-street parking and loading on both sides of the street.
- **Option 2** involves providing protected bicycle lanes as well as additional streetscape enhancements such as street trees and landscaping where possible, but would retain parking on only one side of the street. Some loading zones may be preserved on both sides of the street for improved accessibility.

- **Option 3** involves creating a shared street for cyclists and motorists, while retaining parking and loading on both sides of the street. This would be achieved by closing Seventh Avenue west of Sixth Street to motor vehicle traffic.

At its meeting on November 8, 2021, the Sustainable Transportation Task Force (STTF) suggested that staff consider an alternative Option 2 involving one-way traffic operations along Seventh Avenue between Sixth Street and Eighth Street to preserve more on-street parking and loading spaces. This option could preserve approximately 13 to 16 existing parking spaces and 2 to 3 existing loading zones, depending on finalized design details, and may provide opportunities to slightly increase parking supply relative to today. However, there are also challenges to consider in terms of limited vehicle access to adjacent residential properties and businesses, as well as potentially increased traffic circulation on parallel corridors such as Hamilton Street and Princess Street. Further exploration of this configuration option would require additional technical analysis and public engagement, adding up to 3 months to the project schedule. Additional information is provided later in this report.

NWSS Cycling Connector

Three routing options for the NWSS Cycling connector to link Seventh Avenue to the east entrance of NWSS were presented to the public:

- **Option 1** would be protected bicycle lanes along Fifth Street and a shared lane connection between the 900 and 800 block of Sixth Street. Note that two sub-options (1A: on-street with parking removal and 1B: off-street in the wide boulevards) were presented to the public for this route.
- **Option 2** would provide uni-directional protected bicycle lanes along Sixth Street (a bike lane on each side of the street in the same direction as motorized traffic), with parking/loading prohibited on one side of the street.
- **Option 3** is a hybrid route with protected bicycle lanes along Fifth Street to Eighth Avenue, along Eighth Avenue to Sixth Street, and along Sixth Street to the east entrance of NWSS. Note that the likely configuration consists of bi-directional bicycle lanes on the north side of Eighth Avenue (one bike lane serving both directions of travel on one side of the street), as well as uni-directional or bi-directional bicycle lanes along Fifth and Sixth Streets.

It should be noted that these are not the only possible configurations for each project. However, only the most probable configurations were presented to the public to keep the analysis and engagement manageable. Multiple design permutations are possible for each route and design details will still need to be evaluated.

Stakeholder and Public Engagement

The first round of engagement for the projects was launched in September 2021 with a variety of marketing and promotional materials to raise awareness for the projects and provide opportunities for the public to participate in the process. This included a project video with over 220 views, sending notification letters to over 2,800 addresses,

distributing e-newsletters to over 1,800 Be Heard New West subscribers, as well as hosting five stakeholder meetings with participants representing 10 stakeholder groups, and two pop-up events with over 220 participants.

In addition to feedback submitted via email, the online interactive map, and the Question and Answer section of the project website, over 260 online surveys were completed. It should be noted that the survey participants are self-selecting, so the results are not a statistically representative sample of the population.

For the RCG Upgrade, Option 2 (protected bike lanes with parking on one side of the street) received the most support with 46% of participants indicating it as the preferred configuration option.

For the NWSS Cycling Connector, Option 2 (via Sixth Street) received the most support with 47% of participants indicating it as the preferred routing option.

An Engagement Summary Report and graphic are provided in Attachments #1 and #2.

Multiple Account Evaluation (MAE)

The primary objectives considered in the evaluation of the configuration and routing options for the two projects include:

- Providing a crosstown cycling route for people of all ages and abilities
- Improving the walking experience and accessibility
- Enhancing the public realm and tree canopy cover
- Maintaining transit service and operations
- Accommodating emergency access
- Reducing the impacts of motor vehicles and through traffic in this neighbourhood
- Limiting impacts to people who rely on on-street parking and loading
- Seeking opportunities to incorporate green infrastructure
- Minimizing risk and consider implementation and maintenance
- Considering results of public and stakeholder engagement
- Considering other outcomes and alignment with the City's Seven Bold Steps

For the RCG Upgrades, the project team recommends configuration Option 2 for advancing to conceptual design based on the assessment. This option is preferred given the higher community support and relatively fewer implementation challenges. As noted, however, STTF suggested that one-way traffic operation be considered, subject to Council direction. Should Council wish to consider one-way traffic operation, staff would require additional time to assess the impacts; a one-way configuration on Seventh Avenue would result in approximately 1,000 vehicle trips per day being diverted to nearby streets, and consideration may need to be given for additional traffic calming to mitigate the impacts.

For the NWSS Cycling Connector, the project team recommends Option 2 - it provides the most direct route and received the highest level of support, but with the most impacts on transit, traffic, and businesses - as the preferred routing option. Staff recommend that Option 2 be implemented in the near term using high quality temporary treatments, which would provide an opportunity to observe impacts, particularly to transit, traffic, and business access. If successful, the temporary treatment could remain in place for several years. Staff recommends removing routing Option 1 from further consideration due to its poor cycling connectivity and directness. Option 3 would provide a less direct route and received the lowest level of support, but it would have fewer impacts to transit, traffic, and businesses; it remains a feasible routing option in the event that the impacts of Option 2 are determined to be too significant.

A Multiple Account Evaluation memo with more detailed analysis of the various options is provided in Attachment #3.

SUSTAINABILITY IMPLICATIONS

Implementation of the RCG upgrades and the NWSS Cycling Connector are important projects for the Uptown neighbourhood, as well as the rest of the City and region, because they will provide local and regional connectivity to the BC Parkway and Central Valley Greenway. These projects will encourage active mobility and will reallocate street space, both of which align with the City's Master Transportation Plan goals, Council Strategic Priorities, and Seven Bold Steps for Climate Action. The project offers improved opportunities for residents, visitors, and employees of New Westminster to travel using sustainable modes. The design of the projects will be particularly oriented to those who might not otherwise feel comfortable walking and cycling on the existing active transportation network. Furthermore, the project seeks to increase tree canopy and implement more sustainable rainwater management techniques on the selected corridors, some of which currently lack green space. The projects also support socioeconomic equity goals by enabling more affordable and comfortable mobility options for those in need. The needs of seniors, families and people with disabilities will be carefully considered throughout the design and implementation process.

FINANCIAL IMPLICATIONS

A budget of \$450,000 for the engagement and design aspects of the projects, as well as \$2,500,000 for construction, is included in the 2020-2024 Financial Plan, and the draft 2021-2025 Financial Plan. The implementation of an interim NWSS Cycling Connector using high quality, but lower-cost materials in the near term (currently estimated to cost approximately \$200,000, based on recent experience with the interim Agnes Greenway) leaves the remaining budget for the RCG Upgrades, potentially enabling improved landscaping, green infrastructure, and greenway amenities to be integrated into the design. Implementation of permanent infrastructure for the NWSS Cycling Connector would be achieved through a combination of development contributions at the time of redevelopment of adjacent properties, and capital investment through future budget allocations.

The RCG Upgrades have confirmed funding from TransLink in the amount of \$590,750, and an application for additional cost-sharing has been submitted for the 2022 funding year for implementation of the proposed infrastructure. Grants from other sources are also being considered.

INTERDEPARTMENTAL LIAISON

In the planning for this project and the development of an initial concept for scoping and grant applications, staff from Engineering have convened a project steering committee comprising representatives from several departments and divisions, including Climate Action, Planning & Development, Parks & Recreation, Finance, Engineering Operations, and the CAO's office (Public Engagement, Communications).

OPTIONS

The following options have been presented for the Council's consideration:

1. **THAT** Council receive the Uptown Active Transportation Improvements Engagement Summary Report and Multiple Account Evaluation memo for information.
2. **THAT** Council endorse configuration Option 2 for the Rotary Crosstown Greenway Upgrades.
3. **THAT** Council direct staff to undertake further technical analysis and public engagement on converting Seventh Avenue to one-way between Sixth Street and Eighth Street and report back to Council.
4. **THAT** Council endorse routing Option 2 for the New Westminster Secondary School Cycling Connector
5. **THAT** Council direct staff to implement an interim New Westminster Secondary School Cycling Connector along Sixth Street (routing Option 2) using high quality lower-cost materials to test the feasibility and evaluate impacts of the routing option on transit and traffic operations.
6. **THAT** Council provide alternate direction to staff.

Staff recommend options 1, 2, 4, and 5.

CONCLUSION

The Rotary Crosstown Greenway Upgrades and New Westminster Cycling Connector will provide New Westminster residents and visitors with all-ages-and-abilities cycling facilities, which will help the City achieve its transportation and climate action goals.

ATTACHMENTS

Attachment #1 – Uptown Active Transportation Improvements Round 1 Engagement
Summary Report

Attachment #2 – Uptown Active Transportation Improvements Round 1 Engagement
Summary Graphic

Attachment #3 – Uptown Multiple Account Evaluation Memo

APPROVALS

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This report was reviewed by:

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This report was approved by:

Lisa Leblanc, P.Eng., Director, Engineering Services

Lisa Spitale, Chief Administrative Officer

Attachment # 1

Uptown Active Transportation Improvements Round 1 Engagement Summary Report

UPTOWN

ACTIVE TRANSPORTATION IMPROVEMENTS

Round 1 | Engagement Summary Report



NEW WESTMINSTER



PREPARED FOR:

City of New Westminster
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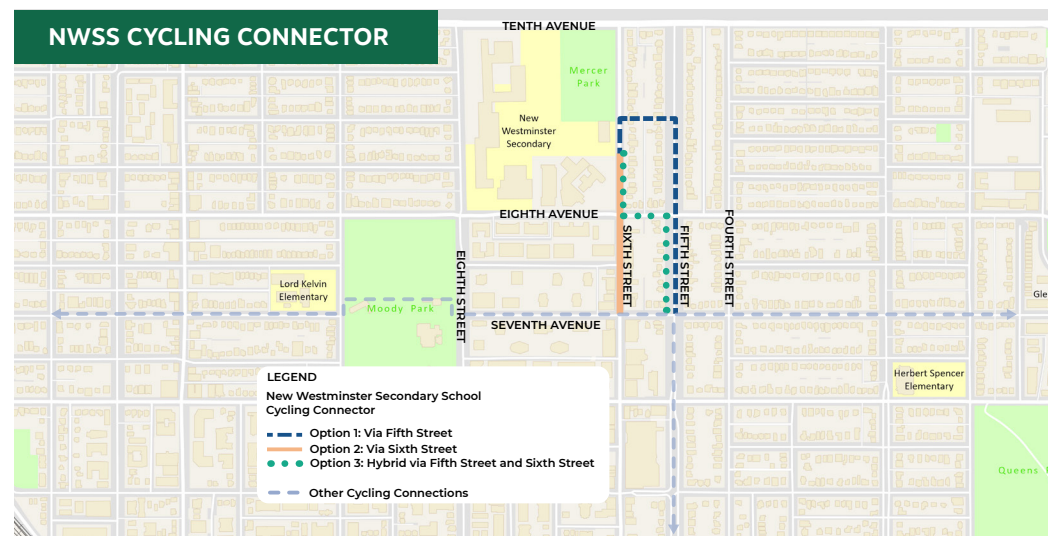


1 INTRODUCTION

In September 2021, the City of New Westminster launched the design and public engagement process for the Uptown Active Transportation Improvements. This includes two exciting projects that will reimagine some of our streets to make it easier for people of all ages and abilities to move comfortably and safely on foot, bicycle, and other non-motorized wheels in Uptown. These two projects include:

- The **Rotary Crosstown Greenway (RCG) Upgrades** on Seventh Avenue will improve safety, comfort, and accessibility on the section of the existing RCG along Seventh Avenue, between Eighth Street and Fifth Street. These upgrades will replace the interim bike lanes installed on Seventh Avenue in 2017.
- The new **New Westminster Secondary School (NWSS) Cycling Connector** project will link the RCG on Seventh Avenue to the east entrance of the new school on Sixth Street, enabling students, staff and visitors to conveniently and safely cycle, wheel or walk to the school from the east and the south.

These projects take direction from the vision set in the City of New Westminster's Council-approved Uptown Streetscape Vision, to create "a vibrant commercial district with a distinct identity, accessible and sustainable transportation, and inviting and engaging public spaces that are welcoming to all people."





"The Uptown neighbourhood is an important residential and commercial area in New Westminster and in need of improved sustainable transportation infrastructure. The active transportation projects will significantly improve the cycling and walking experience and create safer and more accessible connections to other parts of the city."

- Mayor Jonathan Cote

PROJECT TIMELINE



November 2020

Uptown Streetscape Vision endorsed by City of New Westminster Council



Fall 2021

Phase 1 Engagement



Fall / Winter 2021

Technical Analysis and Select Preferred Options



Winter 2021

What We Heard Report for Engagement Round One



Winter 2022

Conceptual Design of Preferred Options



Winter 2022

Round Two Engagement on Conceptual Design of Preferred Options



Spring 2022

Detailed Design of Preferred Options



2022

Construction

The projects align with the City's Master Transportation Plan goals and 7 Bold Steps for Climate Action - specifically, Car-light Community and Quality People-Centred Public Realm.

Another key goal of the Uptown Active Transportation Improvements is to provide a crosstown greenway and connection to NWSS that is safer and more comfortable for people of all ages and abilities,

improves the cycling and walking experiences, as well as improves accessibility.

Additional goals include enhancing the public realm and tree canopy, seeking opportunities for green infrastructure, and reducing the impacts of motor vehicles and through traffic in this neighbourhood, while limiting the impacts to people who rely on on-street parking.



2 RAISING AWARENESS

A variety of marketing tools and promotional materials were used to raise awareness for the Uptown Active Transportation Improvements engagement and the opportunities for the public to participate in the process.

PROJECT VIDEO

A five-minute video was posted to the Uptown Active Transportation Improvements project page and the City of New Westminster's YouTube channel on September 23. The video provided an overview of the projects as well as the rationale and goals of the RCG Upgrades and NWSS Cycling Connector projects. The video shared three proposed design options for the RCG Upgrades and three routing options for the NWSS Cycling Connector projects. The video also referenced the opportunities available over the subsequent month to provide input on the designs and routing options.

The video received 231 views as of October 18.

DIRECT MAIL

Simultaneously with the engagement launch, a letter was distributed through Canada Post to 2,811 addresses within close proximity to the study area. The letter included important information about the Uptown Active Transportation Improvements, highlighting both the RCG Upgrades as well as the NWSS Cycling Connector projects, as well as a map outlining the area of the RCG improvements, and the proposed routing options for the NWSS Cycling Connector. There was a paragraph that



[Project Video](#)

described the projects and the engagement opportunities which was translated into Simplified Chinese, Vietnamese and Tagalog. Recipients were informed of the areas on which their feedback was sought as well as upcoming engagement opportunities including the online survey, interactive mapping tool, and upcoming pop-ups. Recipients were directed to the project website on Be Heard New West, the City's online public engagement space, for further information on the projects.

STAKEHOLDER OUTREACH

An email and letter were distributed to 22 impacted stakeholder groups during the week of September 22. The International Association for Public Participation (IAP2) defines stakeholders as any individual, group of individuals, organizations, or political entity with a stake in the outcome of a decision. For the Uptown Active Transportation

Improvements this includes the Uptown Business Association, Glenbrooke North Residents' Association, Moody Park Residents' Association, New Westminster Secondary School Parent Advisory Committee (PAC), Lord Kelvin School PAC, New Westminster Secondary School, School District #40, Century House, HUB Cycling, New Westminster Walkers' Caucus, New Westminster Schools Welcome Centre, New Westminster Homelessness Coalition Society, and others. A complete list of all stakeholders contacted is included in **Appendix A**.

The letter included an introduction to the projects and invited stakeholders to participate in an engagement event on the Uptown Active Transportation Improvements, which would provide the opportunity to learn about the proposed designs for RCG Upgrades, as well as proposed routes for the NWSS Cycling Connector. Invitations to engage with the project team in their preferred manner during the weeks of October 11 and 18 were extended. The email also referenced the project site and the engagement opportunities available there. All stakeholders received follow up emails and/or telephone calls.

Additionally, a "business walkabout" was conducted on October 5, where stakeholder letters were delivered to 24 businesses on Sixth Street between Seventh Avenue and Eighth Avenue. One-on-one connections were made with 19 owners, managers, or staff to ensure that they knew of the Uptown Active Transportation Improvements, the opportunity to provide input, and were invited to participate in a focus group.

SOCIAL MEDIA

The City's social media accounts were used to promote the project and engagement process. Two posts were published to the City's Facebook account, resulting in seven comments and two shares. In addition, four stories were published to the City's Instagram account along with one post. The City's Twitter account was also utilized, publishing two tweets during the engagement.



"A key goal of these projects is to provide a Crosstown Greenway and connection to NWSS that are safer and more comfortable for people of all ages and abilities. Providing safe routes to school is one of the fundamental aspects of encouraging more community members to choose active transportation."

- Councillor Patrick Johnstone, Chair of the Sustainable Transportation Advisory Committee

CITYPAGE

The Uptown Active Transportation Improvements process was regularly promoted in the City's weekly Citypage newsletter, which is distributed by email and also printed in The Record newspaper.

NEWSLETTER

Two newsletters were distributed to over 1,800 Be Heard subscribers on September 28 and October 14. The first newsletter went out on Sept. 28 to more than 1,900 emails, and the second went on Oct. 13 to more than 2,500 emails.

The newsletter provided information on a variety of projects, including the Uptown Active Transportation Improvements Projects, and an invitation to review the options, and provide feedback through the webpage or an upcoming Pop-Up event (both newsletters included blurbs on a variety of projects).

PROJECT POSTER

Interactive posters/signage were designed and distributed throughout the project area. The posters included a map of the project routes, a short description of the Uptown Active Transportation Improvements, and the URL to the Be Heard New West, as well as a QR code directly linking to the survey page.

These were installed at 20 carefully-selected locations along the project corridors.

Furthermore, several stakeholders including the New Westminster Secondary School, New Westminster Secondary



School Parents Advisory Council, members of the Walkers' Caucus, Multiculturalism Advisory Committee, Seniors Advisory Committee, Facilities Infrastructure and Public Realm Advisory Committee shared the poster with their networks. Additionally, several residents from Seventh Avenue took small posters from the pop-up on September 25 to post in their building lobbies to increase the likelihood of neighbours seeing them.

RAIN PAINT

Rain paint was another means by which the Uptown Active Transportation Improvements were promoted. By applying an “invisible” paint to the sidewalk using stencils which highlighted various forms of active transportation, meaningful conversations were sparked with those passing by. These rain paint activations were located near posters promoting the project, and further directed people to the project webpage. On October 7, 14 people were engaged (see engagement activities below) and either completed an engagement activity or were directed to the poster.



3 FALL 2021 ENGAGEMENT

3.1 Purpose of Engagement

The first round of consultation for the Uptown Active Transportation Improvements Project was held from September 22 to October 20, 2021. During this month-long period, the project team facilitated a series of activities to educate, excite, and spark conversations about the Uptown Active Transportation Improvements among community members.

Specifically, this round of engagement focused on:

- **informing** stakeholders and the public about the design options for RCG Upgrades and the routing options for the NWSS Cycling Connector
- **consulting** with stakeholders and the public to collect their feedback on their preferences on designs for RCG Upgrades and routes for NWSS Cycling Connector

3.2 Engagement Activities

The Vancouver Coastal Health region was in Stage 3 of British Columbia's Restart Plan during this engagement. As such, engagement opportunities were online as well as in-person, via outdoor pop-ups.

Community members were directed to the Uptown Active Transportation Improvements project site on Be Heard to participate in the engagement process. As of October 25, the project page had received 584 unique visitors. The project site featured important background information,

including a detailed FAQ, as well as interactive tools to collect feedback.

A Question-and-Answer tool provided visitors to the website the opportunity to submit questions and receive a response from the City. An interactive mapping tool provided visitors the opportunity to indicate a specific location with a pin and submit their comment as it pertained to that particular location on the routes. Community members were also able to provide feedback through a community survey, or by sending an email.

There were also additional engagement opportunities beyond the tools available on the Uptown Active Transportation Improvements project page: Stakeholders were invited to provide feedback through a series of virtual stakeholder focus groups, meetings and interviews, two pop-up events in the project area, and a rain paint activity.



3.3 What We Heard

The following is a summary of the engagement that took place and a snapshot of what was heard.

All verbatim comments were recorded and can be found in the document library on the Uptown Active Transportation Improvements site or by clicking [here](#).

RCG UPGRADES DESIGN OPTIONS



- Option 1** | Protected Bicycle Lanes – Parking on 2 Sides of Street
- Option 2** | Protected Bicycle Lanes – Parking on 1 Side of Street and Additional Street Enhancements
- Option 3** | Shared Local Street Bikeway with Closure at Seventh Avenue and Sixth Street

Across engagement activities, including the pop-ups, survey, stakeholder focus groups and interviews, participants were asked to provide their preferences and feedback on different design options for the RCG and routing options for the NWSS Cycling Connector. The images below provide a description for each of the options referenced in the following summary.

NWSS CYCLING CONNECTOR ROUTING OPTIONS



- Option 1A** | Fifth Street On-Street Protected Bicycle Lanes
- Option 1B** | Fifth Street Off-Street Protected Bicycle Lanes
- Option 2** | Sixth Street On-Street Protected Bicycle Lanes
- Option 3 Hybrid Option** | Fifth Street to Eighth Avenue to Sixth Street – Combined On-street and Off-Street Bicycle Lanes



In total, over 225 people participated in the pop-up engagement events. Pop-up Number One at Moody Park engaged an estimated 78 people with most people staying for at least ten minutes to speak and share feedback. Pop-up Number Two engaged an estimated 148 people, many of whom were students from New Westminster Secondary School.

POP-UPS

Community members were invited to participate in two in-person pop-up events. The first was held on September 25 at Moody Park from 11:00 a.m. to 3:00 p.m. The second was held on October 14 at the New Westminster Secondary School where Sixth Street intersects with the school, from 2:30 to 6:30 p.m. The second pop-up also served as a targeted engagement for students, staff and administration at the high school.

In total, over 225 people participated in the pop-up engagement events.

For both pop-ups, tents, games and snacks were offered to make the event engaging and enticing for people of all ages. Sidewalk-level sites were selected to ensure that people with a diversity of mobility needs could easily participate. Safety measures were in place to ensure that activities were COVID-safe and accessible.

Boards provided information on the projects, including design options for the Rotary Crosstown Greenway Upgrades and route options for the high school Cycling Connector. Participants were invited to learn about and provide information on either or both corridors. For each project, a board outlined proposed options and a second board was designed to capture their preferred option, as well as additional comments and top priorities and concerns.

Key themes from the pop-up events include:

POP-UP INPUT: RCG UPGRADES

Design Options & Level of Support:

Of the three design options, **Option 2 received the most support** (selected by 65 participants), followed by Option 1 (38) and Option 3 (9).

Comments & Concerns by Design Option

Option 1 Concerns:

- Need for sufficient barriers so that cyclists don't get "doored" by cars
- Need for better paving and maintenance of sidewalks and roads
- Ensure any planters or greenery do not block cyclists' and pedestrians' vision and sightlines

Option 2 Concerns:

- Need to maintain sightlines around parked vehicles
- Loss of parking
- A need for accessible crossings at intersections
- Improvement of sightlines by having a bi-directional bike route on the side without parking
- Lower the bike lanes to level ground for accessibility

Option 3 Concerns:

- Not safe for all road users
- The cul-de-sac could also be on the west side

- Closing Seventh Avenue at Sixth Street to vehicles is a concern in terms of traffic flow
- Landscaping will be a key to success for this option

General Concerns

Parking and Driveways

- Many driveways exit onto Seventh Avenue and must cut across bike lanes to enter or exit
- Residents rely on the street parking close to their buildings
- Loss of parking and loading zones with strong car-oriented use on Seventh Ave
- Safety concerns with strong car-oriented use on Seventh Avenue

Accessibility for Seniors:

- Seniors have diverse mobility needs and rely on parking
- Design changes should consider the needs of those who cannot use modes of active transportation

User Conflict From User Groups Sharing Mixed Use / Multi Modal Pathways:

- Different users of this stretch of the greenway struggle to share the space in a way that is safe, accessible and functional for all
- Future improvements must consider the different priorities for each user group (e.g. safety, accessibility, functionality)
- Youth do not feel safe biking to school with this route because of the high stress with all the cars

Suggested Improvements

Safety:

- Enough space to protect cyclists from car doors
- User separation (especially for pedestrians and cyclists)
- Better lighting is needed
- Need for better sightlines for bikes and cars

Consider those with Diverse Mobility Needs:

- Levelled driveways to make it easier for mobility scooters to pass smoothly
- Longer crossing times for those who move at a slower pace

Improvements to the Broader Area:

- Crosswalk at Princess Street
- Traffic management between Fourth and Fifth Street
- Improvements to the cycling connections from Moody Park

Street Condition and Maintenance:

- Potholes and cracks in the pavement deter cyclists

POP-UP INPUT: NWSS CYCLING CONNECTOR

Route Options & Level of Support:

For the NWSS Connector, participants were asked to indicate their preference on Option 1, 2 or 3. The options were not further broken down into 1A and 1B. **Option 2 received the most support** (55 participants). Option 1 received the second most support (24 participants), and Option 3 received the least support, with (13 participants).

Priorities for the NWSS Connector:

- The primary priority for the NWSS Connector was safety. Specifically:
 - Design should prioritize student safety
 - At intersections (especially Fifth Street and Eighth Avenue)
 - Features including: clear signage, safe connections, wide enough lanes for all users, distinct separation between different modes of transportation
- Other priorities included:
 - Choosing a desirable route that will be used by the community
 - Choosing a direct route
 - Linking the NWSS Cycling Connector to other key destinations such as Mercer Stadium or to bike paths that connect to Burnaby and beyond

Comments & Concerns by Routing Option

Option 1 Concerns:

- Potential challenges in sharing the laneway with existing residents, as well as other users of the lane (i.e., service vehicles and waste management)
- Potential conflict and increased volume in the laneway
- Indirect route that would not always be used as people would take shortcuts
- Parking and effective garbage pick-up is needed on Fifth Street
- New development will stress the area even further
- Safety at Fifth Street and Eighth Avenue intersection

Option 2 Concerns:

- Consistency and reliability of transit
- Loss of parking for businesses along Sixth Street
- Impact on transit
- Increase in traffic congestion
- Safety issues at driveways and drive-throughs

Option 3 Concerns:

- Congestion along Eighth Avenue
- Congestion along Sixth Street
- High-traffic area, not suited for cyclists and pedestrians in its current state

Overall, participants were pleased to have the opportunity to engage, and sought to ensure that key user groups along the route were engaged through Round 1 Engagement.



STAKEHOLDER MEETINGS

In total, the project team was involved in five stakeholder meetings – hosting three focus groups/interviews and joining in two pre-existing meetings. There was representation from Uptown Business Association, the City of New Westminster's Seniors Advisory Committee, the City of New Westminster's Facilities, Infrastructure and Public Realm Advisory Committee, seniors, Greater Vancouver Family Services, School District #40, and the New Westminster Secondary School Parents Advisory Committee.

Stakeholders were presented with background information on the goals of the Uptown Active Transportation Improvements. The project team presented three design options for RCG Upgrades, explained the trade-offs for each, and asked participants to indicate their preferred option. Similarly for the NWSS Cycling Connector, the project team presented three routes (one with two design options), explained the trade-offs for each, and then asked participants to indicate their preferred option.

PREFERRED OPTIONS BY STAKEHOLDER MEETING:

Stakeholder Meeting #1: Walkers Caucus Meeting

Five participants attended a meeting held on October 12, all of whom were seniors, pedestrian advocates and either experienced or had friends or loved ones who experienced mobility barriers. Some lived directly along the routes.

Preferences for the **RCG** design options during the meeting included:

One participant preferred **Option 1**, one participant preferred both **Option 2 and 3**, and one participant preferred **Option 3**.

For the **New Westminster Secondary School Connector**, **Option 2** was preferred as a group.

This stakeholder group followed up with a letter to Mayor and Council with general support for the project and their concerns and comments have been included in the stakeholder feedback below.

Stakeholder Meeting #2: Business Interview

One participant representing the Uptown Business Association completed a focused interview on October 13.

Preference for the **RCG** design options was **Option 2** and lack of support for Option 3.

Preference for the **NWSS** Connector was **Option 3** and lack of support for Option 2.

Stakeholder Meeting #3: Sustainable Transportation Advisory Committee Meeting

Eighteen participants, including a City Council member, community members, representatives of local groups and organizations, persons with lived experiences as well as seven City staff attended a meeting held on October 13.

In the interests of time, participants chose to share comments and concerns about each design and routing option, rather than to identify their preferences.

Stakeholder Meeting #4: Community Focus Group

Five participants including a representative of the Seniors' Advisory Committee, two representatives of the Facilities Infrastructure and Public Realm Advisory Committee, one representative of Family Services of Greater Vancouver, and one representative formerly on the Access Ability Committee attended a focus group on October 14.

Preference for the **RCG Upgrades** was for **Option 1** with 3 of 5 people preferring this option. However, two participants selected multiple options and Option 2 was preferred by two participants. Only one participant selected Option 3.

For the **NWSS Cycling Connector** preference was for **Option 1B** with three supporters. Once again, several participants chose multiple options and Option 2, which was preferred by two participants, was a close second.

Stakeholder Meeting #5: School Focus Group

Two participants including NWSS PAC and School District 40 attended this meeting on October 19.

Preference for the **RCG Upgrades** was tied between **Option 1** and **Option 2**.

For the **NWSS Cycling Connector**, the preferred option was again tied between **Option 1** and **Option 2**.

Five Stakeholder meetings were held with representation from the Uptown Business Association, Greater Vancouver Family Services, School District #40, the New Westminster Secondary School Parents' Advisory Committee, as well as members of the Walkers' Caucus, the City of New Westminster's Sustainable Transportation Advisory Committee, and a member of the City of New Westminster's Seniors' Advisory Committee, and the City of New Westminster's Facilities, Infrastructure and Public Realm Advisory Committee

RCG UPGRADES: ADVANTAGES & CONCERNS

Option 1 (protected bicycle lanes – parking on 2 sides of street):

Concerns:

- **Not very different** to what already exists
- The telephone **poles are in the way**
- **As a commuter, does not work**

Advantages:

- This option **avoids concerns around eliminating parking** – lots of seniors live in these buildings and visitors rely on street parking
- New Westminster is getting denser, people are coming from further away, resulting in concerns about parking access to Moody Park

Option 2 (protected bicycle lanes – parking on 1 side of street and additional street enhancements):

Concerns:

- **Impact of removing parking on one side** – lots of seniors in the area, many who use HandyDART
- **Sightlines** – when travelling from garages and parked on the street, cars cannot see cyclists
 - Why not eliminate the parking on the north instead of the south side?
- North of Eighth Avenue, cars parallel parking on Sixth Street causing **bottlenecks**

Advantages:

- Option 2 & 3 are better for the **pedestrian experience** – it is a greenway and needs more trees and greenery
- Preference is for something with **separation and landscaping**

Option 3 (shared local street bikeway with closure at Seventh Avenue and Sixth street):

Concerns:

- This is a no-go to some stakeholders – it **limits access to Uptown**; supporting/improving pedestrian/cycling routes should **not come at the cost of limiting vehicle access**
- **Vehicle restriction** on Sixth Street and Seventh Avenue – concern about what will happen to traffic that leads right to Century House – there are a number of seniors
- Cycling perspective – adults are more comfortable sharing a roadway with drivers, but option 3 would **not be safe for kids**
- Traffic calming sometimes leads to **aggressive driving**
- Resident on Hamilton concerned about **rat running on Hamilton**, if cul-de-sac put in on Seventh Avenue
- Would go with Option 3 if there was another cul-de-sac at Fifth Street at Seventh Ave

Advantages:

- Really like the cul-de-sac as a legitimate way to calm traffic

Priorities for RCG Upgrades:

Traffic management

- Would like to see 30km/h speed limits on these streets
- Would like to see cul-de-sacs and other traffic calming measures along the length of RCG
- All of Seventh Ave needs traffic management

Pedestrian Experience/Greenery:

- You shouldn't be able to drive from Glenbrook Middle School to Moody Park - it's a greenway!
- These projects offer an opportunity to increase the greenery aspect of the Crosstown Greenway and enhance the experience of pedestrians

Safety Concerns at Specific Locations

- Sixth Street at Seventh Ave is particularly dangerous for pedestrians
- Currently, left turns off Seventh Ave onto Sixth Street on a bike are challenging

Other General Comments

- Don't call this a "bike lane improvement" **focus on "greenway"** so people don't immediately discredit it
- **Whatever you do, do it well**- Temporary paint installations do not work – you need to do much more with streetscapes so people can say "wow" – focus on leaving as much space as you can
- **Do not affect the volume of traffic that the road is able to handle**

- **Narrow width of streets** - there are many streets where two cars cannot pass each other
- New development at Rexall on Sixth **will involve wider sidewalks**
- **Elevating the curb** where on-street parking adjacent to the bikeway **comes at a cost**
- **Cars are important as well as parking** – consider the ages of the people in the nearby buildings; aging people are using cars and it is getting more difficult to drive and park in New Westminster
- **Areas need to be open enough to get out of cars**, so that you are not going into the bike lanes

NEW WESTMINSTER SECONDARY SCHOOL ROUTES ADVANTAGES & CONCERNS

Option 1 – 1A & 1B:

Concerns:

- **Indirectness** – will people use it?
- **Impacts on Fifth Street residents**
- **Dislike** for using the laneway
- **Dislike** for the **hook back**/dog leg routing
- **Concerns that no one will use this** / no student will use this option unless they live in this block (not direct)
- **Takes too long to implement**
- **Does not improve intersection of Sixth Street and Eighth Avenue** where there is massive student flow. The west side sidewalk is widely used - we need to protect pedestrians and separate them from bikes and scooters, etc.

Advantages:

- Love the idea of a bike route through the housing, good way to **connect the school community**
- It would be **good if bus loading could be moved away from Sixth and Eighth Avenue** – it is dangerous with too much going on here

Option 1A (Fifth Street on-street protected bicycle lanes):

Concerns:

- **Businesses are not going to support** any options that involve **removing parking**

Advantages:

- None identified

Option 1B (Fifth Street off-street protected bicycle lanes):

Concerns:

- **Takes away space from the boulevard and development**

Advantages:

- **Least parking impact**
- **Safest** and still **allows for accessibility**
- This option is **the safest at nighttime** for students to be riding – there are people at home in their houses

Option 2 (Sixth Street on-street protected bicycle lanes):

Concerns:

- **Directness** – especially how students can access the space after school
- **Impacts on businesses**
- **Impacts on transit routes**
- **Businesses may not support any options that involve removing parking**
- **Transit vehicles stopping in the middle of the roadway**
- **Sixth Street** should be **enhanced more as a pedestrian corridor**, it is not friendly for cyclists overall
- This route should have a **wider sidewalk from the school entrance down to the corner of Sixth Street and Eighth Avenue**, with no bicycles allowed on the sidewalk in that location.
- **Sixth and Eighth intersection conditions should be**

reviewed- need for wider sidewalks, increased crossing time, and perhaps a scramble intersection

- **Annoying to be on the main road**
- **Concern about northbound streets crossing the bike lane** – keep off Eighth as students will ride up Fifth Street anyways

Advantages:

- **Traffic calming**
- **Most direct**, makes street **safer** for all users
- **Pedestrians will choose the most direct** possible
- **Don't mind if buses stop on the street and block traffic**
- Treats cyclists most like motor vehicles – this feels like it is **making bikes a normal part of the street**
- **Could serve both east and west movements**

Option 3 (Fifth Street to Eighth Avenue to Sixth Street – combined on-street and off-street bicycle lanes):

Concerns:

- **Few students will use**

Advantages:

- **Does not impact transit**
- **Combine with another option to the high school from the west**

Other General Questions/ Comments/Suggestions:

- **More street calming all the way down Seventh Avenue**
- **Block options for cars to rat run** from Eighth Street to Sixth Street via Seventh Avenue or Hamilton Street
- NWSS will connect to London Dublin Greenway, and **will**

be a part of a broader network

- **Missing connection from the west to the high school**
- **Jumping on Sixth Street vs Fifth Street does not make a difference at all from a commuter perspective coming from the east** – 1 & 3 are the preferred routes
- **Consider micro-mobility, scooters, etc.**
- Additional consideration for option 2 & 3 – should the buses stop where they are vs. **moving stops up to where the school is** (so 2-lane bike route would not affect transit on the east side of Sixth Street)
- **Questions around how many kids are using bikes?** And what season are they using them in?
- **Behaviour change component at NWSS critical** to the success of this project

Priorities across both corridors

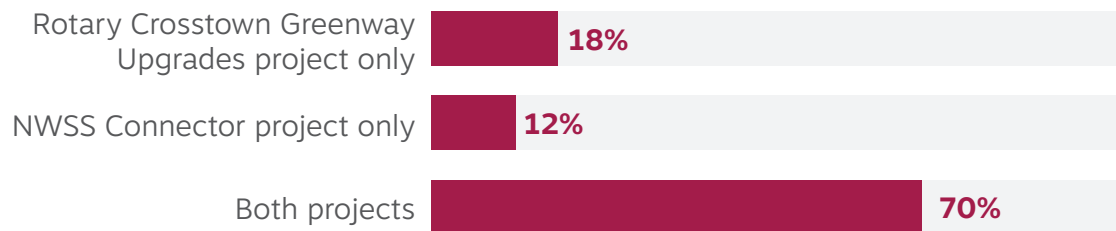
- **Pedestrians need more sidewalk space that is conflict-free** (from poles, street furniture, signage, careless parking and fast-moving quiet wheeled vehicles)
 - Legally permitting bicycles on sidewalks is an important pedestrian safety concern.
 - It is Important to account for growth in micro-mobility devices – these have the potential for conflict and are perceived as dangerous by pedestrians, seniors in particular.
- **Increased pedestrian safety at crossings** (for example the Seventh Ave and Sixth Street intersection, and the Seventh Ave and Eighth Street junction) – also referencing support for the proposed cul-de-sac on 7th, but that additional safety measures are still required

COMMUNITY SURVEY

An online survey was available on the project website between September 22 and October 17. The survey was designed to collect feedback on the design options for RCG Upgrades and the routing options for the NWSS Cycling Connector. The survey also collected input on the top priorities for each corridor, as well as demographic information.

In total, 308 people visited the survey with 264 of those visitors completing the survey.

Question 1: Please select which projects you would like to provide input on:



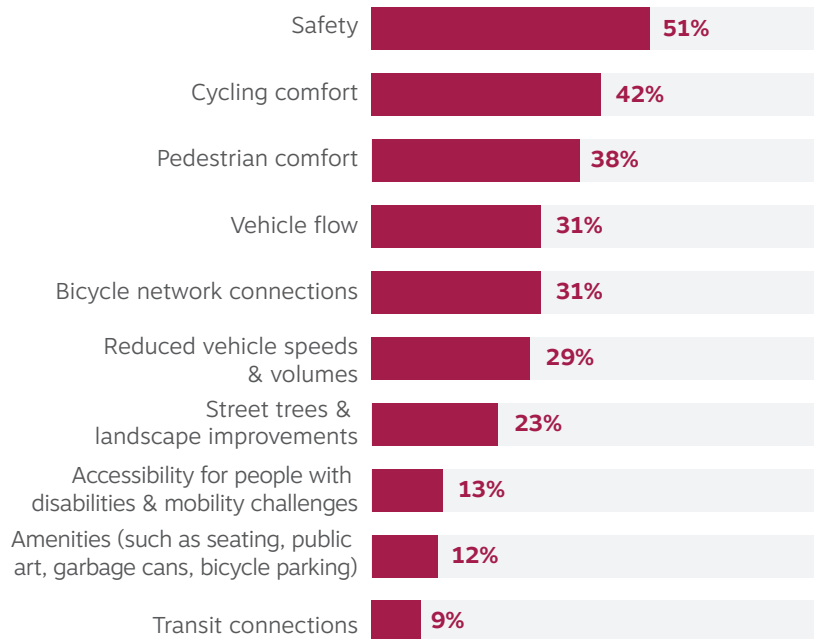
The majority of survey respondents (70%) provided input on both projects. 18% of survey respondents chose to only provide input on the RCG Upgrades, while 12% chose to only provide input on the NWSS Cycling Connector.



The online survey for the Uptown Active Transportation Projects saw 264 completed surveys with 70% of participants providing input on both the RCG Upgrades and the NWSS Cycling Connector

ROTARY CROSSTOWN GREENWAY

Question 2: What are your top three priorities for the improvements to this section of the Rotary Crosstown Greenway?

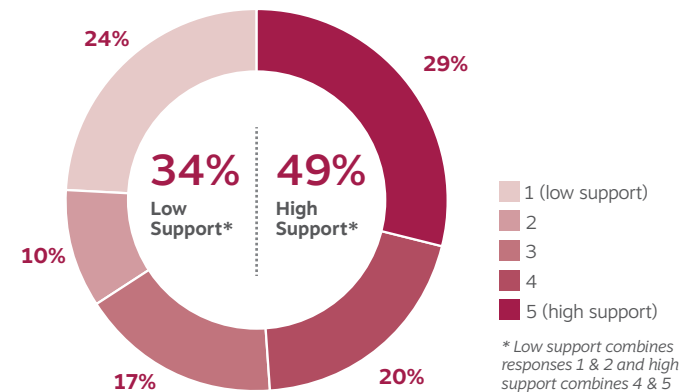


The top three priorities for survey respondents for the RCG Upgrades project include safety (51%), cycling comfort (42%), and pedestrian comfort (38%).

Of those who selected “other” the top comments included:

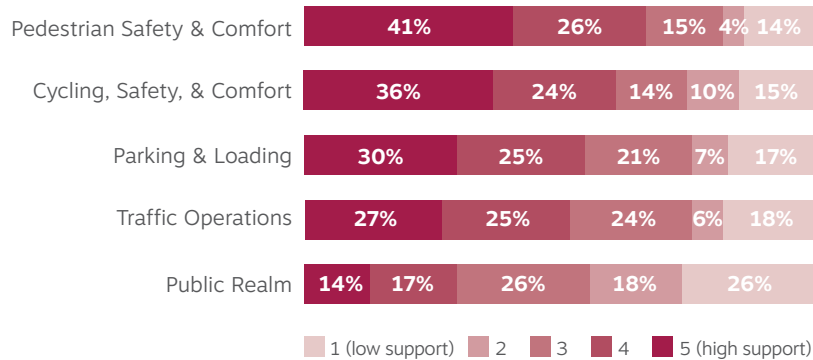
- Parking (8 comments)
- Landscaping and maintenance (5 comments)
- Pavement quality (2 comments)
- The cost (2 comments)
- Ensuring enough room for all users (2 comments)

Question 3: What is your level of support for Option 1?



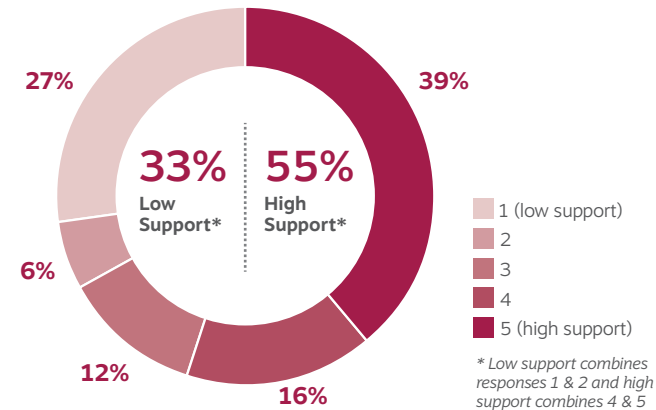
Almost half of survey respondents were supportive of Option 1, with 49% rating their level of support a four or higher.

Question 4: How well do you think Option 1 addresses the following key aspects:



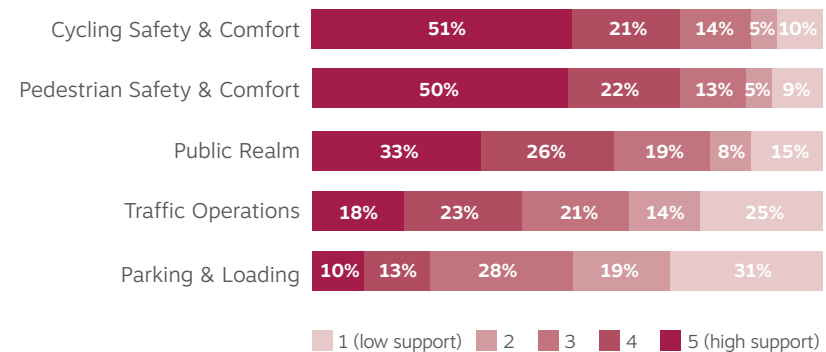
Based on the survey, respondents indicated that Option 1 (protected bike lane with parking on two sides) most effectively addressed pedestrian safety and comfort, with 67% rating their level of agreement a four or higher. Sixty percent (60%) of survey respondents indicated that Option 1 addresses cycling safety and comfort rating their level of agreement a four or higher. Respondents indicated that Option 1 is less effective at enhancing the public realm, with 31% rating their level of agreement a four or higher.

Question 5: What is your level of support for Option 2? Please rate your level of support from 1 (low support) to 5 (high support)



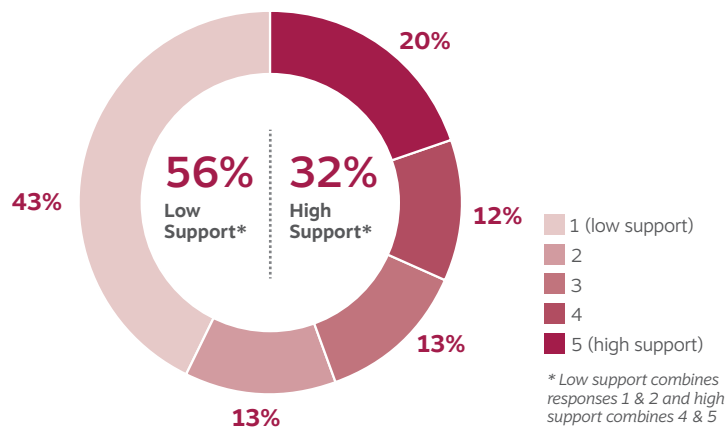
Over half of survey respondents were supportive of Option 2 (protected bike lane with parking on one side of street), with 55% rating their level of support a four or higher.

Question 6: How well do you think Option 2 addresses the following key aspects:



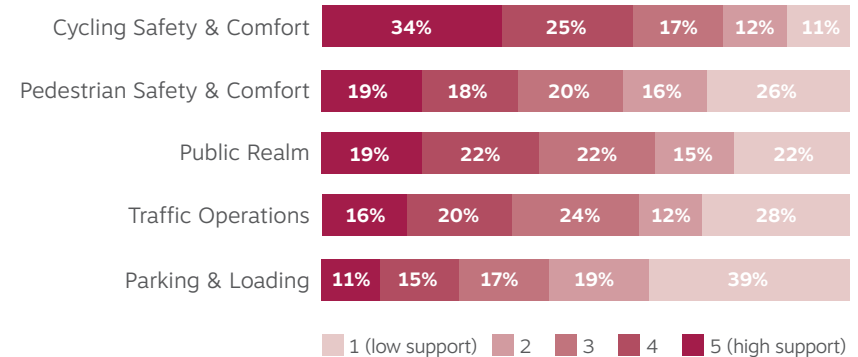
Based on the survey, respondents indicated that Option 2 (Protected Bike Lane with Parking on one side of street) addressed cycling safety and comfort as well as pedestrian safety and comfort (72% rating their level of agreement a four or higher). Respondents indicated that Option 2 does not address parking and loading as well, with only 23% of respondents rating their level of agreement a four or higher.

Question 7: What is your level of support for Option 3



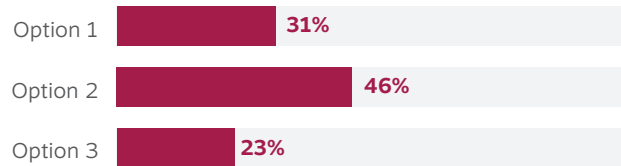
Less than half of survey respondents indicated high support for Option 3 (shared local streets bikeway with closure at Seventh Ave & Sixth Street), with 32% of respondents rating their level of support a four or higher.

Question 8: How well do you think Option 3 addresses the following key aspects:



Respondents to the survey indicated that this option supports pedestrian safety and comfort (59% rating their agreement a four or higher). Respondents indicated that this Option does not support traffic operations, with 26% indicating their level of agreement a four or higher.

Question 9: Of the three design options presented, which is your preferred option?



Based on the responses to this survey, the preferred option is Option 2 (46%), followed by Option 1 (31%), and Option 3 (23%).

Respondents were asked to elaborate on why they preferred the answer they chose. In total, 211 comments were received. A cross-tabulation analysis was conducted to identify the top comments for each option. The breakdown of these comments is below.

Option 1 (protected bike lane with parking on two sides):

- It maintains parking (30 comments)
- It maintains the flow of traffic (12 comments)
- It is the safest option for cyclists (12 comments)
- Balances all user needs (10 comments)
- It is the most accessible for diverse needs (seniors who require parking, more vulnerable users have separated lanes) (7 comments)
- It accommodates local resident needs (7 comments)
- It separates all user groups (6 comments)
- It is the least obstructive to the local area (5 comments)

Option 2 (protected bike lane with parking on one side of street):

- It is the safest option for cyclists (23 comments)
- Support for greenery (20 comments)
- It enhances the public realm (19 comments)
- It prioritizes active transportation (18 comments)
- Balances all user needs (12 comments)
- It separates all user groups (12 comments)
- It is the safest for pedestrians (11 comments)
- Support for widening the cycling lanes (10 comments)
- Cyclists are protected from car doors (6 comments)
- It supports climate goals (5 comments)
- It reduces traffic volumes (4 comments)
- It reduces traffic speeds (4 comments)
- It is the safest option for youth (3 comments)
- It maintains the flow of traffic (3 comments)

Option 3 (shared local streets bikeway with closure at Seventh Ave & Sixth Street):

- It reduces traffic volumes (16 comments)
- It reduces traffic speeds (14 comments)
- It would be the most effective as bikes currently do not use the cycling lanes (10)
- It maintains parking (6 comments)
- It is the safest for driveways and sightlines (5 comments)
- It is the safest option for cyclists (4 comments)
- It is the safest option for pedestrians (3 comments)
- It will eliminate trucks along the corridor (3 comments)

Respondents were asked if they had any additional comments regarding any of the three options. In total, there were 125 comments. While there were no significant overarching themes, summarized comments for each option are outlined below.

Option 1 (protected bicycle lanes – parking on 2 sides of street):

- It is the best compromise for all road users
- This option could benefit from the street closure as seen in Option 3
- This option risks cyclists being hit by doors
- Does not improve the public realm

Option 2 (protected bicycle lanes – parking on 1 side of street and additional street enhancements):

- This option could benefit from the street closure as seen in Option 3
- Support for the separation of users

Option 3 (shared local street bikeway with closure at Seventh Avenue and Sixth street):

- Opposition towards the closure at Seventh Ave
- This option would be the least safe for children and students
- This option only redistributes rat running, and does not solve the problem

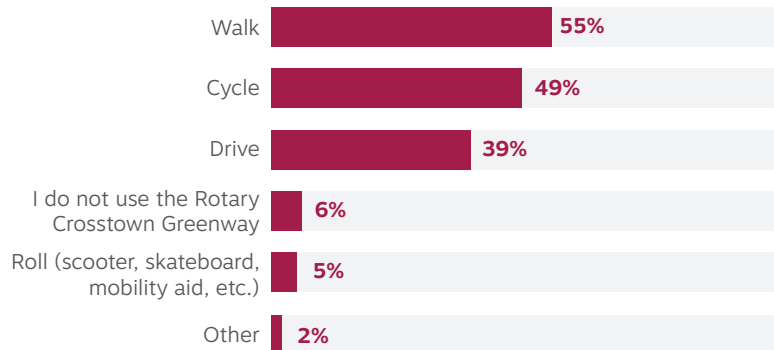
General comments that were provided include:

- The importance of maintaining parking for residents and businesses
- The need to better enforce cycling laws
- The need for a safety plan for e-bikes and scooters
- The importance of pavement quality for all options
- The safety issues regarding sightlines for parking
- The importance of traffic calming throughout the neighbourhood



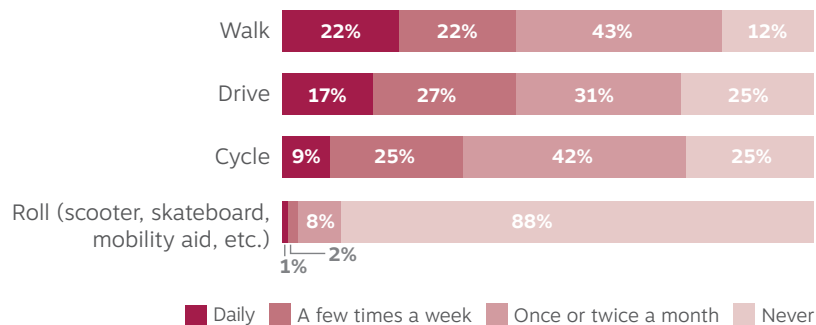
46% of Survey respondents preferred option 2 for the RCG Upgrades - with the top 5 reasons provided as follows: safety, cycling comfort, pedestrian comfort, vehicle flow, and bicycle network connections.

Question 10: How do you typically travel along the Rotary Crosstown Greenway between Fifth Street and Eighth Street?



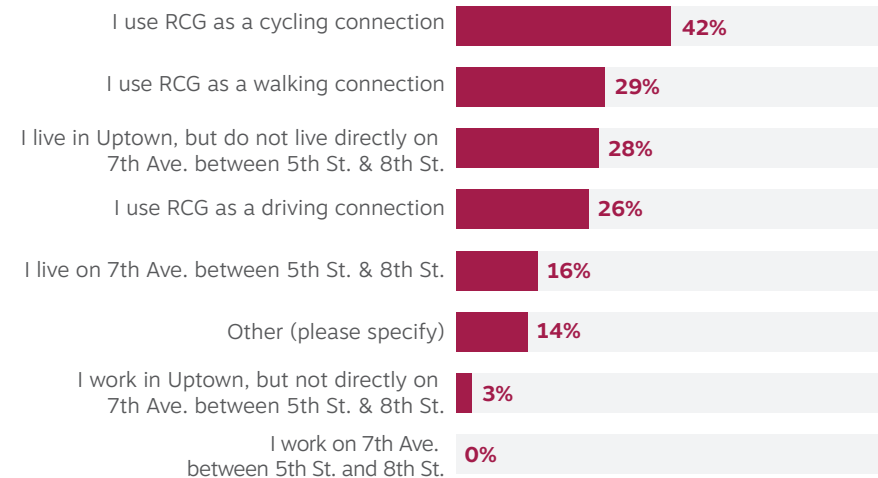
Respondents most often walk along the RCG (55%). The second most frequent transportation modes are cycling (49%) or driving (39%).

Question 11: How often do you use the Rotary Crosstown Greenway between Fifth Street and Eighth Street?



Based on the survey results respondents walk (22% walk daily) or drive (17% drive daily) most often. Nine percent of survey respondents cycle daily, and one percent roll daily.

Question 12: What is your connection to the Rotary Crosstown Greenway between Fifth Street and Eighth Street?



The most common connections to the RCG between Fifth Street and Eighth Street are:

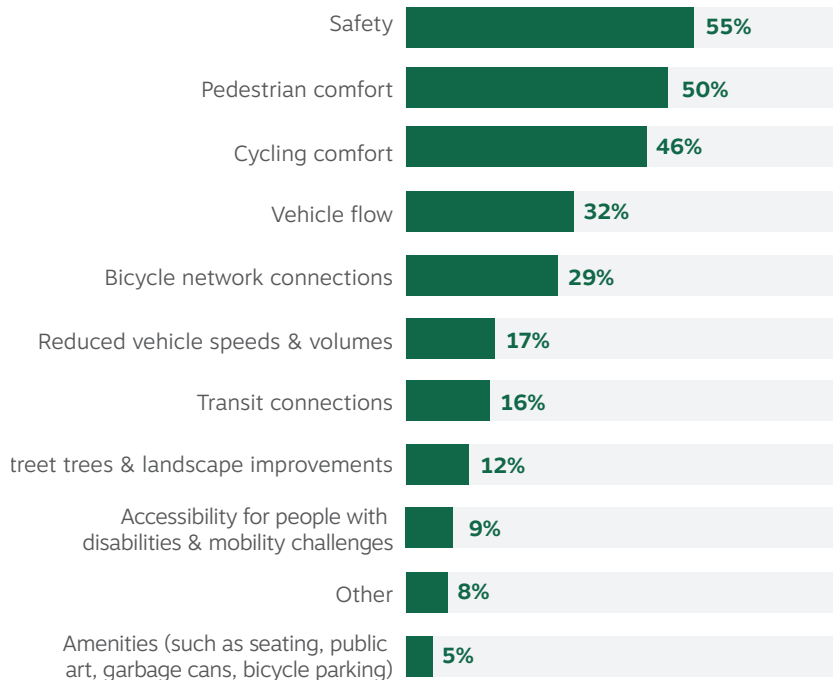
- Respondents use it as a cycling connection (42%)
- Respondents use it as a walking connection (29%)
- Respondents live in uptown but not on the direct route (28%)

Respondents were invited to comment on any other connections they have to this corridor. Top comments include:

- Visiting friends of family (5 comments)
- Working in the areas (3 comments)
- Use it was a walking connection (3)
- Visiting businesses in the area (2 comments)
- Use it for cycling exercise (2 comments)

NEW WESTMINSTER SECONDARY SCHOOL CYCLING CONNECTOR

Question 13: What are your top three priorities for the proposed New Westminster Secondary School Cycling Connector?

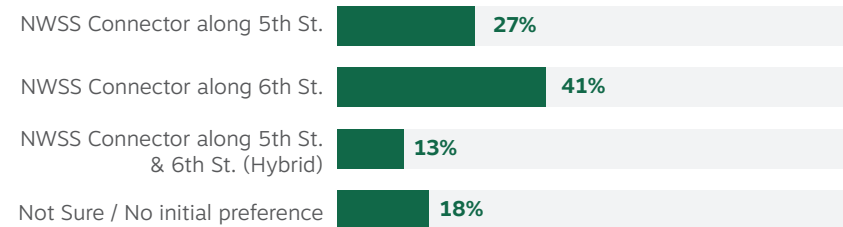


The top three priorities for the NWSS Cycling Connector include safety (55%), pedestrian comfort (50%) and cycling comfort (46%).

Of those who selected “other” the top comments included:

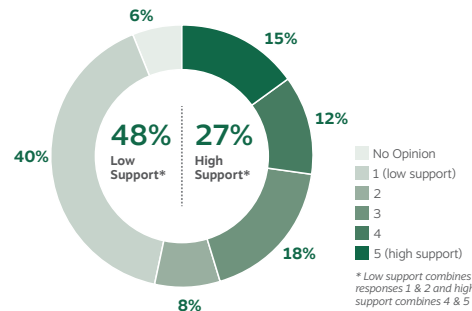
- Direct routing (5 comments)
- Maintaining parking for residents (4 comments)

Question 14: Before considering some of the key considerations and trade-offs, which are outlined below, what is your initial routing preference?



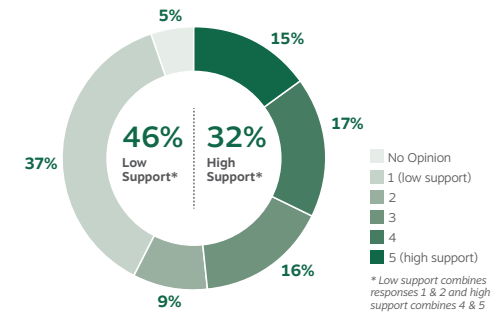
Before considering some of the key trade-offs, respondents preferred the NWSS Cycling Connector along Sixth Street (41%), followed by the route along Fifth Street (27%) and the hybrid route (13%).

Question 15: After reviewing some of the key considerations and trade-offs, what is your level of support for Option 1A: Via Fifth Street (On-Street Protected Bicycle Lanes)?



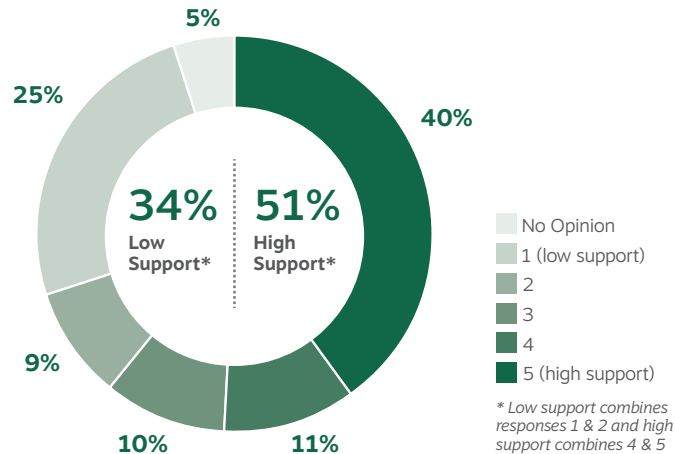
Based on responses to this survey, 27% of respondents indicated high support for Option 1A: Via Fifth Street, rating their support level a four or higher.

Question 16: After reviewing some of the key considerations and trade-offs, what is your level of support for Option 1B: Via Fifth Street (Off-Street Protected Bicycle Lanes)?



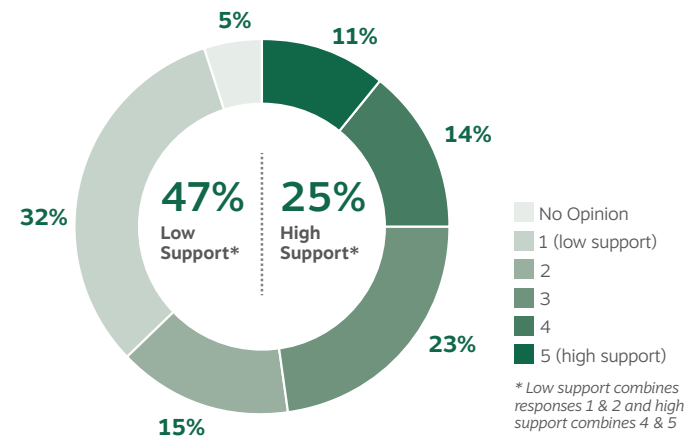
Survey respondents were slightly more supportive of Option 1B: Via Fifth Street, with 32% rating their level of support a four or higher.

Question 17: After reviewing some of the key considerations and trade-offs, what is your level of support for Option 2: Via Sixth Street?



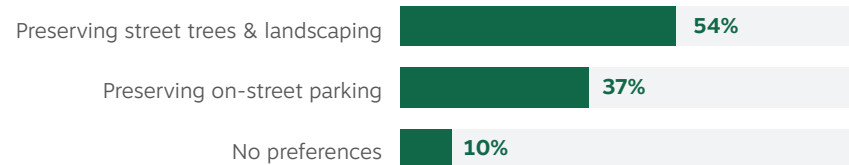
Over half of survey respondents were supportive of Option 2: Via Sixth Street, with 51% rating their level of support a four or higher.

Question 18: After reviewing some of the key considerations and trade-offs, what is your level of support for Option 3: Hybrid via Fifth Street and Sixth Street?



One quarter of survey respondents (25%) indicated high support for Option 3: Hybrid Via Fifth Street and Sixth Street.

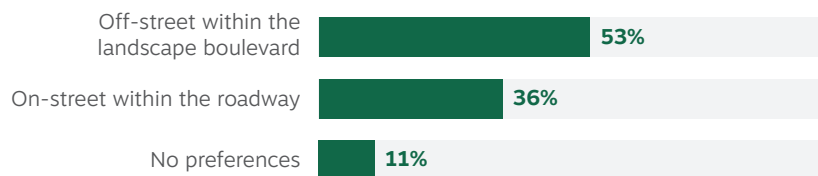
Question 19: Is preserving on-street parking or preserving landscaping and street trees more important to you?



Fifty-four percent (54%) of respondents indicated that

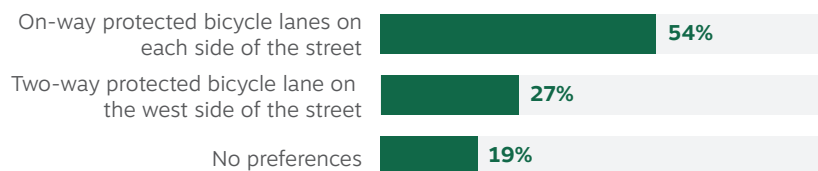
preserving street trees and landscaping were more important to them than preserving on-street parking (37%). Ten percent (10%) of respondents had no preference.

Question 20: Would you prefer that the cycling lane(s) be located on-street, within the roadway, or off-street, within the landscaped boulevard?



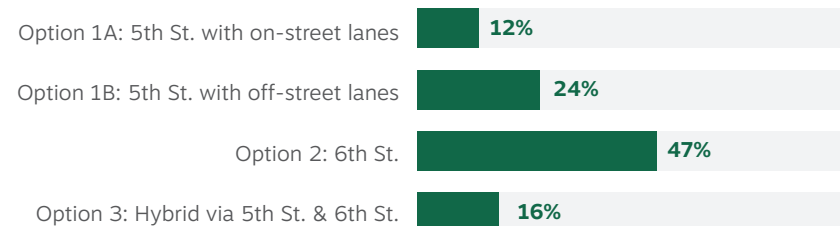
Over half (53%) of survey respondents preferred cycling lanes to be off-street within the landscaped boulevard, while 36% preferred them to the on-street within the roadway. Eleven percent had no preference.

Question 21: Do you have a preference for the configuration of the cycling lanes?



Over half (54%) of survey respondents preferred one-way protected bicycle lanes on each side of the street, whereas 27% preferred a two-way protected bicycle lane on the west side of the street. Nineteen percent had no preference.

Question 22: Given your understanding of the trade-offs and considerations of each option, which is your preferred option?



After learning of the trade-offs and considerations for each option, the routing options held similar levels of support. The preference was still Option 2: Sixth Street (47%), followed by Option 1: Fifth Street (12% for on-street lanes and 24% for off-street lanes), and Option 3: Hybrid Via Fifth Street and Sixth Street (16%).

Respondents were asked to elaborate on why they preferred the answer they chose. In total, 196 comments were received. A cross-tabulation analysis was conducted to identify the top comments for each option. The breakdown of these comments is below.

Option 1A (Fifth Street on-street protected bicycle lanes):

- Safest option for bikes (6 comments)
- Protects greenery (large trees, landscaping) (5 comments)
- Sixth Street is too busy with many users (3 comments)

Option 1B: (Fifth Street with off-street bike lanes):

- It maintains more parking (10 comments)
- Safer option overall (8 comments)

- It is already wide enough for multiple users (5 comments)
- Least impact on cars (7 comments)
- Least impacts to transit (4 comments)
- Safest option for bikes (4 comments)
- Least impacts to Sixth Street (3 comments)
- Maintains traffic flow (2 comments)
- Separates users well (2 comments)

Option 2 (Sixth Street off-street protected bicycle lanes):

- It is the most direct route (and would be used the most) (73 comments)
- Safer option overall (11 comments)
- It has the least impacts to residents (7 comments)
- It provides access to businesses (5 comments)
- It needs less new infrastructure (5 comments)
- It avoids the safety issues at Eighth Ave (5 comments)
- It will be the least expensive option (5 comments)
- It maintains parking for residents (6 comments)
- It avoids the conflicts in the laneways associated with Option 1 (4 comments)
- Sixth Street is wide enough for shared uses (3 comments)
- It will better connect to the broader cycling networks (3 comments)
- It is not dependent on redevelopment (2 comments)

Option 3 (Fifth Street to Eighth Avenue to Sixth Street – combined on-street and off-street bicycle lanes):

- It is also a direct route (9 comments)
- Safer option overall (4 comments)

- Least impacts to businesses on Sixth Street (3 comments)
- Sixth Street is too busy (3 comments)
- It would be a pleasant route (2 comments)

Respondents were asked if they had any additional comments regarding any of the four options. In total, there were 194 comments. Summarized comments for each option are outlined below.

Option 1

- With less traffic and wider streets, it is a safer option
- Support for increasing safety at the Fifth Street and Eighth Avenue Intersection
- Concerns with the safety of the alleyway route (mixing user groups could be unsafe)
- It is a less direct route
- Tree maintenance and power poles are potential hazards

Option 2 (Sixth Street off-street protected bicycle lanes):

- It is the most direct route
- It is not dependent on development
- Sixth Street is wide enough

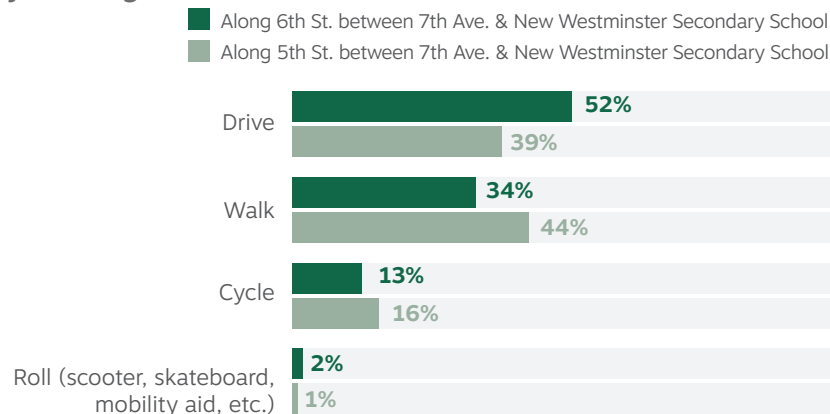
Option 3 (Fifth Street to Eighth Avenue to Sixth Street – combined on-street and off-street bicycle lanes):

- Least disruptive to transit and drivers
- Is still a direct route for students
- There are too many dangerous intersections for cyclists
- Bike lanes along Eighth Avenue would not feel safe

General comments:

- There are not enough cyclists in New West to justify the costs
- The route should be chosen with students in mind
- The route should prioritize pedestrians
- The route should prioritize vehicle traffic and parking
- Consider the option with the least impacts to transit

Question 23: How do you typically travel along the following routes?

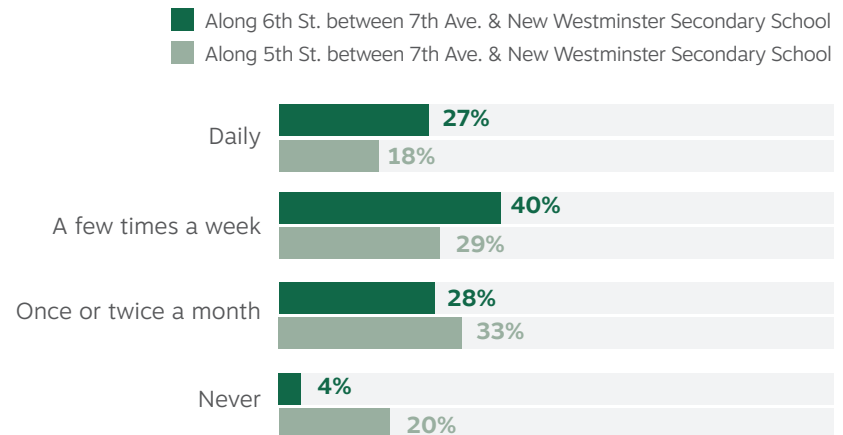


Fifty-two percent of survey respondents typically drive along Sixth Street between Seventh Ave and NWSS, whereas 34% typically walk and 13% typically cycle.

Forty-four percent of survey respondents typically walk along Fifth Street between Seventh Ave and NWSS, whereas 39% typically drive, and 16% typically cycle.

Approximately 1% of respondents roll along either route.

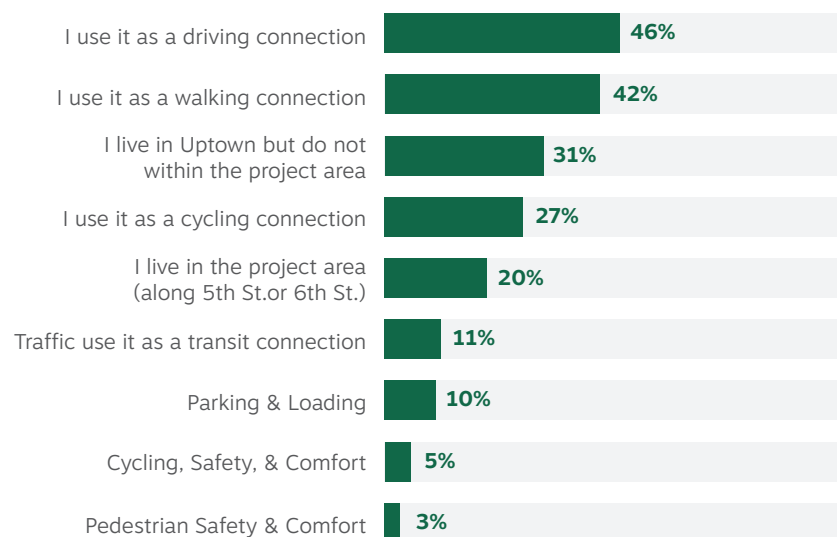
Question 24: How often do you typically travel along the following routes?



Survey respondents travel along Sixth Street between Seventh Ave and NWSS more often, as 27% indicated they travel this route daily and 40% indicated they travel along this route a few times per week.

In comparison, 18% of respondents travel along Fifth Street between Seventh Ave and NWSS daily, whereas 29% indicated that they travel along this route a few times per week.

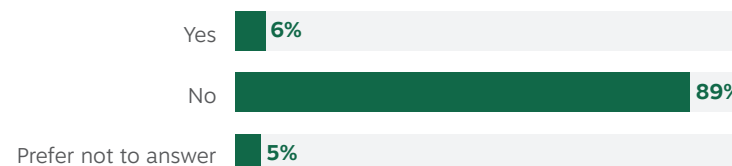
Question 25: What is your connection to Fifth Street and Sixth Street between Seventh Avenue and New Westminster Secondary School?



The most common connections to Fifth Street and Sixth Street between Seventh Avenue and NWSS are:

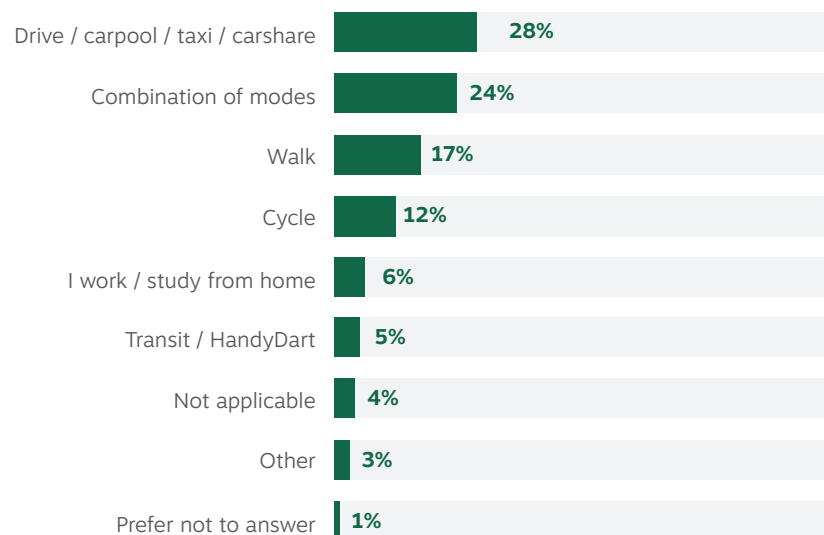
- Respondents use it as driving connecting (46%)
- Respondents use it as walking connection (42%)
- Respondents live in uptown but not within the project area (31%)

Question 26: Do you have any limitations that impact your mobility? (e.g., use a wheelchair or scooter)



The majority of respondents (89%) do not have limitations that impact their mobility.

Question 27: What is your usual mode of transportation for your daily weekday routine (e.g., commute to work or school)?



The most common mode of transportation based on survey responses is driving (28%), followed by a combination of modes (24%) and walking (17%).

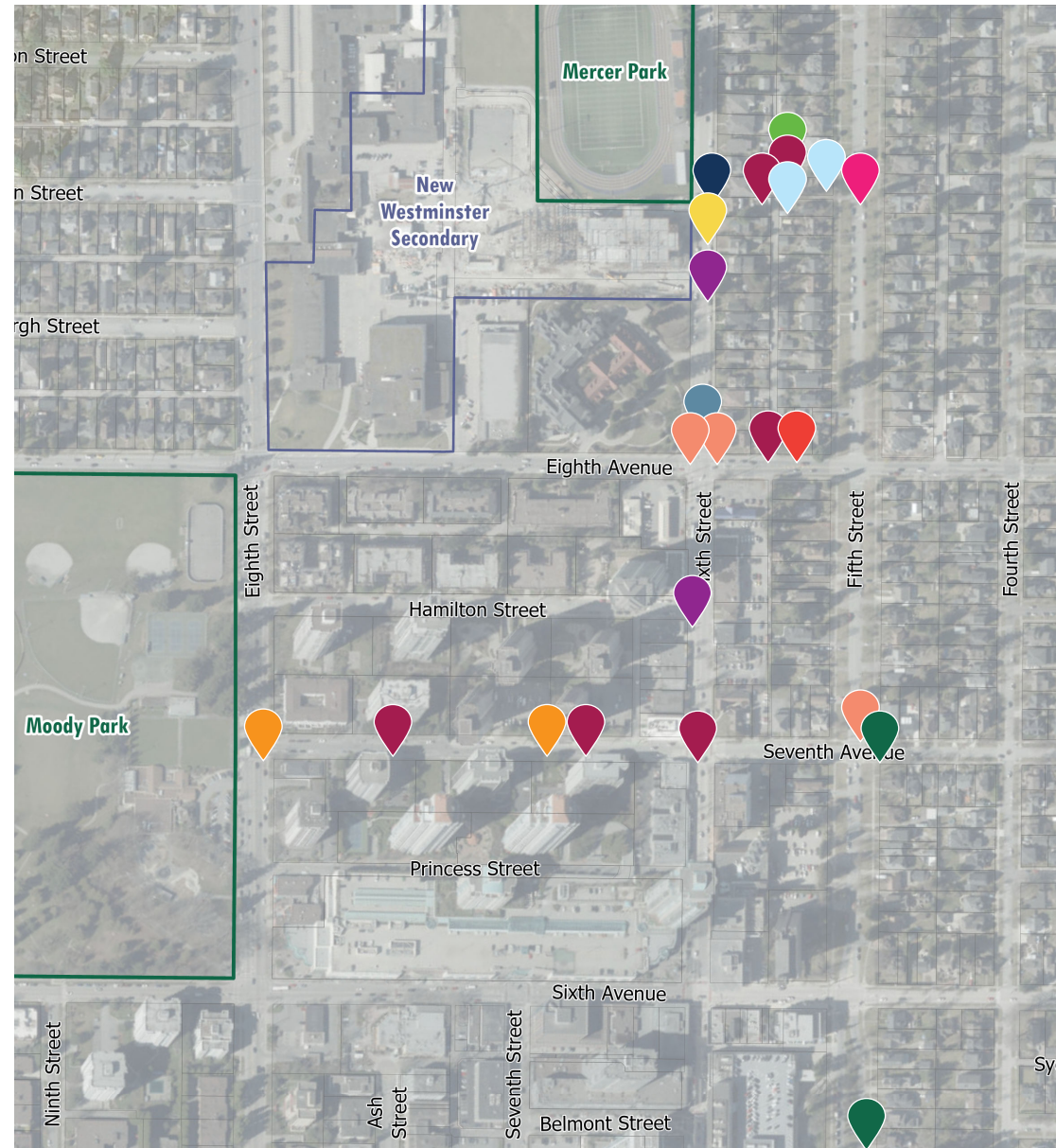
INTERACTIVE MAP

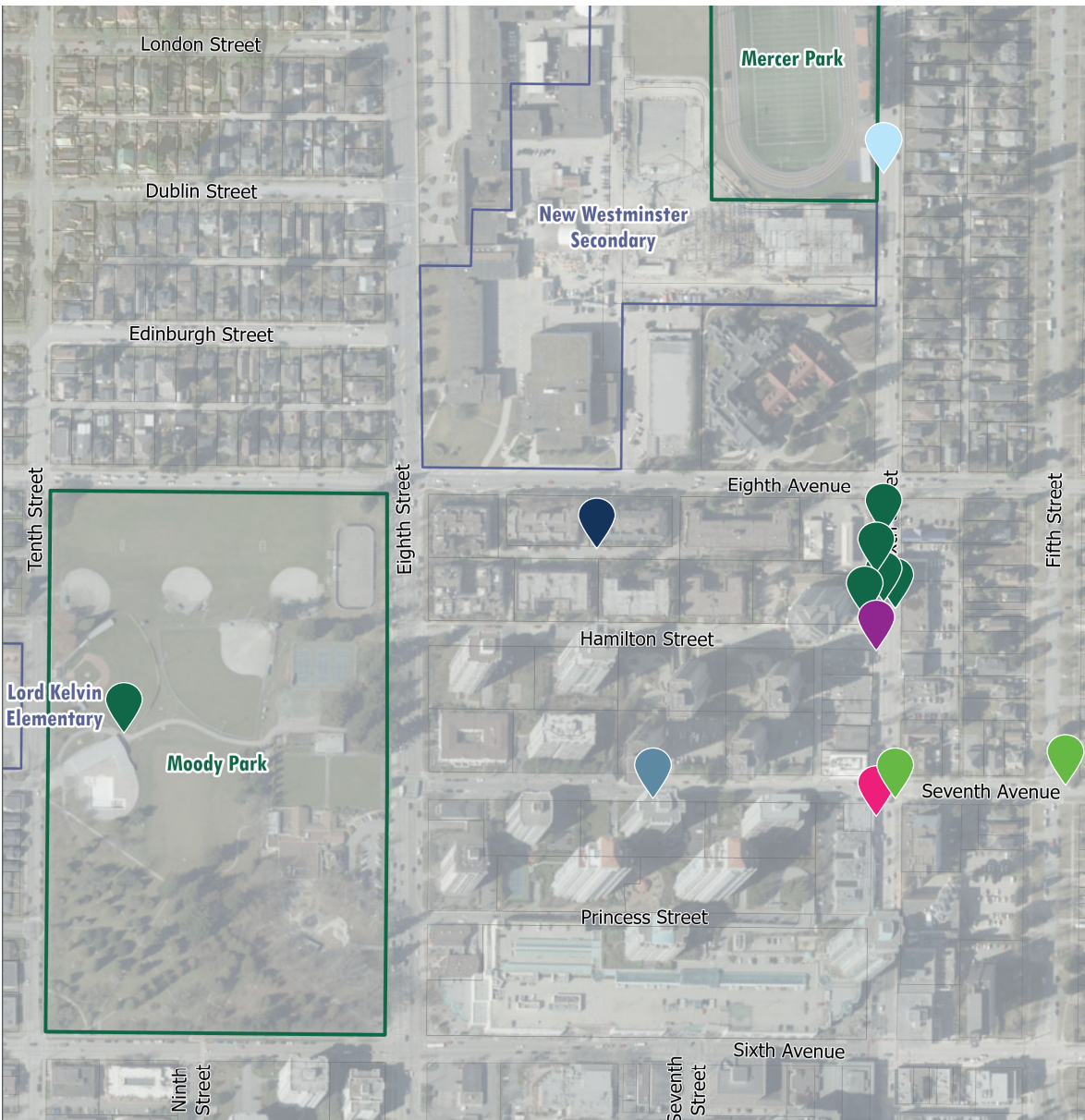
Community members were able to contribute their feedback through the Interactive Mapping Tool submitting 50 pins from 36 visitors and 15 unique contributors between September 22 and October 17.

The main themes of the comments are included on the following pages, along with the corresponding locations. The full comments are available in the verbatim document.








CONCERNS

-  Sight Lines/Blind Spots
-  Intersections
-  Speed/Rush Hour
-  Alley & Mixed-Use Traffic
-  Narrow Width of Street or Alley
-  Due to transit stops
-  General lack of safety for bikes
-  Pedestrian Lights
-  Indirectness of Route
-  Disadvantages of bi-directional bike path
-  Difficult driving areas
-  Other





IDEAS

-  Directness of Route (support for Sixth Street route for NWSS connector)
-  Closures to Vehicle Traffic
-  Continued Connections
-  Visibility Improvements
-  Crosswalk improvements
-  Width of street provides room for designated bike lane
-  Suggestions for Left Turn at 6th Street and 6th Avenue

RAIN PAINTING

On October 7, 14 people expressed interest, and five people engaged and provided input on the Uptown Active Transportation Improvement projects during a rain painting session where an invisible paint was applied to the sidewalks using stencils which highlighted various forms of active transportation. A series of speaking points, printed maps showing the NWSS connector routes, and renders of the RCG options as well as voting sheets were prepared and visible to those passing by. Five people stopped for discussion, three indicated their preferred options for the RCG and NWSS Cycling Connector projects, and two voted solely on the NWSS Connector options.

RCG Upgrades

1 Participant preferred Option 1 (Protected Bike Lane with Parking on 2 sides of street)

1 Participant preferred Option 2 (Protected Bike Lane with Parking on 1 side of street)

NWSS Cycling Connector

4 Participants preferred Option 2

1 Participant preferred Option 3

The additional nine people who were interested but did not indicate their preference, were directed to a nearby poster which promoted the project and provided the URL for the website and QR code for the survey.

BUSINESS WALKABOUT

Stakeholder letters were delivered to 24 businesses on Sixth Street between Seventh Avenue and Eighth Avenue. One-on-one connections were made with 19 owners, managers or staff to raise awareness for the upcoming engagement activities.

Key themes that were heard throughout the walkabout include:

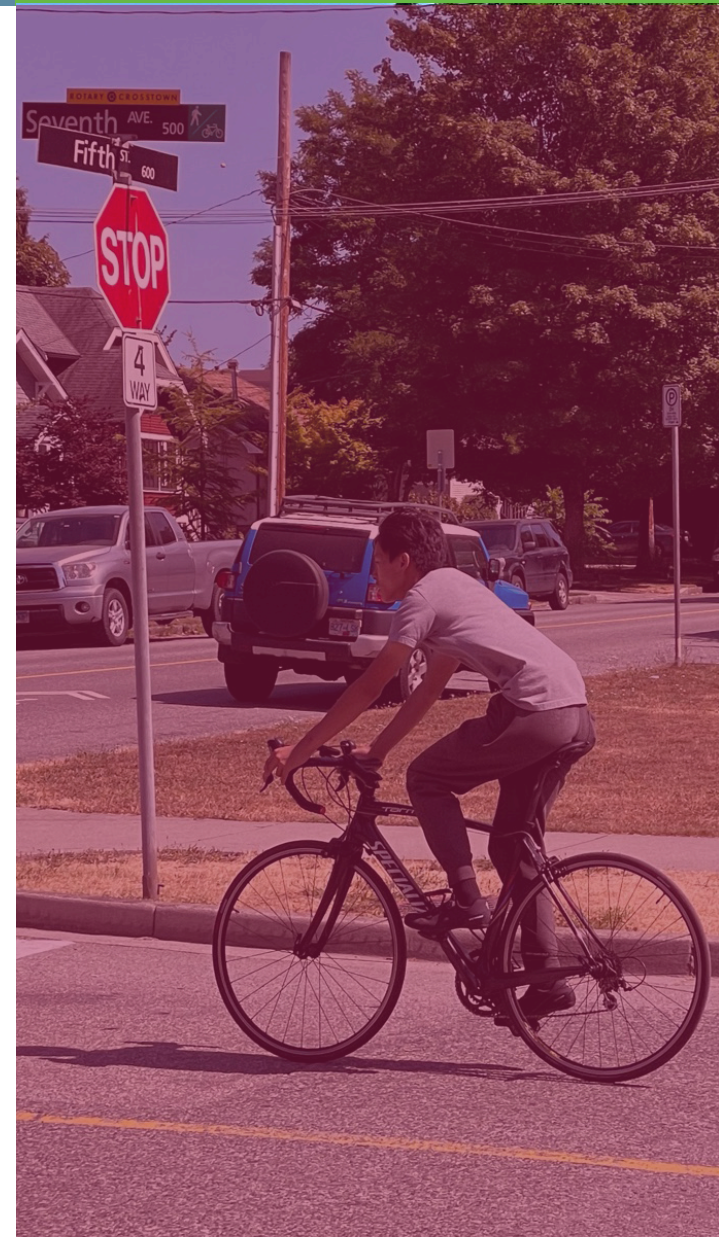
- Issues with safety along the corridor (theft and crime)
- A lack of adequate street lighting for many of the businesses
- Issues with rideshare deliveries making illegal turns
- Issues with motor vehicle speeds
- The importance of parking for customers along Sixth Street
- The dangers for large trucks or deliveries and poor sightlines along the RCG

4 NEXT STEPS

The input collected through the engagement for the Uptown Active Transportation Improvements has been an invaluable part of establishing the level of support for the design options for RCG and routing options for the NWSS Cycling Connector, as well as which of these is preferred by stakeholders and public participants.

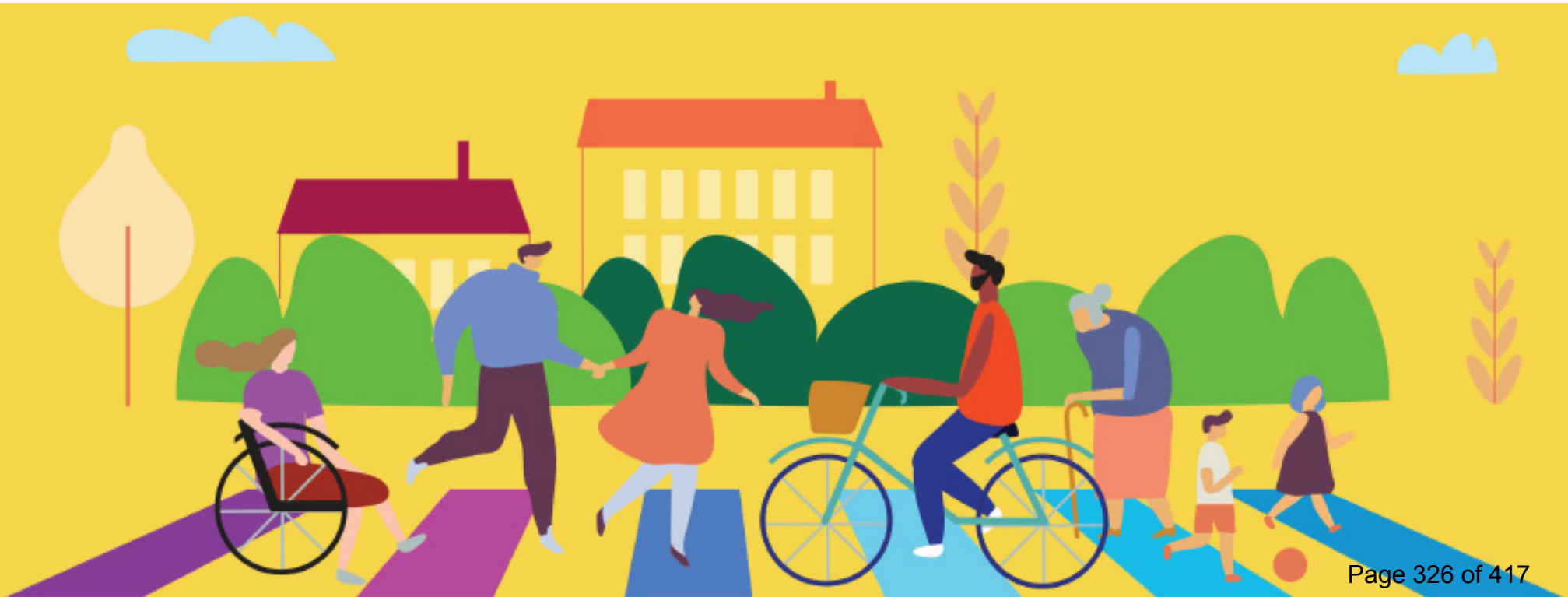
With round one of public engagement now complete, the project team will now use this information as well as their technical analysis to select a preferred route for the NWSS Cycling Connector and a preferred design for RCG. The preferred route and design will be presented to New Westminster's Sustainable Transportation Task Force for their endorsement. There will be a second round of public engagement early in 2022, that will present design options for the preferred NWSS route as well as a more detailed design for RCG.

We would like to thank all community members for their participation and valued input into the design process! We look forward to sharing more with you and your continued involvement.



There will be a second round of public engagement early in 2022, that will present design options for the preferred NWSS route as well as a more detailed design for RCG.

APPENDIX A | STAKEHOLDER GROUPS



APPENDIX A: STAKEHOLDER GROUPS

1. Brow of the Hill Residents' Association
2. Century House Association
3. City of New Westminster Seniors' Advisory Committee
4. City of New Westminster Multiculturalism Advisory Committee
5. City of New Westminster Sustainable Transportation Advisory Committee
6. City of New Westminster Facilities, Infrastructure and Public Realm Advisory Committee
7. Creating Accessible Neighborhoods – New Westminster Member
8. Family Services of Greater Vancouver
9. Former member of the Access Ability Advisory Committee
10. HUB Cycling
11. Lord Kelvin School Parents' Advisory Committee
12. Uptown Business Association
13. Moody Park Residents' Association
14. New Westminster and Burnaby Walkers' Caucus
15. New Westminster Chamber of Commerce
16. New Westminster Homelessness Coalition Society
17. Glenbrooke North Residents' Association
18. New Westminster Local Immigration Partnership Council
19. New Westminster Secondary School
20. New Westminster Secondary School Parents' Advisory Committee
21. New Westminster Schools Welcome Centre
22. School District #40
23. Queen's Park Residents' Association

Attachment # 2

Uptown Active Transportation Improvements Round 1 Engagement Summary Graphic

UPTOWN

ACTIVE TRANSPORTATION IMPROVEMENTS

Engagement Round One by the Numbers

PROMOTION + ENGAGEMENT ACTIVITIES:

Social Media



- **2** Facebook Posts
- **4** Instagram Stories and **1** Post
- **2** Tweets

Stakeholders



- **2** Promotional Posters distributed to networks of **7** stakeholder groups
- Stakeholder Letters distributed to **22** impacted stakeholder groups
- Business Walkabout and Hand Delivery of **24** Letters and Invitations to Stakeholder Meetings
- **5** Stakeholder Meetings representing **10** different stakeholder groups

In-Person Engagement



- Interactive Posters – installed at **20** locations
- Rain Paint application to sidewalks
- **2** Pop Ups – **226** Participants

Letters & Newsletters



- Notification Letter sent to **2811** addresses
- **2** Newsletters – distributed to over **1800** Be Heard New West subscribers
- Regular Promotion in City's weekly Citypage newsletter – distributed by email and printed in the Record newspaper

Digital



- **1** Project Video – **229** views
- Uptown Active Transportation Improvements Project Site on Be Heard – **584** views
- Surveys completed – **264**
- Questions and Answers submitted on Project Site– **9**
- Questions submitted to transportation Email – **10**
- Pins on Interactive Map – **36**



NEW WESTMINSTER

Rotary Crosstown Greenway Upgrades

WHAT WE HEARD

Top 3 Priorities



Safety



Pedestrian
Comfort



Cycling
Comfort

Design Preference on Survey

Option 3 (Shared Local Street Bikeway with Closure at Seventh Avenue and Sixth Street)
23%

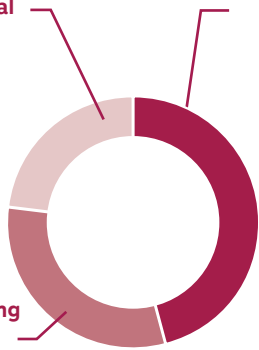
Reduces Traffic Volume, Reduces Traffic Speed

Option 2 (Protected Bicycle Lane – Parking on 1 Side of Street)
46%

Safest for Cyclists, Support for Greenery Enhances the Public Realm

Option 1 (Protected Bicycle Lane – Parking on 2 Sides of Street)
31%

Maintains Parking, Maintains Flow of Traffic, Safest option for Cyclists

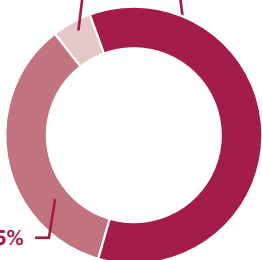


Design Preference at Pop-Ups

Option 3 - 5%

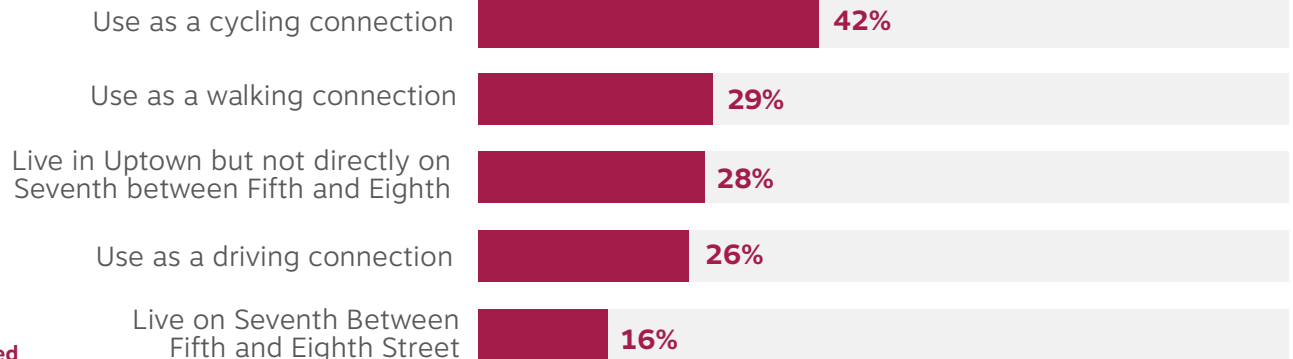
Option 2 - 60%

Option 1 - 35%



WHO WE HEARD FROM

How they are Connected to Rotary Crosstown Greenway



There are challenges with multi use pathways – cyclists treat pedestrians the way cars treat them

Make sure bike barrier is wide enough so people don't get doored

Additional bike parking

From Sixth Street and Eighth Avenue to Sixth Street and Belmont there are lots of seniors and disabled (blind or deaf), those living with mental health issues

Telephone poles along Seventh are in the way

Maintenance of the street - clean streets and condition of surfaces

SAFETY for pedestrians

For the cycle lanes to be well utilized additional space is needed for green infrastructure such as trees. Reducing parking on one side of the street is a small trade off to make for this benefit

Protected bike lanes are needed on this street as often there are cars parked in the lanes

Changing elevation of driveways throws off mobility scooters

New Westminster Secondary School Cycling Connector

WHAT WE HEARD

Top 3 Priorities



Safety



Pedestrian
Comfort



Cycling
Comfort

Route Preference (After Reviewing Trade-Offs) on Survey

Option 2 (Protected Bicycle Lanes via Sixth Street and Additional Street Enhancements- On Street) - Direct/ Will Get Used the Most; Safer, Least Impact to Residents

47%

Option 1B (Protected Bicycle Lanes Via Fifth Street – Off Street) - Maintains More Parking; Safer Overall; Wide Enough for Multiple Users

24%

Option 1A (Protected Bicycle Lanes Via Fifth Street – On Street) – Safest; Protects Greenery; Sixth Street is Too Busy

12%

Option 3 (Protected Bicycle Lanes - Hybrid via Fifth Street and Sixth Street – Combination of On-Street or Off-Street) – Reduces Traffic Volume, Reduces Traffic Speed, Most Effective as Bikes Currently do Not Use Cycling Lane

16%

WHO WE HEARD FROM

■ typically walk ■ typically cycle
■ typically drive ■ roll ■ n/a

Along Sixth Street between Seventh Avenue and NWSS



Along Fifth Street between Seventh Avenue and NWSS



Connection to Fifth Street and Sixth Street between Seventh Avenue and NWSS

Use as a Driving Connection

46%

Use as a Walking Connection

42%

Live in Uptown but Do not Live within the Project Area

31%

Use as a Cycling Connection

27%

Live in the Project Area

20%

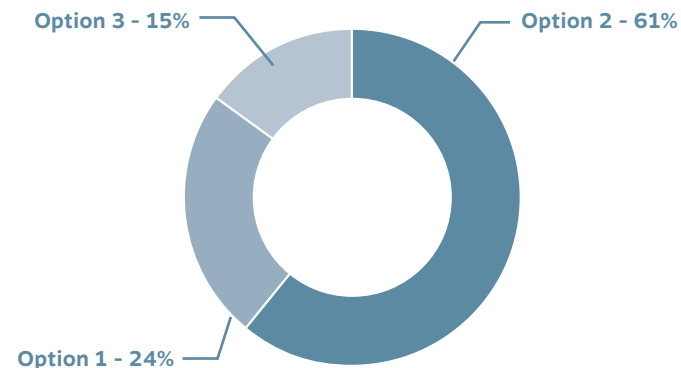
Use as a Transit Connection

11%

Use for Parking and Loading

10%

Route Preference at Pop Ups



54% of survey respondents indicated that preserving street trees and landscaping was more important than preserving on-street parking.

37% indicated that preserving on-street parking was more important than preserving street trees and landscaping.

53% of respondents preferred that cycling lane(s) be located off-street within the landscaped boulevard as opposed to on-street within the roadway

54% of respondents indicated a preference for one-way protected bicycle lanes on each side of the street as opposed to a two-way protected bicycle lane on the west side of the street

New Westminster Secondary School Cycling Connector

Many experienced cyclists don't use the protected bike lanes anyway, so let us plan for them to be on the road

Vehicle traffic on sixth street in the Uptown area is already restricted and slow moving much of the time Closing off a side street will not improve the flow of vehicle traffic

Businesses will not support cycling paths at the expense of loss of parking

Maintain greenspace

Safety – separation between bikes and pedestrians and cars

Places to hang out on Fifth (covered areas, trellis, greenway)

Ensure people use the route. Make it cool!

A Signal and traffic control is needed at Fifth Street and Eight Avenue

The biggest problem is high speed rat running

Extend bike paths into Burnaby

Bike lanes must be separated physically – there are many Seniors with visual/auditory barriers

I would like to see improvements to cycling and walking infrastructure, while also improving the public realm

Something simply has to be done to reduce volume and speed and there has been little enforcement up to now.

Roads are for travelling and parking – not for trees

Some people can't walk or cycle – we need parking and it just disappearing. Sixth Street is congested – you can't play with it any further!

I think closing the street to traffic at Seventh Avenue and Sixth Street would make turning left on to Eighth Avenue from Sixth Street much harder than it already is

There are skinny sidewalks on Fifth

Attachment # 3

Uptown Multiple Account Evaluation Memo

DATE: November 4, 2021
TO: Michael Leong
CC: Brent McMurtry, Barry Fan, Mike Anderson
FROM: Brian Patterson, Sarah Tremblay
FILE: 1274.0047.01
SUBJECT: City of New Westminster – Uptown Multiple Account Evaluation

1.0 INTRODUCTION

The City of New Westminster has launched the design process for the Uptown Active Transportation Improvements. This includes two projects that together will reimagine some of the City's streets to enable people of all ages and abilities to move comfortably and safely on foot, bicycle, and other non-motorized wheels in Uptown.

- The **Rotary Crosstown Greenway (RCG) Upgrades** will improve safety, comfort, and accessibility on the section of the existing Rotary Crosstown Greenway along Seventh Avenue, between Eighth Street and Fifth Street. These upgrades will replace the interim bike lanes installed on Seventh Avenue in 2017.
- The **New Westminster Secondary School (NWSS) Cycling Connector** project will link the Rotary Crosstown Greenway on Seventh Avenue to the east entrance of the new school on Sixth Street, enabling students, staff and visitors to conveniently and safely cycle, wheel or walk to the school from the east and south.

The City recently completed the first round of engagement for both of these projects, which focused on presenting three *design* options for the RCG Upgrades and three *routing* options for the NWSS Cycling Connector, as summarized below.

RCG Upgrades Project Design Options

Option 1

Protected bicycle lanes with parking on both sides of the street.



Option 2

Protected bicycle lanes with parking on one side of the street and additional street enhancements such as street trees, landscaping, and pedestrian amenities.

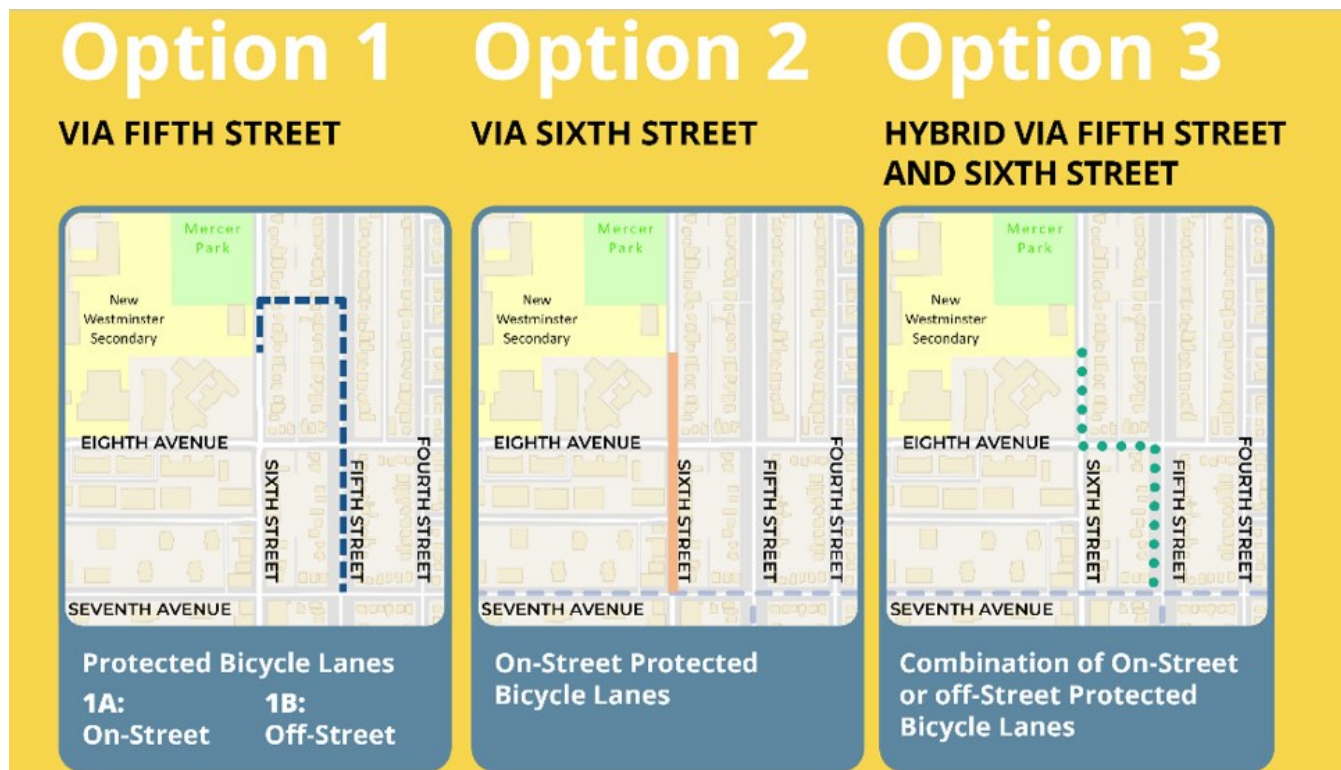


Option 3

Shared local street bikeway with street closure at Seventh Avenue and Sixth Street to reduce traffic volumes and speeds, plus protected bicycle lanes from Sixth Street to Fifth Street.



NWSS Cycling Connector Project Routing Options



Based on input from the first round of public engagement and a technical assessment of each option, a comprehensive evaluation framework was developed to identify the benefits and impacts of each option as it relates to a wide range of criteria. The intent of the evaluation framework is to assist the decision-making process by identifying the relative trade-offs between each option, and select the option that meets the most project objectives and provides the greatest benefit to the City of New Westminster’s residents.

Based on this evaluation, a preferred option for each project has been identified and will be recommended to New Westminster staff and Sustainable Transportation Task Force for endorsement. Following the confirmation of a preferred option for each project, the project team will advance a preferred conceptual design for each project.

2.0 EVALUATION FRAMEWORK

A comprehensive Multiple Account Evaluation (MAE) was developed for each option for each project. Criteria were developed based on the project goals and objectives outlined in the Project Charter, as well as additional criteria that were thought to be relevant. Each of the criteria in the evaluation framework were assessed on a scale of 1 to 5 both quantitatively and qualitatively, with the intent to highlight key trade-offs, benefits and impacts, relative to the other options. The scoring mechanism used was a range of colours that denote an outcome (very good, good, neutral / satisfactory, poor or worst) as shown in the image below. This assessment is not intended to quantify the results of the evaluation, but is instead intended to allow decision-makers to visually reach an understanding of the key trade-offs to inform a decision. The options were not assigned a score so as to avoid any aggregation of criteria and maintain key nuances.

DATE: November 4, 2021
SUBJECT: City of New Westminster – Uptown Multiple Account Evaluation

FILE: 1274.0047.01

Evaluation Framework Legend

Very Good
Good
Neutral / Satisfactory
Poor
Worst

2.1 EVALUATION CRITERIA

The evaluation criteria were selected based on the project's goals, as well as the City's Seven Bold Steps and broader societal objectives. The evaluation criteria for the framework are presented below.

- **Objective 1: Crosstown Cycling Route for All Ages and Abilities**
 - **Cycling Network Connectivity:** Establishes continuous connections to key destinations and for longer commutes for cyclists. Provides connection and accessibility to existing facilities and network.
 - **Cycling Safety and Comfort:** Extent of separation between people riding bicycles and motor vehicle traffic, based on facility type and buffer space. For lower speed and lower volume roadways, comfort may not require separation but measures to ensure lower traffic speed and volume is maintained.
- **Objective 2: Improved Walking Experience and Accessibility**
 - **Pedestrian Safety and Comfort:** Impact on pedestrian environment, including crossings, sidewalk conditions/width, buffer space, and conflict zones.
 - **Accessibility:** Impact on mobility and ease of use for those with mobility impairments.
- **Objective 3: Enhanced Public Realm and Tree Canopy Cover**
 - **Public Realm and Streetscape:** Impact on the overall streetscape and urban realm through the implementation of street furniture, lighting and interesting places, as well as integration with other land use planning initiatives.
 - **Tree Canopy Cover:** Implementation of new street trees to enhance the tree canopy cover
- **Objective 4: Maintain Transit Service and Operations**
 - **Impacts to Transit Operations:** Impact on reliable transit operations for buses and comfortable access opportunities for transit customers.
- **Objective 5: Accommodated Emergency Access**
 - **Impacts to Emergency Access:** Impact to emergency services vehicles resulting from roadway or traffic operation changes.

DATE: November 4, 2021
SUBJECT: City of New Westminster – Uptown Multiple Account Evaluation

FILE: 1274.0047.01

- **Objective 6: Reduce Impact of Motor Vehicles and Through Traffic in Neighbourhoods**
 - **Impacts to Motor Vehicles:** Impact on traffic operations relative to the intended road network classification and function of the street, including congestion/delays and greenhouse gas emissions from such delays or rerouting.
 - **Neighbourhood Transportation Impacts:** Impacts to neighbouring streets from increased traffic volumes and/or parking due to rerouting and neighbourhood circulation.
- **Objective 7: Limit Impacts to People Who Rely on On-Street Parking and Loading**
 - **On-street Parking and Loading:** Impact to on-street parking and loading.
- **Objective 8: Seek Opportunities to Incorporate Green Infrastructure**
 - **Green Infrastructure Opportunities:** Opportunities to implement green infrastructure such as stormwater infiltration trenches, soil cells, structural soil, and rain gardens in curb extensions.
- **Objective 9: Minimize Risk and Consider Implementation and Maintenance**
 - **Ease of Implementation:** Extent of implementation challenges, such as property impacts, utility impacts, legislative changes, curb work, etc.
 - **Utility Conflicts:** Impact to existing shallow and deep utilities, and associated accessories.
 - **Maintenance and Operations:** Potential maintenance challenges related to street sweeping and snow removal.
 - **Ability to Deliver Within Budget:** The ability to provide all desired elements within the available capital budget.
- **Objective 10: Consider Results of Public and Stakeholder Engagement**
 - **Survey Results:** Overall acceptability and support based on public and stakeholder engagement based on survey results as well as public and stakeholder input. It should be noted that the results are not considered statistically significant or representative and only represent the feedback received by those who participated in the engagement process.
- **Objective 11: Consider Other Outcomes and Alignment with the City's Seven Bold Steps**
 - **Equity:** Changes which benefit one group at the expense of another.
 - **Public Health:** Ability to promote active transportation mode share and reduce the potential for collisions.
 - **Climate Emergency:** Ability to address the City of New Westminster's Seven Bold Steps.

3.0 EVALUATION RESULTS

The results of the MAE are provided below for each project.

3.1 ROTARY CROSSTOWN GREENWAY MULTIPLE ACCOUNT EVALUATION

CRITERIA	OPTION 1	OPTION 2	OPTION 3
OBJECTIVE 1: CROSSTOWN CYCLING ROUTE FOR ALL AGES AND ABILITIES			
Cycling Network Connectivity	+ Direct connection to/from existing cycling facilities in Moody Park and eastern continuation of the Rotary Crosstown Greenway.	+ Direct connection to/from existing cycling facilities in Moody Park and eastern continuation of the Rotary Crosstown Greenway.	+ Direct connection to/from existing cycling facilities in Moody Park and eastern continuation of the Rotary Crosstown Greenway.
Cycling Safety and Comfort	<ul style="list-style-type: none"> + Protected bicycle lanes provide physical separation between all users, and uni-directional facilities improve safety by following motor vehicle traffic operations. - Cyclists will travel more comfortably than the current configuration with physical separation between parked vehicles and traffic; however, the bicycle facility is at a constrained width that does not allow passing or side-by-side operations. - Visibility is limited due to on-street parking on both sides of the street. - Parking conflicts with cyclists on both sides of the street. 	<ul style="list-style-type: none"> + Protected bicycle lanes provide physical separation between all users, and uni-directional facilities improve safety by following motor vehicle traffic operations. + Cyclists will travel more comfortably than the current configuration or Option 1 with physical separation between parked vehicles and traffic and wider cycling facilities than Option 1, thereby allowing passing and more comfortable cycling. - Visibility is somewhat limited due to on-street parking on one side of the street. + Parking conflicts with cyclists only on one side of the street. 	<ul style="list-style-type: none"> + The street closure at Seventh Avenue and Sixth Street would reduce traffic volumes somewhat by restricting through traffic; however, traffic volumes may still be higher than desired thresholds for a AAA facility due to traffic volumes from high density along the corridor. - Cyclists must share the road with vehicles, which still creates potential for conflict and a less comfortable environment for both. - Visibility is limited due to on-street parking on both sides of the street.
OBJECTIVE 2: IMPROVED WALKING EXPERIENCE AND ACCESSIBILITY			
Pedestrian Safety and Comfort	<ul style="list-style-type: none"> + Improved pedestrian crossings. + Additional physical separation between pedestrians and vehicle + traffic. - No opportunities for widened sidewalks. - Visibility is limited due to on-street parking on both sides of the street. 	<ul style="list-style-type: none"> + Improved pedestrian crossings. + Additional physical separation and buffer space between pedestrians and vehicle traffic. + Opportunity to widen sidewalks where feasible. - Visibility is somewhat limited due to on-street parking on one side of the street. 	<ul style="list-style-type: none"> + Improved pedestrian crossings, including significant improvements at the Seventh Avenue and Sixth Street intersection with the closure of the west leg of the intersection. + Additional physical separation and buffer space between pedestrians and vehicle traffic may be possible, but is highly dependent on budget. + Opportunity to widen sidewalks where feasible. - Visibility is limited due to on-street parking on both sides of the street.
Accessibility	<ul style="list-style-type: none"> + Improved pedestrian crossings and additional buffer space between pedestrians and vehicle traffic. ■ Parking accessibility from sidewalk can be challenging when bicycle lane is not at sidewalk grade. Sidewalk level bicycle lane may be more accessible for people with mobility devices, but may not be detectable for people with vision loss. This impact can be further addressed through subsequent phases of design. + Parking on both sides of the street improves accessibility, including vehicle drivers and occupants who require accessible parking. 	<ul style="list-style-type: none"> + Improved pedestrian crossings and additional buffer space between pedestrians and vehicle traffic. ■ Parking accessibility from sidewalk can be challenging when bicycle lane is not at sidewalk grade. Sidewalk level bicycle lane may be more accessible for people with mobility devices, but may not be detectable for people with vision loss. This impact can be further addressed through subsequent phases of design. - Parking only on one side of the street addresses accessibility, including vehicle drivers and occupants who require accessible parking. 	<ul style="list-style-type: none"> + Improved pedestrian crossings and additional buffer space with landscaping and street trees between pedestrians and vehicle traffic may be possible, but is highly dependent on budget + Improved pedestrian crossing at Seventh Avenue and Sixth Street significantly improves accessibility. + Depending on landscape treatment, parking accessibility to the sidewalk can be maintained + Parking on both sides of the street improves accessibility, including vehicle drivers and occupants who require accessible parking.
OBJECTIVE 3: ENHANCED PUBLIC REALM AND TREE CANOPY COVER			
Public Realm and Streetscape	<ul style="list-style-type: none"> - Minimal opportunity to provide minimal new landscaping through potential curb extensions. 	<ul style="list-style-type: none"> + New landscaping provided on both sides of the street, acting as a buffer between pedestrians and cyclists from parked and moving vehicles. + New landscaping provides opportunity for shrub and flower plantings that provide habitat for pollinator species. 	<ul style="list-style-type: none"> + New landscaping provided on both sides of the street, acting as a buffer between pedestrians and cyclists from parked and moving vehicles may be possible, but is highly dependent on budget + A wider boulevard adjacent to the sidewalk (and without a bisecting bicycle lane) allows for more opportunities for parklets and boulevard rooms, especially if curb bump outs into parking are used
Tree Canopy Cover	<ul style="list-style-type: none"> - There would not be enough space to include additional street trees. 	<ul style="list-style-type: none"> + 1.4 m wide north boulevard may provide enough soil volume to plant small street trees that do not grow to conflicting heights with overhead utilities. + 2.0 m wide south boulevard could support small street trees without additional soil volume or medium to large street trees with the addition of soil cells to achieve required minimum soil volumes. 	<ul style="list-style-type: none"> + 2.2 m wide north boulevard may provide enough soil volume to plant small street trees that do not grow to conflicting heights with overhead utilities. + 2.1 m wide south boulevard could support small street trees without additional soil volume or medium to large street trees with the addition of soil cells to achieve required minimum soil volumes. + 2.1 m wide south boulevard could support medium to large street trees without the addition of soil cells if bump outs into parking spaces are used.

OBJECTIVE 4: MAINTAIN TRANSIT SERVICE AND OPERATIONS			
Impacts to Transit Operations	There are no transit operations on this corridor.	There are no transit operations on this corridor.	There are no transit operations on this corridor.
OBJECTIVE 5: ACCOMMODATE EMERGENCY ACCESS			
Impacts to Emergency Access	<ul style="list-style-type: none">+ There are no impacts to emergency access.- 5.4 m drive aisle width is below desired width; however, this reflects existing conditions.	<ul style="list-style-type: none">+ There are no impacts to emergency access.+ Drive aisle width increased to 5.6 m.	<ul style="list-style-type: none">- Significant traffic calming and diversion required which may impact emergency services. Design of street closure can ensure emergency vehicles can be accommodated.+ Drive aisle width increased to 5.6 m.
OBJECTIVE 6: REDUCE IMPACT OF MOTOR VEHICLES AND THROUGH TRAFFIC IN NEIGHBOURHOOD			
Impacts to Motor Vehicles	<ul style="list-style-type: none">+ There are no changes to motor vehicle access and circulation.- 5.4 m drive aisle width is below desired width and may limit two-way circulation without one vehicle having to wait for the other; however, this reflects existing conditions.	<ul style="list-style-type: none">+ There are no changes to motor vehicle access and circulation.+ Drive aisle width increased to 5.6 m.	<ul style="list-style-type: none">- Motor vehicle access is restricted with the street closure at Seventh Avenue and Sixth Street; while this may be a positive for residents along the corridor, it was noted as a significant concern for business access.+ Drive aisle width increased to 5.6 m.
Neighbourhood Transportation Impacts	<ul style="list-style-type: none">+ There are no impacts to neighbourhood transportation.	<ul style="list-style-type: none">+ There are no impacts to neighbourhood transportation due to vehicle access.- Reduction of on-street parking may result in additional parking demands on adjacent streets and increased circulation to find parking.	<ul style="list-style-type: none">+ Street closure at Seventh Avenue and Sixth Street and potential street narrowing can reduce traffic volumes and speeds, which can improve safety and liveability for residents on Seventh Avenue.- Due to traffic calming and diversion, there is likely more pressure and traffic directed to Hamilton Street and Princess Street, as well as more people using north-south laneway west of Sixth Street.
OBJECTIVE 7: LIMIT IMPACTS TO PEOPLE WHO RELY ON ON-STREET PARKING AND LOADING			
On-street Parking and Loading	<ul style="list-style-type: none">+ There are limited impacts to on-street parking; although on-street parking is maintained on both side of the street, some parking may be impacted due to visibility.+ There is no impact to loading zones.	<ul style="list-style-type: none">- Parking removed on one side of the street.- Loading zones removed on one side of the street.	<ul style="list-style-type: none">+ There would be no impacts to on-street parking.+ There is no impact to loading zones.
OBJECTIVE 8: SEEK OPPORTUNITIES TO INCORPORATE GREEN INFRASTRUCTURE			
Green Infrastructure Opportunities	<ul style="list-style-type: none">- There is no space in median for subsurface GI facility. Subsurface facility likely constrained by adjacent sanitary sewer.	<ul style="list-style-type: none">+ There is an opportunity for bioswale, raingarden, or tree well structures, with the wider eastbound median being more feasible.+ GI on either side would be limited by depth of existing underground utilities within medians.	<ul style="list-style-type: none">+ There is an opportunity for bioswale, raingarden, or tree well structures. Both sides present wide enough boulevard space for linear GI facility. If street is narrowed to provide landscaping opportunities, which is highly dependent on budget.+ GI on either side would be limited by depth of existing underground utilities within medians.
OBJECTIVE 9: MINIMIZE RISK AND CONSIDER IMPLEMENTATION AND MAINTENANCE			
Ease of Implementation	<ul style="list-style-type: none">+ Easiest facility to implement.- If bicycle lanes are raised, new catch basins would be required.	<ul style="list-style-type: none">- Full reconstruction required on the south side of the street.- North side of the street limited to boulevard construction depending on whether bicycle lane is raised. .	<ul style="list-style-type: none">- Most challenging facility to implement.- Full reconstruction required on both sides of the street and on the west leg of the intersection at Sixth Street.
Utility Conflicts	<ul style="list-style-type: none">- Existing hydro pole conflicts along north side, proposed buffer would be in close proximity to underground utilities (combined sewer, watermain, Telus) in 500 block.	<ul style="list-style-type: none">- Existing hydro pole conflicts along north side, proposed landscaped buffer would be over top underground utilities (combined sewer, watermain, Telus) in 500 block.	<ul style="list-style-type: none">- Proposed landscaped boulevard would be over top underground utilities (watermain, Telus) in 500 block.
Maintenance & Operations	<ul style="list-style-type: none">- Protected bicycle lanes require additional snow removal and street sweeping.- Narrow protected bike lanes may make maintenance more challenging, if facility is not raised to sidewalk level.	<ul style="list-style-type: none">- Protected bicycle lanes require additional snow removal and street sweeping.+ Wider bicycle lanes will accommodate maintenance equipment but may still be challenging if not raised to sidewalk level..	<ul style="list-style-type: none">+ Easiest maintenance and operations without protected bicycle lanes.- New plaza at street closure at Seventh Avenue and Sixth Street will require maintenance.
Ability to Deliver Within Budget	<ul style="list-style-type: none">+ Easiest	<ul style="list-style-type: none">- Some elements may be challenging to fit within budget.	<ul style="list-style-type: none">- Many elements may be challenging to fit within budget.

OBJECTIVE 10: CONSIDER RESULTS OF PUBLIC AND STAKEHOLDER ENGAGEMENT			
Survey Results	+ 49% in support + 31% selected as preferred design option	+ 55% in support + 46% selected as preferred design option	- 32% in support - 23% selected as preferred design option
OBJECTIVE 11: CONSIDER OTHER OUTCOMES AND ALIGNMENT WITH THE CITY’S SEVEN BOLD STEPS			
Equity	+ There are no changes at the expense of another group.	- Loss of parking/loading may affect seniors or persons with disabilities, although this is the most comfortable option for cycling, which provides equity for cyclists .	+ There are no changes at the expense of another group.
Public Health	+ Promotes a comfortable walking and cycling environment.	+ Minimizes potential for conflict with reduced parking + Promotes a comfortable walking and cycling environment + Increased street trees	+ Promotes a comfortable walking and cycling environment . + Increased street trees.
Climate Emergency	+ Car light community. + People-centred public realm.	+ Car light community. + Robust urban forest. + People-centred public realm.	+ Car light community. + Robust urban forest. + People-centred public realm.

3.2 NEW WESTMINSTER SECONDARY SCHOOL CYCLING CONNECTOR MULTIPLE ACCOUNT EVALUATION

CRITERIA	OPTION 1A	OPTION 1B	OPTION 2	OPTION 3
OBJECTIVE 1: CROSSTOWN CYCLING ROUTE FOR ALL AGES AND ABILITIES				
Cycling Network Connectivity	<ul style="list-style-type: none"> - Least direct connection to New Westminster Secondary School from existing cycling facilities on Rotary Crosstown Greenway, as cyclists coming from the west will need to travel 'backwards.' - It is likely that many cyclists will not follow this alignment and will still take the most direct route. 	<ul style="list-style-type: none"> - Least direct connection to New Westminster Secondary School from existing cycling facilities on Rotary Crosstown Greenway, as cyclists coming from the west will need to travel 'backwards.' - It is likely that many cyclists will not follow this alignment and will still take the most direct route. 	<ul style="list-style-type: none"> + Most direct connection to New Westminster Secondary School from existing cycling facilities on Rotary Crosstown Greenway. 	<ul style="list-style-type: none"> - Less direct connection to New Westminster Secondary School from existing cycling facilities on Rotary Crosstown Greenway.
Cycling Safety and Comfort	<ul style="list-style-type: none"> + Protected bicycle lanes provide physical separation between all users, and uni-directional facilities improve safety by following motor vehicle traffic operations. - The lane connection is very narrow, poorly light and is shared with motor vehicles and may not be comfortable for cyclists. 	<ul style="list-style-type: none"> + Protected bicycle lanes provide physical separation between all users, and uni-directional facilities improve safety by following motor vehicle traffic operations - The lane connection is very narrow, poorly light and is shared with motor vehicles and may not be comfortable for cyclists. - Bicycle facilities set back from roadway behind street trees may limit visibility and safety at intersections 	<ul style="list-style-type: none"> + Protected bicycle lanes provide physical separation between all users, and uni-directional facilities improve safety by following motor vehicle traffic operations - More noise and emissions due to traffic may make this less comfortable for cyclists. 	<ul style="list-style-type: none"> + Protected bicycle lanes provide physical separation between all users, and uni-directional facilities improve safety by following motor vehicle traffic operations - If bicycle facilities on Fifth Street are provided off-street, bicycle facilities set back from roadway behind street trees may limit visibility and safety at intersections
OBJECTIVE 2: IMPROVED WALKING EXPERIENCE AND ACCESSIBILITY				
Pedestrian Safety and Comfort	<ul style="list-style-type: none"> + Improved pedestrian safety, crossings and additional buffer space between pedestrians and vehicle traffic. + New traffic signal at Eighth Avenue and Fifth Street will improve pedestrian safety. 	<ul style="list-style-type: none"> + Improved pedestrian safety, crossings and additional buffer space between pedestrians and vehicle traffic. + New traffic signal at Eighth Avenue and Fifth Street will improve pedestrian safety. 	<ul style="list-style-type: none"> + Improved pedestrian safety, crossings and additional buffer space between pedestrians and vehicle traffic. + Intersection improvements at Eighth Avenue and Sixth Street will improve pedestrian safety. 	<ul style="list-style-type: none"> + Improved pedestrian safety, crossings and additional buffer space between pedestrians and vehicle traffic. + New traffic signal at Eighth Avenue and Fifth Street will improve pedestrian safety.
Accessibility	<ul style="list-style-type: none"> + Improved pedestrian crossings and additional buffer space between pedestrians and vehicle traffic. 	<ul style="list-style-type: none"> + Improved pedestrian crossings. 	<ul style="list-style-type: none"> + Improved pedestrian crossings and additional buffer space between pedestrians and vehicle traffic. 	<ul style="list-style-type: none"> + Improved pedestrian crossings and additional buffer space between pedestrians and vehicle traffic.
OBJECTIVE 3: ENHANCED PUBLIC REALM AND TREE CANOPY COVER				
Public Realm and Streetscape	<ul style="list-style-type: none"> + There are no impacts to the public realm. 	<ul style="list-style-type: none"> - Less green space available in landscaped boulevards. 	<ul style="list-style-type: none"> + There are no impacts to the public realm. 	<ul style="list-style-type: none"> - Less green space available in landscaped boulevards if off-street bicycle lanes are selected on Fifth Street.
Tree Canopy Cover	<ul style="list-style-type: none"> + There are no impacts to street trees. + Additional street trees may be accommodated in the wide boulevard. 	<ul style="list-style-type: none"> - Landscaping and street trees may be impacted 	<ul style="list-style-type: none"> + There are no impacts to street trees. 	<ul style="list-style-type: none"> - Landscaping and street trees may be impacted.
OBJECTIVE 4: MAINTAIN TRANSIT SERVICE AND OPERATIONS				
Impacts to Transit Operations	<ul style="list-style-type: none"> + There are no transit operations on this corridor. - New traffic control at Eighth Avenue and Fifth Street may have some 	<ul style="list-style-type: none"> + There are no transit operations on this corridor. 	<ul style="list-style-type: none"> - Buses will have to stop in-lane and may have moderate impacts on transit speed and reliability. 	<ul style="list-style-type: none"> - Buses will have to stop in-lane and may have significant impacts on transit speed and reliability. - TransLink and CMBC may be supportive of relocating eastbound bus stop on Eighth Avenue slightly further east to

	impact on transit service on Eighth Avenue.	<ul style="list-style-type: none">- New traffic control at Eighth Avenue and Fifth Street may have some impact on transit service on Eighth Avenue.	<ul style="list-style-type: none">- TransLink and CMBC may not be supportive of relocating bus stops on Sixth Street- Design will need to consider impacts of Human Rights Tribunal ruling if floating bus stops are required.	<p>the far side of the lane to minimize impact of buses stopping in-lane.</p> <ul style="list-style-type: none">- Design will need to consider impacts of Human Rights Tribunal ruling if floating bus stops are required.
OBJECTIVE 5: ACCOMMODATE EMERGENCY ACCESS				
Impacts to Emergency Access	<ul style="list-style-type: none">+ There are no impacts to emergency access.	<ul style="list-style-type: none">+ There are no impacts to emergency access.	<ul style="list-style-type: none">+ There are no impacts to emergency access.- Reduced street space may result in emergency vehicles blocking transit / traffic	<ul style="list-style-type: none">+ There are no impacts to emergency access.- Reduced street space may result in emergency vehicles blocking transit / traffic
OBJECTIVE 6: REDUCE IMPACT OF MOTOR VEHICLES AND THROUGH TRAFFIC IN NEIGHBOURHOOD				
Impacts to Motor Vehicles	<ul style="list-style-type: none">+ There are no changes to motor vehicle access and circulation.	<ul style="list-style-type: none">+ There are no changes to motor vehicle access and circulation.	<ul style="list-style-type: none">- Transit vehicles stopping in lane with traffic will cause moderate delays.	<ul style="list-style-type: none">- Transit vehicles stopping in lane with traffic will cause significant delays which are likely unacceptable.
Neighbourhood Transportation Impacts	<ul style="list-style-type: none">+ There are no impacts to neighbourhood transportation.	<ul style="list-style-type: none">+ There are no impacts to neighbourhood transportation.	<ul style="list-style-type: none">- Due to increased delays on Sixth Street, there is likely more pressure and traffic directed to Fifth Street.	<ul style="list-style-type: none">- Due to increased delays on Sixth Street and Eighth Avenue, there is likely more pressure and traffic directed to Fifth Street and Seventh Avenue.
OBJECTIVE 7: LIMIT IMPACTS TO PEOPLE WHO RELY ON ON-STREET PARKING AND LOADING				
On-street Parking and Loading	<ul style="list-style-type: none">- On street non-metered parking removed from both sides of the street.	<ul style="list-style-type: none">- Small parking loss along Sixth Street.	<ul style="list-style-type: none">- On street parking removed from both sides of the street north of Eighth Avenue and one side of the street south of Eighth Avenue.	<ul style="list-style-type: none">- Sixth Street: On street non-metered parking removed on one side of the street.- Eighth Avenue: Minimal on-street non-metered parking removed on one side of the street.- Fifth Street: Same considerations as Option 1.
OBJECTIVE 8: SEEK OPPORTUNITIES TO INCORPORATE GREEN INFRASTRUCTURE				
Green Infrastructure Opportunities	<ul style="list-style-type: none">+ Opportunity for rain garden in wide boulevard for water quantity capture facility, depending on local infiltration rates.- No GI facility is recommended for water quality treatment in absence of dedicated storm sewer to connect to.	<ul style="list-style-type: none">+ Opportunity for rain garden in boulevard (area not converted to bike lane) for water quantity capture facility, depending on local infiltration rates.- No GI facility is recommended for water quality treatment in absence of dedicated storm sewer to connect to.- Potential impacts to CNW GI design at Eighth Avenue and Fifth Street intersection.	<ul style="list-style-type: none">+ Opportunity for rain garden water quality at intersections of Sixth Street/Hamilton Street and Sixth Street/Seventh Avenue to connect with dedicated storm sewers on Hamilton Street and Seventh Avenue.+ Limited space elsewhere on Sixth Street for water quantity capture facility.	<ul style="list-style-type: none">+ Opportunity on Fifth Street section for rain garden water quantity capture facility depending on local infiltration rates.+ No GI facility is recommended for water quality treatment in absence of dedicated storm sewer to connect to.
OBJECTIVE 9: MINIMIZE RISK AND CONSIDER IMPLEMENTATION AND MAINTENANCE				
Ease of Implementation	<ul style="list-style-type: none">+ Sufficient existing pavement width.- Connection to NWSS reliant on timing of housing development.	<ul style="list-style-type: none">- Boulevard construction adds some complexity to weave around obstructions.- Connection to NWSS reliant on timing of housing development.- Requires coordination due to CNW GI design at Eighth Avenue and Fifth Street intersection.	<ul style="list-style-type: none">- Reconstruction of the east side of the street north of Eighth Avenue required.- Raised protected bike lanes on the west side between Eighth Avenue and Seventh Avenue require drainage new catch basins.	<ul style="list-style-type: none">+ Sufficient existing pavement width available with parking removal.+ Opportunity to use low-cost treatments.+ Opportunities to relocate transit stop on Sixth Street north of Eighth Avenue to reduce conflicts between people biking and riding transit.
Utility Conflicts	<ul style="list-style-type: none">+ No major conflicts, bike lane may be over top existing watermain in 800 block.	<ul style="list-style-type: none">- More utility conflicts to consider, including hydro poles and several below ground utilities within boulevard.- Requires coordination due to CNW GI design at Eighth Avenue and Fifth Street intersection.	<ul style="list-style-type: none">- Some utility conflicts to consider, including several below ground utilities (combined sewers) in 700 block.	<ul style="list-style-type: none">- More utility conflicts to consider, including additional underground utilities along Eight Ave (water, combined sewer).

Maintenance & Operations	<ul style="list-style-type: none">- Protected bicycle lanes on street will require sweeping and snow removal.- New signal at Fifth Street and Eighth Avenue will require ongoing maintenance.	<ul style="list-style-type: none">- Off-street bicycle lanes will require sweeping and snow removal.- New signal at Fifth Street and Eighth Avenue will require ongoing maintenance.	<ul style="list-style-type: none">- Protected bicycle lanes on both sides of the street will require sweeping and snow removal.- Bicycle lanes at road grade may be more challenging to maintain.	<ul style="list-style-type: none">- Cycling facilities on Fifth Street, Sixth Street, and Eighth Avenue will require sweeping and snow removal.- New signal at Fifth Street and Eighth Avenue will require ongoing maintenance.
Ability to Deliver Within Budget	<ul style="list-style-type: none">+ Opportunity for low-cost treatments along the corridor.- Significant cost for the new signal at Eighth Avenue	<ul style="list-style-type: none">- Higher cost option as low-cost treatments are not possible with off-street pathway.- Significant cost for the new signal at Eighth Avenue	<ul style="list-style-type: none">+ Opportunity for low-cost treatments along the corridor.	<ul style="list-style-type: none">+ Opportunity for low-cost treatments along the corridor.- Significant cost for the new signal at Eighth Avenue
OBJECTIVE 10: CONSIDER RESULTS OF PUBLIC AND STAKEHOLDER ENGAGEMENT				
Survey Results	<ul style="list-style-type: none">- 27% in support- 12% selected as preferred design option	<ul style="list-style-type: none">- 32% in support- 24% selected as preferred design option	<ul style="list-style-type: none">+ 51% in support+ 47% selected as preferred design option	<ul style="list-style-type: none">- 25% in support- 16% selected as preferred design option
OBJECTIVE 11: CONSIDER OTHER OUTCOMES AND ALIGNMENT WITH THE CITY'S SEVEN BOLD STEPS				
Equity	<ul style="list-style-type: none">- Residents on Fifth Street and the laneway between Fifth and Sixth Street are impacted the most.	<ul style="list-style-type: none">- Residents on Fifth Street and the laneway between Fifth and Sixth Street are impacted the most.	<ul style="list-style-type: none">- Businesses on Sixth Street are impacted the most.	<ul style="list-style-type: none">- Residents on Fifth Street are impacted the most.
Public Health	<ul style="list-style-type: none">+ Promotes a comfortable walking and cycling environment	<ul style="list-style-type: none">+ Promotes a comfortable walking and cycling environment	<ul style="list-style-type: none">+ Promotes a comfortable walking and cycling environment	<ul style="list-style-type: none">+ Promotes a comfortable walking and cycling environment
Climate Emergency	<ul style="list-style-type: none">+ Car-light community+ People-centred public realm	<ul style="list-style-type: none">+ Car-light community	<ul style="list-style-type: none">+ Car-light community+ People-centred public realm	<ul style="list-style-type: none">+ Car-light community+ People-centred public realm

4.0 RECOMMENDATIONS

4.1 ROTARY CROSSTOWN GREENWAY UPGRADES

Based on the MAE, the project team recommends **Option 2** as the preferred option to advance to conceptual design. While Options 2 and 3 have similar benefits in terms of improvements to the cycling experience, pedestrian experience, and public realm, Option 2 was preferred in particular because of significantly higher community support and significantly fewer implementation challenges. Option 1 was the least preferred as it had the fewest benefits relative to the existing bicycle facilities.

4.2 NEW WESTMINSTER SECONDARY SCHOOL CYCLING CONNECTOR

Based on the MAE, the project team recommends **Option 2 or Option 3** as the preferred options to advance to conceptual design. Option 1 was the least preferred and is not recommended to be advanced further, primarily due to poor cycling connectivity. Options 2 and 3 both have a number of benefits and impacts across several accounts, and these trade-offs need to be considered further. While Option 2 is the most direct route and has the greatest level of support, it has the most significant impacts on traffic and transit operations and may have the greatest impact to businesses. On the other hand, Option 3 is a less direct route and received the lowest level of community support, but would have fewer impacts on traffic operations and it is felt that the transit impacts could be mitigated by relocating the bus stop on Eighth Avenue further east.

Sincerely,

URBAN SYSTEMS LTD.

Brian Patterson, RPP, MCIP, PMP
Senior Transportation Planner

Sarah Tremblay
Transportation Planner

cc: Brent McMurtry, Barry Fan

/ST
Enclosure

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P R O C L A M A T I O N

CITY OF NEW WESTMINSTER

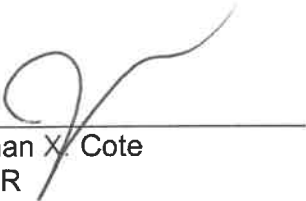
INTERNATIONAL HOLOCAUST REMEMBRANCE DAY

January 27, 2022

- WHEREAS** In 2005, the United National General Assembly designated January 27 – the anniversary of the liberation of Auschwitz-Birkenau – as International Holocaust Remembrance Day; and
- WHEREAS** On this annual day of commemoration, every member state of the U.N. has an obligation to honour the victims of the Nazi era and to develop educational programs to help prevent future genocides; and
- WHEREAS** The U.N. resolution that created IHRD rejects denial of the Holocaust and condemns discrimination and violence based on religion or ethnicity; and
- WHEREAS** Canada offers refuge and new hope to immigrants, refugees and survivors, some of whom have escaped more recent genocides – and it is a place where people learn from each other and share cultures; and
- WHEREAS** We join with the Vancouver Holocaust Education Centre and our Jewish community on an annual day of commemoration to honour the victims of the Nazi era;
- NOW THEREFORE** I, Jonathan Cote, Mayor of the City of New Westminster do hereby proclaim Thursday, January 27, 2022 as

INTERNATIONAL HOLOCAUST REMEMBRANCE DAY

In the City of New Westminster, Province of British Columbia.


Jonathan X. Cote
MAYOR



SPECIAL CITY COUNCIL WORKSHOP**MINUTES****Monday, November 1, 2021****Meeting held electronically and open to public attendance
in Council Chamber, City Hall****PRESENT:**

Mayor Jonathan Cote
Councillor Patrick Johnstone
Councillor Jamie McEvoy
Councillor Chuck Puchmayr
Councillor Mary Trentadue

ABSENT

Councillor Chinu Das
Councillor Nadine Nakagawa

STAFF PRESENT:

Ms. Lisa Spitale	Chief Administrative Officer
Ms. Jacque Killawee	City Clerk
Ms. Emilie Adin	Director of Climate Action, Planning and Development
Mr. Mike Anderson	Acting Manager of Transportation
Mr. Todd Ayotte	Manager, Community Arts and Theatre
Mr. Rupinder Basi	Supervisor of Development Services
Mr. Gabe Beliveau	Acting Manager of Engineering Operations
Mr. Curtis Bremner	Acting Fire Chief, New Westminster Fire and Rescue Services
Ms. Susan Buss	Deputy Chief Librarian
Mr. Jorge Cardenas	Chief Librarian
Mr. Rod Carle	General Manager, Electrical Utility
Mr. Blair Fryer	Manager, Communications and Economic Development
Mr. Richard Fong	Director of Human Resources
Mr. Dean Gibson	Director of Parks and Recreation
Mr. Dave Jansen	Chief Constable

Ms. Lisa Leblanc	Director of Engineering Services
Ms. Lorraine Lyle	Senior Manager of Finance
Mr. Craig MacFarlane	Manager of Legal Services
Ms. Erika Mashig	Manager, Parks and Open Space Planning, Design and Construction
Mr. Rob McCullough	Manager, Museums and Heritage Services
Ms. Jackie Teed	Senior Manager of Climate Action, Planning and Development
Ms. Harji Varn	Chief Financial Officer and Director of Finance
Ms. Nicole Ludwig	Assistant City Clerk

1. CALL TO ORDER AND LAND ACKNOWLEDGEMENT

Mayor Cote opened the meeting at 1:30 p.m. and recognized with respect that New Westminster is on the unceded and unsurrendered land of the Halkomelem speaking peoples. He acknowledged that colonialism has made invisible their histories and connections to the land. He recognized that, as a City, we are learning and building relationships with the people whose lands we are on

2. CONSENT AGENDA

If Council decides, all the recommendations in the reports on the Consent Agenda can be approved in one motion, without discussion. If Council wishes to discuss a report, that report is removed from the Consent Agenda. A report may be removed in order to discuss it, because someone wants to vote against the report's recommendation, or because someone has a conflict of interest with the report. Any reports not removed from the Consent Agenda are passed without discussion.

MOVED AND SECONDED

THAT Council adopt the recommendation for item 2.1 on consent.

Carried.

All members present voted in favour of the motion.

2.1 Budget 2022: Responses to Council Questions from October 4th Workshop

An informational report to provide Council with staff responses to several outstanding questions posed by Council at the October 4th Capital Budget Workshop.

THAT Council receive the November 1, 2021, report entitled "Budget 2022: Responses to Council Questions from October 4th Workshop" for information as part of the Budget deliberation process.

Adopted on Consent.

3. PRESENTATIONS

3.1 Budget 2022: Proposed 2022 Operating and Utility Budgets

The purpose of this report is to provide Council with detailed information on the 2022 Proposed General Fund Operating and Utility Budgets. The report also provides the proposed 2022 rate increases for the Electric, Water, Sewer and Solid Waste Utilities and will provide a high level rate projection for Property Taxes before Service Enhancements. Staff is providing Council with all pertinent operating budget information, even information that is more preliminary in nature, so that Council has a fulsome picture of the budget. The expectation is that Council will provide staff with feedback at the November 1st Operating Budget Workshop that will set the direction for the November 29th Draft 2022 Capital and Operating Budget and 5 Year Financial Plan Workshop.

Lisa Spitale, Chief Administrative Officer, introduced the draft budget and members of the Senior Management Team (SMT), Senior Managers, and Managers, as follows:

- Lorraine Lyle, Senior Manager of Finance – Financial Principles and Operating Budget Overview;
- Jackie Teed, Senior Manager of Climate Action, Planning and Development – Affordable Housing and Child Care
- Rob McCullough, Manager, Museums and Heritage Services – Reconciliation, Inclusion and Engagement;
- Erika Mashig, Manager, Parks and Open Space Planning, Design and Construction – Environment and Climate
- Mike Anderson, Acting Manager of Transportation – Sustainable Transportation;
- Blair Fryer, Manager of Communications and Economic Development – Culture and Economic Development;
- Tobi May, Manager, Civic Buildings and Properties;
- Richard Fong, Director of Human Resources – Resiliency and Corporate Support;

- Curtis Bremner, Acting Fire Chief – COVID Response: the City's Task Forces; and,
- Susan Buss, Deputy Chief Librarian – Equity Framework.

The following staff provided updates and future initiatives for the topics listed:

- Rod Carle, General Manager, Electrical Utility – Electrical Operations Core Services; and,
- Gabe Beliveau, Acting Manager, Engineering Operations – Solid Waste, Water, and Sewers

Harji Varn, Director of Finance and Chief Financial Officer, presented on how utility funds are used, an overview of the proposed 2022 Electrical, Solid Waste, Water, and Sewer Utilities Rates, and three scenarios for the 2022 property tax increase.

In response to questions from Council, some of the above-noted staff, along with Mayor Cote and Lisa Leblanc, Director of Engineering Services, provided the following additional information:

- The last two years have been very challenging for the Q to Q Ferry, particularly with reduced sailing capacity due to the COVID-19 pandemic; staff are promoting the ferry to the public as capacity ramps up again;
- 80% of the City's revenues come from stable sources, and only 20% from the Casino; because of this ratio, the City was able to weather the effects of the pandemic;
- The eMobility strategy will come to Council before the end of the year at which point an in-depth conversation can take place;
- Refinement of enhancement requests will take place at the November 29, 2021 meeting; and,
- It is possible to toggle the capital levy contribution between reserves and debt year by year and
- There will be an opportunity for Council to discuss the Police budget with the Police Board on Thursday, November 3, 2021, during their meeting.

In discussion, Council members noted:

- The charge for charging electric vehicles (EVs) may contribute to less use of the City's charging stations;

- This is a good first glimpse of some of the proposed staffing enhancements;
- Need to look closely at insurance costs as they are a significant driver in the operating budget; and
- Concerns if the property tax increase is more than 6% and a desire to keep it closer to 4%.

Council requested additional information on the following topics:

- Progress filling staff vacancies;
- How revenue loss relates to reducing services;
- Whether capital spending has an impact on costs; and,
- Options on how to phase proposals in and further understanding of requested enhancements.

4. END OF THE MEETING

The meeting ended at 3:37 p.m.

Jonathan Cote
MAYOR

Jacque Killawee
CITY CLERK

**THE CORPORATION OF THE CITY OF NEW WESTMINSTER
HERITAGE REVITALIZATION AGREEMENT (323 Regina Street)
BYLAW NO. 8304, 2022**

**A Bylaw to enter into a Heritage Revitalization Agreement under
Section 610 of the *Local Government Act***

WHEREAS the City of New Westminster and the owners of the property located at 323 Regina Street in New Westminster wish to enter into a Heritage Revitalization Agreement in respect of the property;

NOW THEREFORE, the Council of the City of New Westminster enacts as follows:

Citation

1. This Bylaw may be cited as “Heritage Revitalization Agreement (323 Regina Street) Bylaw No. 8304, 2022”.

Heritage Revitalization Agreement

2. The City of New Westminster enters into a Heritage Revitalization Agreement with the registered owners of the property located at 323 Regina Street legally described as PID: 013-593-285; LOT 12 OF LOT 4 SUBURBAN BLOCK 10 PLAN 2620.
3. The Mayor and City Clerk are authorized on behalf of the City of New Westminster Council to sign and seal the Heritage Revitalization Agreement attached to this Bylaw as Schedule “A”.

READ A FIRST TIME this _____ day of _____, 2022.

READ A SECOND TIME this _____ day of _____, 2022.

PUBLIC HEARING held this _____ day of _____, 2022.

READ A THIRD TIME this _____ day of _____, 2022.

ADOPTED this _____ day of _____, 2022.

MAYOR JONATHAN X. COTE

JACQUE KILLAWEE, CITY CLERK

SCHEDULE "A"**HERITAGE REVITALIZATION AGREEMENT (323 Regina Street)**

THIS AGREEMENT dated for reference the 1st day of December, 2021 is

BETWEEN:

GARY JOHN HOLISKO and ROSANNE MARIE HOOD,
323 Regina Street, New Westminster, BC
V3L 1S8

(together, the "Owners")

AND:

THE CORPORATION OF THE CITY OF NEW WESTMINSTER, City Hall, 511 Royal
Avenue, New Westminster, BC V3L 1H9

(the "City")

WHEREAS:

- A. The Owners are the registered owners in fee simple of the land and all improvements located at 323 Regina Street, New Westminster, British Columbia, legally described as PID: 013-593-285; LOT 12 OF LOT 4 SUBURBAN BLOCK 10 PLAN 2620 (the "Land");
- B. There is one principal building situated on the Land, known as the Edgar House (the "Heritage Building"), which is shown on the site plan attached as Appendix 1 (the "Site Plan") labelled as "323 Regina Street";
- C. The City and the Owner agree that the Heritage Building has heritage value and should be conserved;
- D. The Owner wishes to make certain alterations to restore and rehabilitate the Heritage Building (the "Work");
- E. The Owners intend to construct a two storey infill house on the lands, measuring approximately 132 square meters in size (the "Infill House") on that portion of the Land labelled on the Site Plan as "471 Fourth Street Coach House";
- F. Section 610 of the *Local Government Act*, RSBC 2015, Chapter 1 authorizes a local government to enter into a Heritage Revitalization Agreement with the owner of heritage property, and to allow variations of, and supplements to, the provisions of a bylaw or a permit issued under Part 14 or Part 15 of the *Local Government Act*;

- G. The Owner and the City have agreed to enter into this Heritage Revitalization Agreement setting out the terms and conditions by which the heritage value of the Heritage Building is to be preserved and protected, in return for specified supplements and variances to City bylaws;

THIS AGREEMENT is evidence that in consideration of the sum of ten dollars (\$10.00) now paid by each party to the other and for other good and valuable consideration (the receipt of which each party hereby acknowledges) the Owner and the City each covenant with the other pursuant to Section 610 of the *Local Government Act* as follows:

Conservation of Heritage Building

1. Upon execution of this Agreement, the Owner shall promptly commence the Work in accordance with the Heritage Conservation Plan prepared by Katie Cummer, PhD CAHP, of Cummer Heritage Consulting dated July 24, 2021, a copy of which is attached hereto as Appendix 2 (the "Conservation Plan"), and the design plans and specifications prepared by Nancy G Dheilley, dated AUG 6, 2021, NOV 8, 2021, and NOV 17, 2021, a copy of which is attached hereto as Appendix 5 (the "Approved Plans"), full-size copies of which plans and specifications are on file at the New Westminster City Hall.
2. Prior to commencement of the Work, the Owner shall obtain from the City all necessary permits and licenses, including a heritage alteration permit, building permit, and tree permit.
3. The Owner shall obtain written approval from the City's Director of Climate Action, Planning and Development for any changes to the Work, and obtain any amended permits that may be required for such changes to the Work, as required by the City.
4. The Owner agrees that the City may, notwithstanding that such permits may be issuable under the City's zoning and building regulations and the BC Building Code, withhold a heritage alteration permit or building permit applied for in respect of the Heritage Building if the work that the Owner wishes to undertake is not in accordance with the Conservation Plan or the Approved Plans.
5. The Work shall be done at the Owner's sole expense in accordance with generally accepted engineering, architectural, and heritage conservation practices. If any conflict or ambiguity arises in the interpretation of Appendix 2, the parties agree that the conflict or ambiguity shall be resolved in accordance with the "Standards and Guidelines for the Conservation of Historic Places in Canada", 2nd edition, published by Parks Canada in 2010.
6. The Owner shall, at the Owner's sole expense, erect on the Land and keep erected throughout the course of the Work, a sign of sufficient size and visibility to effectively notify contractors and tradespersons entering onto the Land that the Work involves protected heritage property and is being carried out for heritage conservation purposes.
7. The Owner shall, at the Owner's sole expense, engage a member of the Architectural Institute of British Columbia or the Association of Professional Engineers and Geoscientists of British Columbia or the British Columbian Association of Heritage Professionals with

specialization in Building or Planning (the “Registered Professional”) to oversee the Work and to perform the duties set out in section 8 of this Agreement, below.

Role of Registered Professional

8. The Registered Professional shall:
 - (a) prior to commencement of the Work, and at any time during the course of the Work that a Registered Professional has been engaged in substitution for a Registered Professional previously engaged by the Owner, provide to the City an executed and sealed Confirmation of Commitment in the form attached as Appendix 3 and, if the Registered Professional is a member of the Canadian Association of Heritage Professionals, the Registered Professional shall provide evidence of their membership and specialization when submitting such executed Confirmation of Commitment;
 - (b) conduct field reviews of the Work with the aim of ensuring compliance of the Work with the Conservation Plan in Appendix 2;
 - (c) provide regular reports to the City’s Climate Action, Planning and Development Department, on the progress of the Work;
 - (d) upon substantial completion of the Work, provide to the City an executed and sealed Certification of Compliance in the form attached as Appendix 4; and
 - (e) notify the City within one business day if the Registered Professional’s engagement by the Owner is terminated for any reason.

Heritage Designation

9. The Owner irrevocably agrees to the designation of the Heritage Building as protected heritage property, in accordance with Section 611 of the *Local Government Act*, and releases the City from any obligation to compensate the Owner in any form for any reduction in the market value of the Lands or the Heritage Building that may result from the designation.
10. Following completion of the Work, the Owner shall maintain the Heritage Building in good repair in accordance with the Conservation Plan in Appendix 2 and the maintenance standards set out in City of New Westminster Heritage Properties Minimum Maintenance Standards Bylaw No. 7971, 2018, as amended or replaced from time to time, and, in the event that Bylaw No. 7971 is repealed and not replaced, the Owner shall continue to maintain the building to the standards that applied under Bylaw No. 7971 immediately prior to its repeal.
11. Following completion of the Work in accordance with this Agreement, the Owner shall not alter the heritage character or the exterior appearance of the Heritage Building, except as permitted by a heritage alteration permit issued by the City.

Damage to or Destruction of Heritage Building

12. If the Heritage Building is damaged, the Owner shall obtain a heritage alteration permit and any other necessary permits and licenses and, in a timely manner, shall restore and repair the Heritage Building to the same condition and appearance that existed before the damage occurred.
13. If, in the opinion of the City, the Heritage Building is completely destroyed, the Owner shall construct a replica, using contemporary material if necessary, of the Heritage Building that complies in all respects with the Conservation Plan in Appendix 2, the Approved Plans in Appendix 5, and with City of New Westminster Zoning Bylaw No. 6680, 2001 as amended (the "Zoning Bylaw"), as varied by this Agreement, after having obtained a heritage alteration permit and any other necessary permits and licenses.
14. The Owner shall use best efforts to commence and complete any repairs to the Heritage Building, or the construction of any replica building, with reasonable dispatch.

Construction of the Infill House

15. The Owners shall construct the Infill House in strict accordance with the Site Plan and the Approved Plans prepared by Nancy G Dheilly, dated NOV 8, 2021 and NOV 17, 2021, a copy of which is attached hereto as Appendix 5, full-size copies of which plans and specifications are on file at the New Westminster City Hall.
16. Prior to commencement of construction of the Infill house, the Owner shall obtain from the City all necessary approvals, permits, and licenses, including a heritage alteration permit, building permit, and tree permit.
17. The Owner shall obtain written approval from the City's Director of Climate Action, Planning and Development for any changes to the Infill House, and obtain any amended permits that may be required for such changes to the Infill House, as required by the City.
18. The Owner agrees that the City may, notwithstanding that such permits may be issuable under the City's zoning and building regulations and the BC Building Code, withhold a heritage alteration permit or building permit applied for in respect of the Infill House if the work that the Owner wishes to undertake is not in accordance with the Approved Plans.
19. The construction of the Infill House shall be done at the Owner's sole expense and in accordance with generally accepted engineering and architectural practices.

Timing and Phasing

20. The Owner shall commence and complete all actions required for the completion of the Work, as set out in the Conservation Plan in Appendix 2, within three years following the date of adoption of the Bylaw authorizing this Agreement.

21. The Owner shall not construct the Infill House on the Land until the Owner has completed the Work in respect of the Heritage Building to the satisfaction of the City's Director of Climate Action, Planning and Development, has provided the Certification of Compliance described in section 8(d) above.
22. The City may, notwithstanding that such a permit may be issuable under the City's zoning and building regulations and the BC Building Code, withhold a building permit or heritage alteration permit applied for in respect of the Infill House if the Owner has not completed the Work in respect of the Heritage Building, to the satisfaction of the City's Director of Climate Action, Planning and Development.
23. The Owner shall complete all actions required for the completion of the Infill House, as set out in Approved Plans in Appendix 5, within five years following the date of adoption of the Bylaw authorizing this Agreement.

No Subdivision

24. The Owners shall not subdivide the Lands or the buildings located on the Lands by any method, including by way of a building strata plan under the provisions of the Strata Property Act (British Columbia), or any successor legislation dealing with the creation of separate titles to buildings or portions of a building.

Inspection

25. Upon request by the City, the Owners shall advise or cause the Registered Professional to advise, the City's Climate Action, Planning and Development Department, of the status of the Work.
26. Without limiting the City's power of inspection conferred by statute and in addition to such powers, the City shall be entitled at all reasonable times and from time to time to enter onto the Land for the purpose of ensuring that the Owner is fully observing and performing all of the restrictions and requirements in this Agreement to be observed and performed by the Owner.
27. The Owner agrees that the City may, notwithstanding that a final inspection may be issuable under the City's zoning and building regulations and the BC Building Code, withhold a final inspection or occupancy certificate applied for in respect of the Heritage Building or the Infill House if the Owner has not completed the Work with respect to the Heritage Building or construction of the Infill House to the satisfaction of the City's Director of Climate Action, Planning and Development.

Conformity with City Bylaws

28. The City of New Westminster Zoning Bylaw No. 6680, 2001, is varied and supplemented in its application to the Land in the manner and to the extent provided and attached as Appendix 6.

29. The Owner acknowledges and agrees that, except as expressly varied by this Agreement, any development or use of the Land, including any construction, alteration, rehabilitation, restoration and repairs of the Heritage Building or Infill house, must comply with all applicable bylaws of the City.

No Application to Building Interiors

30. Unless otherwise stated in this Agreement or set out in the Conservation Plan, the terms and conditions of this Agreement respecting the Heritage Building and Infill House apply only to the structure and exterior of the buildings, including without limitation the foundation, walls, roof, and all exterior doors, stairs, windows and architectural ornamentation.

Enforcement of Agreement

31. The Owner acknowledges that it is an offence under Section 621(1)(c) of the *Local Government Act* to alter the Land or the Heritage Building in contravention of this Agreement, punishable by a fine of up to \$50,000.00 or imprisonment for a term of up to 2 years, or both.
32. The Owner acknowledges that it is an offence under Section 621(1)(b) of the *Local Government Act* to fail to comply with the requirements and conditions of any heritage alteration permit issued to the Owner pursuant to this Agreement and Section 617 of the *Local Government Act*, punishable in the manner described in the preceding section.
33. The Owner acknowledges that, if the Owner alters the Land, the Heritage Building or the Infill House in contravention of this Agreement, the City may apply to the British Columbia Supreme Court for:
- (a) an order that the Owner restore the Land or the Heritage Building or the Infill House, or all, to their condition before the contravention;
 - (b) an order that the Owner undertake compensatory conservation work on the Land, the Heritage Building, or the Infill House;
 - (c) an order requiring the Owner to take other measures specified by the Court to ameliorate the effects of the contravention; and
 - (d) an order authorizing the City to perform any and all such work at the expense of the Owner.
34. The Owner acknowledges that, if the City undertakes work to satisfy the terms, requirements or conditions of any heritage alteration permit issued to the Owners pursuant to this Agreement upon the Owner's failure to do so, the City may add the cost of the work and any incidental expenses to the taxes payable with respect to the Land, or may recover the cost from any security that the Owner has provided to the City to guarantee the performance of the terms, requirements or conditions of the permit, or both.

35. The Owner acknowledges that the City may file a notice on title to the Land in the Land Title Office if the terms and conditions of this Agreement have been contravened.
36. The City may notify the Owner in writing of any alleged breach of this Agreement and the Owner shall have the time specified in the notice to remedy the breach. In the event that the Owner fails to remedy the breach within the time specified, the City may enforce this Agreement by:
- (a) seeking an order for specific performance of the Agreement;
 - (b) any other means specified in this Agreement; or
 - (c) any means specified in the *Community Charter* or the *Local Government Act*,
- and the City's resort to any remedy for a breach of this Agreement does not limit its right to resort to any other remedy available at law or in equity.

Statutory Authority Retained

37. Nothing in this Agreement shall limit, impair, fetter, or derogate from the statutory powers of the City, all of which powers may be exercised by the City from time to time and at any time to the fullest extent that the City is enabled.

Indemnity

38. The Owner hereby releases, indemnifies and saves the City, its officers, employees, elected officials, agents and assigns harmless from and against any and all actions, causes of action, losses, damages, costs, claims, debts and demands whatsoever by any person, arising out of or in any way due to the existence or effect of any of the restrictions or requirements in this Agreement, or the breach or non-performance by the Owner of any term or provision of this Agreement, or by reason of any work or action of the Owner in performance of its obligations under this Agreement or by reason of any wrongful act or omission, default, or negligence of the Owner.
39. In no case shall the City be liable or responsible in any way for:
- (a) any personal injury, death or consequential damage of any nature whatsoever, howsoever caused, that be suffered or sustained by the Owner or by any other person who may be on the Land; or
 - (b) any loss or damage of any nature whatsoever, howsoever caused to the Land, or any improvements or personal property thereon belonging to the Owner or to any other person,

arising directly or indirectly from compliance with the restrictions and requirements in this Agreement, wrongful or negligent failure or omission to comply with the restrictions and requirements in this Agreement or refusal, omission or failure of the City to enforce or

require compliance by the Owner with the restrictions or requirements in this Agreement or with any other term, condition, or provision of this Agreement.

No Waiver

40. No restrictions, requirements, or other provisions of this Agreement shall be deemed to have been waived by the City unless a written waiver signed by an officer of the City has first been obtained, and without limiting the generality of the foregoing, no condoning, excusing or overlooking by the City on previous occasions of any default, nor any previous written waiver, shall be taken to operate as a waiver by the City of any subsequent default or in any way defeat or affect the rights and remedies of the City.

Interpretation

41. In this Agreement, "Owner" shall mean all registered owners of the Land or subsequent registered owners of the Land, as the context requires or permits.

Headings

42. The headings in this Agreement are inserted for convenience only and shall not affect the interpretation of this Agreement or any of its provisions.

Appendices

43. All appendices to this Agreement are incorporated into and form part of this Agreement.

Number and Gender

44. Whenever the singular or masculine or neuter is used in this Agreement, the same shall be construed to mean the plural or feminine or body corporate where the context so requires.

Joint and Several

45. If at any time more than one person (as defined in the *Interpretation Act* (British Columbia)) owns the Land, each of those persons will be jointly and severally liable for all of the obligations of the Owner under this Agreement.

Successors Bound

46. All restrictions, rights and liabilities herein imposed upon or given to the respective parties shall extend to and be binding upon their respective heirs, executors, administrators, successors and assigns.

IN WITNESS WHEREOF the Owner and the City have executed this Agreement as of the date written above.

Signed, Sealed and Delivered in the
presence of:

)
)
)
)
)

Name

GARY JOHN HOLISKO

Address

ROSANNE MARIE HOOD

Occupation

ROSANNE MARIE HOOD

THE CORPORATION OF THE CITY OF NEW WESTMINSTER

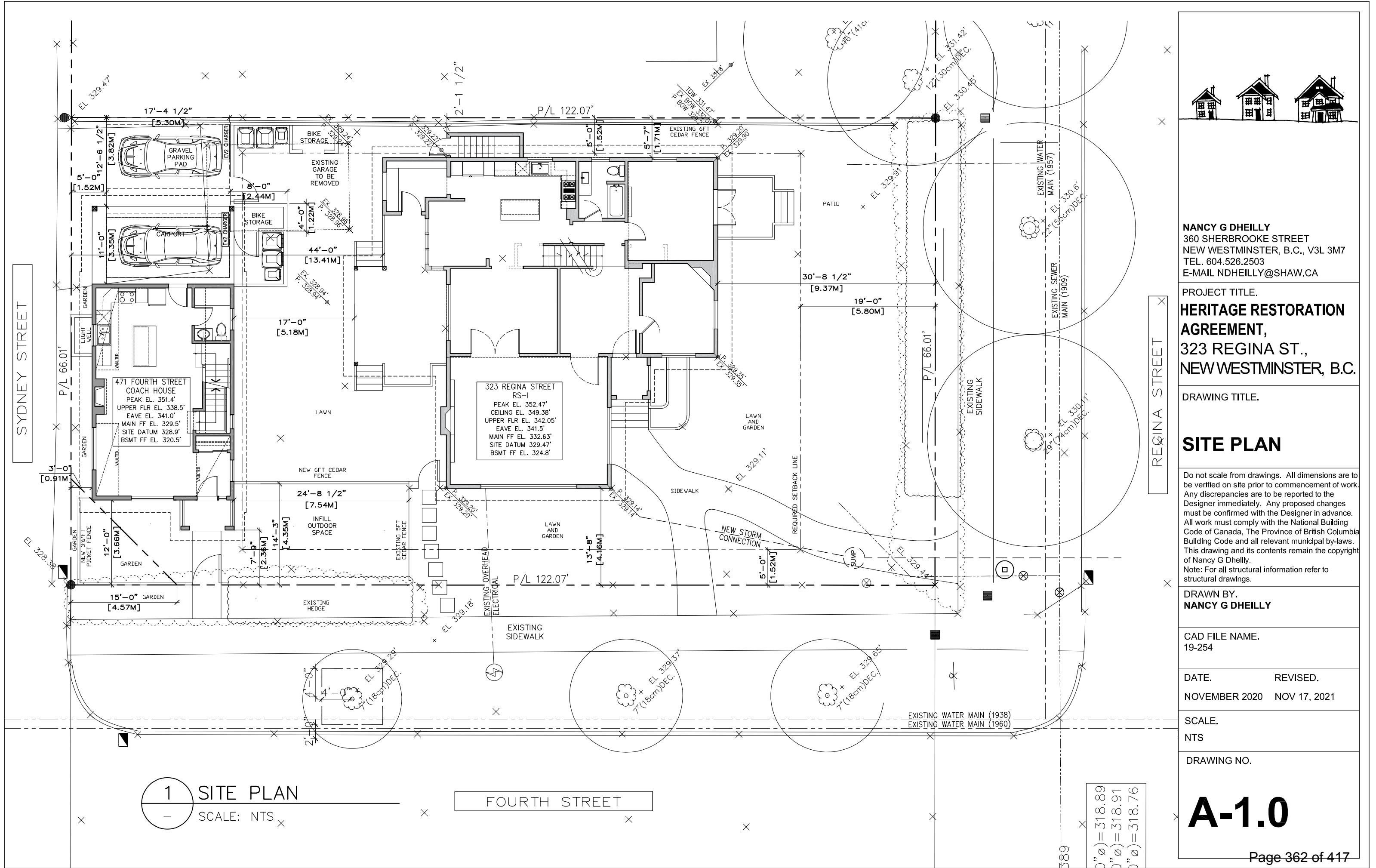
by its authorized signatories:

Mayor Jonathan X. Cote

Jacqueline Killawee, City Clerk

APPENDIX 1

SITE PLAN



1 SITE PLAN
— SCALE: NTS

FOURTH STREET



NANCY G DHEILLY
360 SHERBROOKE STREET
NEW WESTMINSTER, B.C., V3L 3M7
TEL. 604.526.2503
E-MAIL NDHEILLY@SHAW.CA

PROJECT TITLE.
HERITAGE RESTORATION AGREEMENT,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.

SITE PLAN

Do not scale from drawings. All dimensions are to be verified on site prior to commencement of work. Any discrepancies are to be reported to the Designer immediately. Any proposed changes must be confirmed with the Designer in advance. All work must comply with the National Building Code of Canada, The Province of British Columbia Building Code and all relevant municipal by-laws. This drawing and its contents remain the copyright of Nancy G Dheilly. Note: For all structural information refer to structural drawings.

DRAWN BY.
NANCY G DHEILLY

CAD FILE NAME.
19-254

DATE. REVISED.
NOVEMBER 2020 NOV 17, 2021

SCALE.
NTS

DRAWING NO.

A-1.0

APPENDIX 2
CONSERVATION PLAN

Heritage Conservation Plan

Edgar House, 323 Regina Street, New Westminster, BC

July 24, 2021



Fig. 1: View of the front of Edgar House at 323 Regina Street, New Westminster, BC, 2020, as visible from the corner of Regina Street and Fourth Street. (Source: Holisko)

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528.0 Introduction

The subject house, Edgar House, is a Storybook style, one and a half storey, stuccoed, wood-frame construction with concrete foundation located at 323 Regina Street in New Westminster (Fig. 2). It is located in the northwest corner of the Queen's Park neighbourhood in New Westminster.



Fig. 2: Map of the area surrounding 323 Regina Street, outlined in yellow. (Source: City of New Westminster Map Viewer, CityViews, 2020)

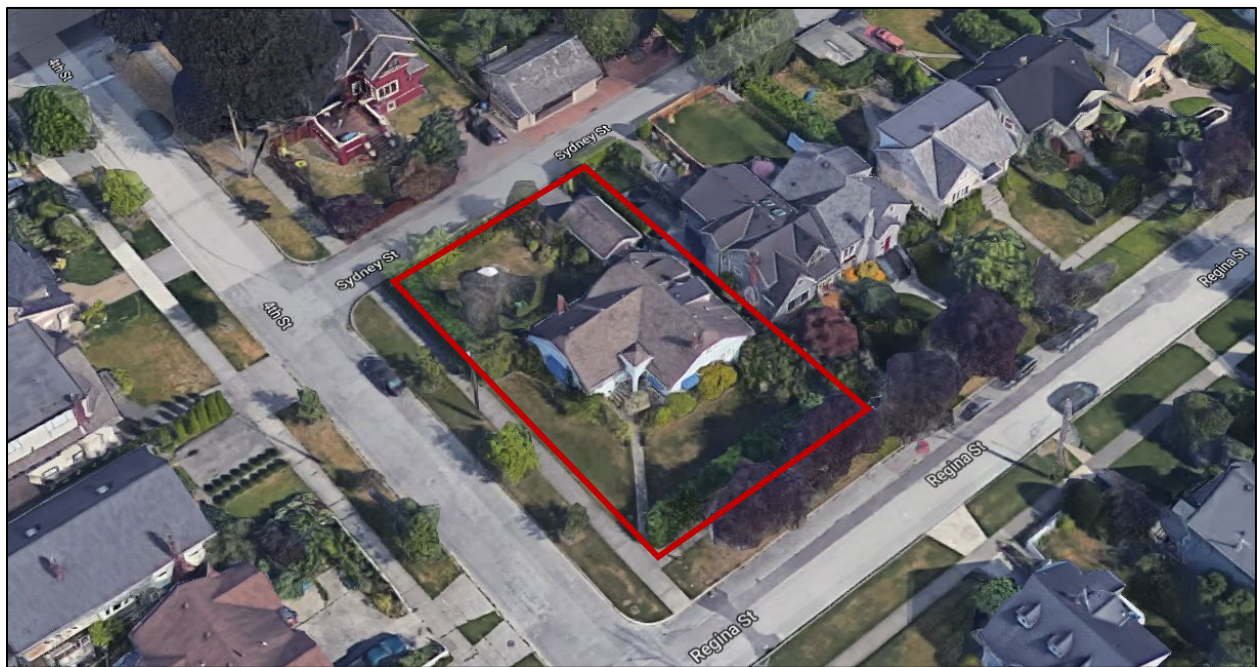
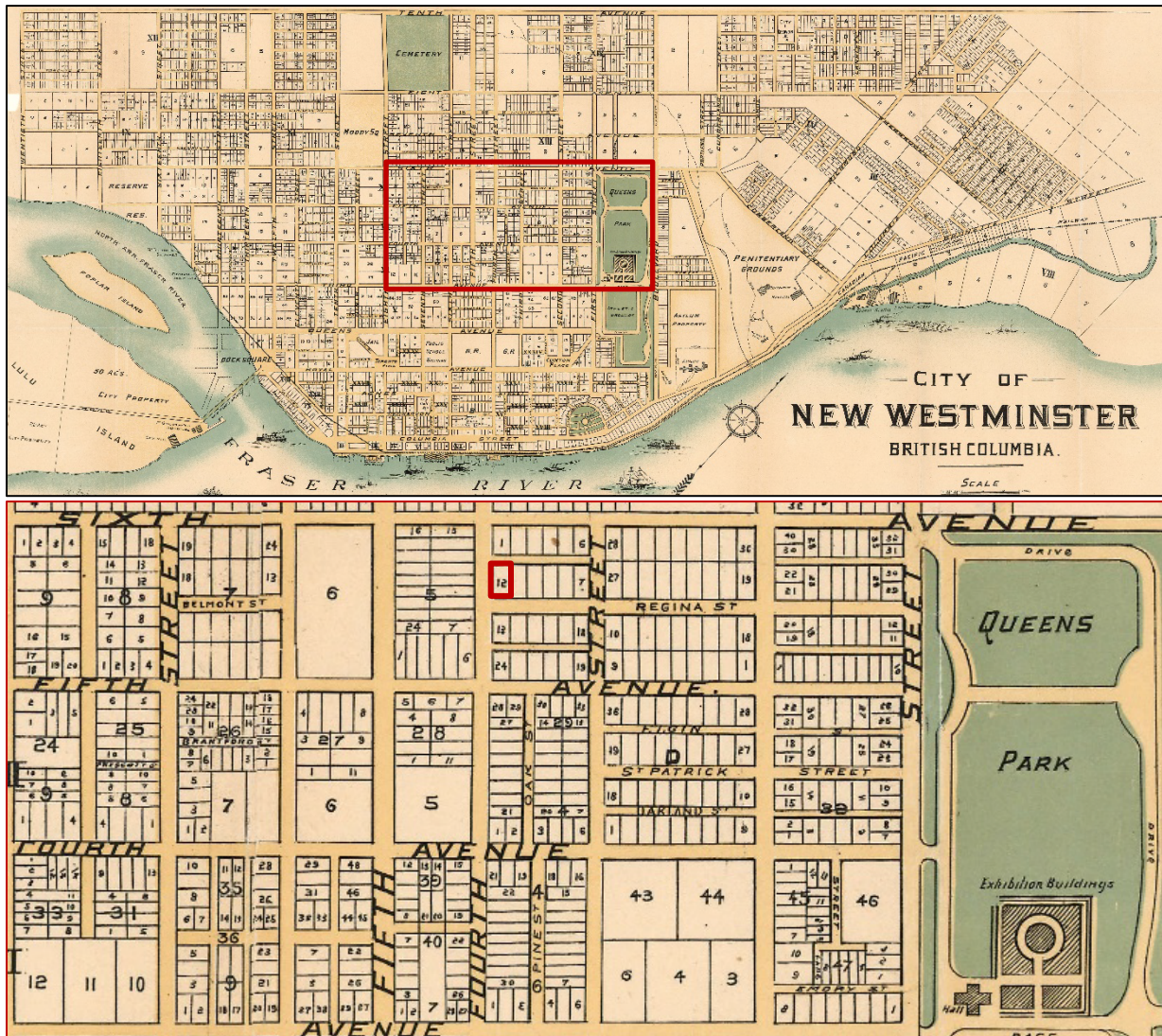


Fig. 3: Aerial view of 323 Regina Street, outlined in red. (Source: Google, 2019)

2.0 Historic Context

In 1859, the British Royal Engineers surveyed the area to become known as New Westminster, which at the time was to be the new colonial capital of the crown colony of British Columbia (Hainsworth and Freund-Hainsworth 2005, pp. 18-19). They overlaid a grid pattern on the natural topography of the area (Fig. 4a), parallel to the Fraser River (Mather and McDonald 1958, p. 22). The design, still present today, had the streets running up the hill, perpendicular to the river, and the avenues across the area, parallel to the river. The head engineer, Colonel Richard Moody, envisioned a formally planned “Garden City” with prominent public parks and elegant wide avenues (Wolf 2005, pp. 18-20). These well-landscaped parks and avenues are clearly visible in the 1928 aerial photograph of the area (Fig. 7 below).



Figs. 4a and 4b: Fig. 4a (above) shows the wider context of the City of New Westminster, 1892. Note the grid pattern of the streets and avenue. In Fig. 4a (above), the neighbourhood of 323 Regina Street is outlined in red. Its lot is outlined in bold red in Fig. 4b (below). (Source: City of Vancouver Archives, AM1594-MAP 617)

“The Royal Engineers marked out the area now known as Queen’s Park including road allowances for wide streets and landscaped boulevards, land reserves, and squares in 1859. The next year the Royal Engineers surveyed 75.5 acres for what became Queen’s Park itself. The area very soon began to attract merchants and entrepreneurs seeking a prestigious location away from the noise and pollution of the downtown and river front.” (DCD *et al.* 2009, p. 41). Shortly thereafter, New Westminster experienced two major building

booms. The first beginning in the 1880s with the extension of the Canadian Pacific Railway line and the second in the 1900s, following the destructive fire of 1898 that destroyed much of Downtown (Mather and McDonald 1958). At the beginning of the 20th century, Queen's Park "was filled up as an elite residential neighbourhood. In 1906 Queen's Park acquired paved street and concrete sidewalks, in 1912 a sewer system, and a year later street curbs, making it the first fully serviced neighbourhood in New Westminster" (DCD *et al.* 2009, p. 42).

The subject property at 323 Regina Street is located in the northwest quadrant of this "elite residential neighbourhood" known as Queen's Park. Interestingly, it was a relatively later development in the neighbourhood, being built in 1928, compared to the numerous Edwardian era constructions, distinctly visible in a 1913 Fire Insurance Map (Figs. 5a and 5b). It is worth comparing this to a 1957 Fire Insurance Map (Fig. 6), which shows a few additional developments built during the interim decades, including the captioned study site, which is visible in a 1928 aerial photograph of the area, showing the property being developed (Fig. 7). A newspaper advert from the same year, illustrates and promotes the house and its numerous qualities (Fig. 8).

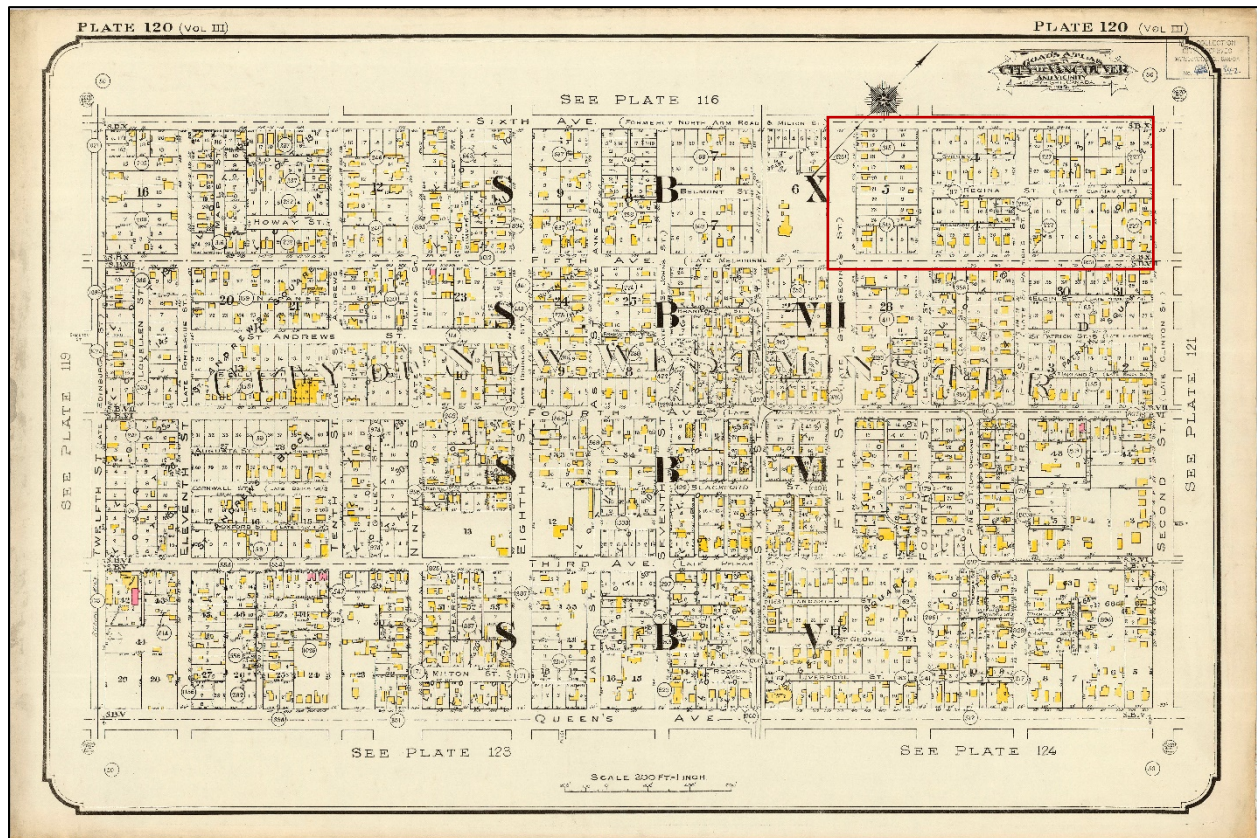


Fig. 5a: Fire Insurance Map of New Westminster, 1913. The neighbourhood of 323 Regina Street is outlined in red. The property is outlined in bolded red in Fig. 5b (below). (Source: City of Vancouver Archives, 1972-472.07, Plate 120)

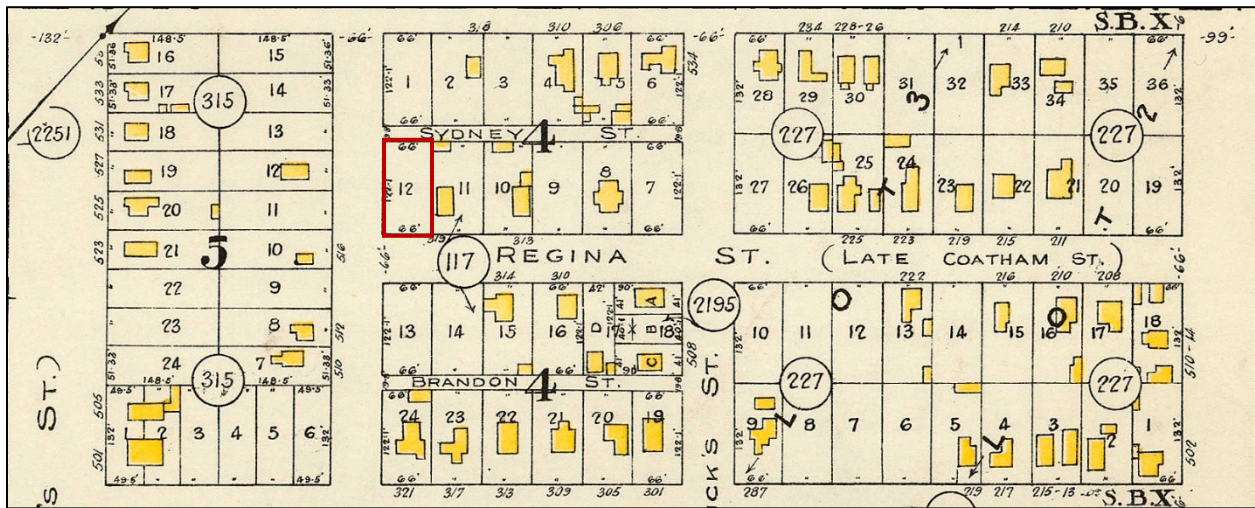


Fig. 5b: Excerpt of Fire Insurance Map of New Westminster, 1913. The empty lot of 323 Regina Street is outlined in red. (Source: City of Vancouver Archives, 1972-472.07, Plate 120)

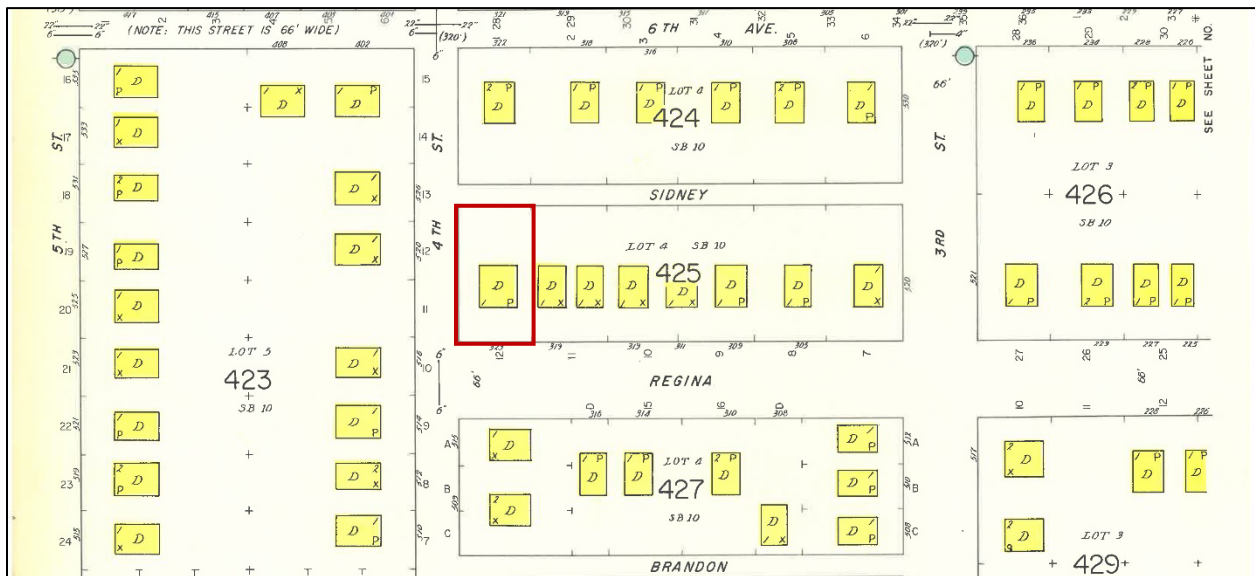


Fig. 6: Fire Insurance Map of New Westminster, 1957. The developed lot of 323 Regina Street is outlined in red. (Source: City of New Westminster Archives 1957, sheet 42)



Fig. 7: Section from a Royal Canadian Air Force aerial photograph of New Westminster, 1928. Note that 323 Regina Street has been cleared for development, however, no structure is yet built on the lot. (Source: Library & Archives Canada, AA287_058)

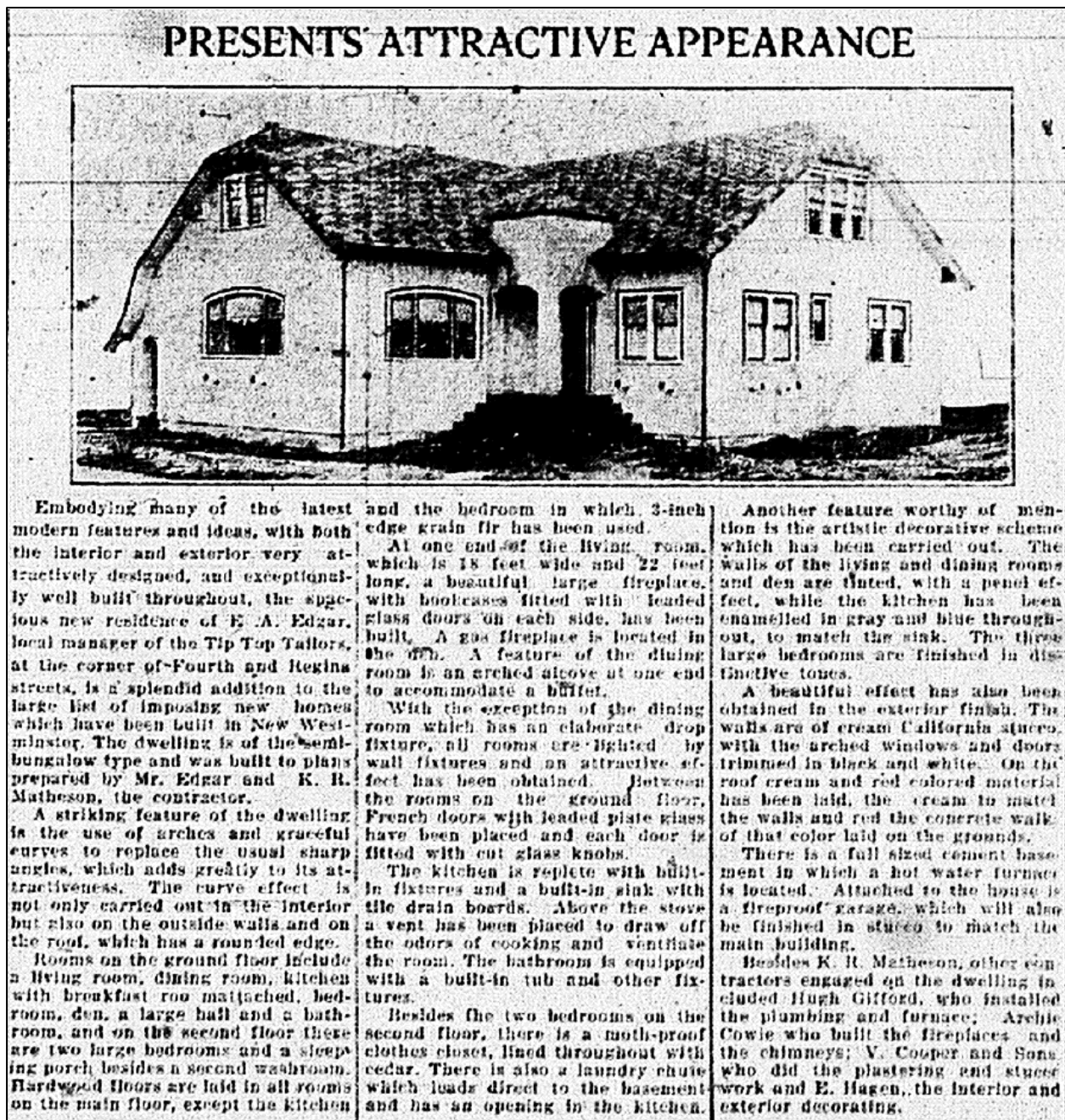


Fig. 8: Newspaper article on 323 Regina Street. (Source: The British Columbian, October 8, 1928, p. 7)

From the above newspaper clipping, the elements of particular note include (transcribed here for ease of reading):

- "The spacious new residence of E. A. Edgar, local manager of the Tip Top Tailors, at the corner of Fourth and Regina streets, is a splendid addition to the large list of imposing new homes which have been built in New Westminster."
- "The dwelling is of the semi-bungalow type and was built to plans prepared by Mr. Edgar and K.R. Matheson, the contractor."
- "A striking feature of the dwelling is the use of arches and graceful curves to replace the usual sharp angles, which adds greatly to its attractiveness. The curve effect is not only carried out in the interior, but also on the outside walls and on the roof, which has a rounded edge."

- "A beautiful affect has also been obtained in the exterior finish. The walls are of cream California stucco with the arched windows and doors trimmed in black and white. On the roof cream and red colored material has been laid, the cream to match the walls and red the concrete walk of that color laid on the grounds."
- "Attached to the house is a fireproof garage, which will also be finished in stucco to match the main building."
- "Besides K.R. Matheson, other contractors engaged on the dwelling included Hugh Gifford, who installed the plumbing and furnace; Archie Cowie who built the fireplaces and the chimneys; V. Cooper and Sons who did the plastering and stucco work and E. Hagen, the interior and exterior decorating."

The design of this house has elements of the English Storybook tradition, however, it most closely resembles the French Storybook style, which are typically "small and whimsical...with hipped or side-gabled roofs and a projecting living room wing (under an L-shaped roof, in some cases), with a turret tucked into the L and forming a shelter over the front door... Windows may have arched tops, and an arched, quoined opening in the turret may frame the front door. Their cladding is coloured stucco. Roof edges may be rolled as in the English Storybook Style" (VHF). This style, along with the English Storybook style, "emerged in North America after WWI. Soldiers returning from European battlefields brought with them a familiarity with architectural styles. Among these were French farmhouses and castles. Builders translated elements of these traditional buildings into practical cottages. After a period of upheaval, the value of the picturesque and the traditional increased following the war. This contributed to the development of the French Storybook style, with its quaint tower and European flair" (*ibid.*). Its cat-slide and jerkinhead roof connects to the English Storybook style as well (VHF).

323 Regina Street was recognized in the 1980s as having heritage significance and added to the City of New Westminster's Heritage Resource Inventory, being photographed and described as follows (Fig. 9). These elements have persisted and directly influence the site's Statement of Significance, outlined in the following section.

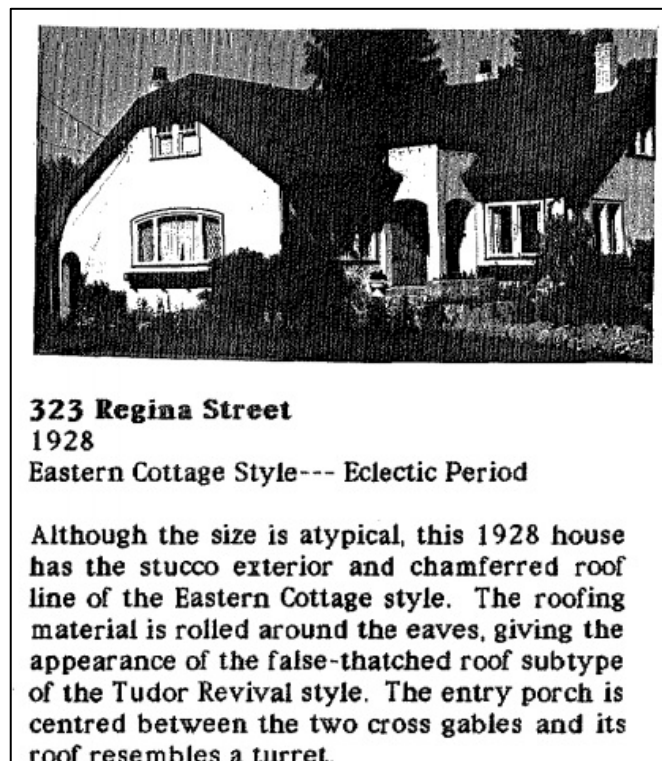


Fig. 9: Heritage inventory photograph and description of 323 Regina Street. (Source: Sleath 1989, p. 177)

3.0 Statement of Significance

The following is the Statement of Significance of Edgar House at 323 Regina Street.

3.1 Description of Historic Place

This historic place, Edgar House, is a Storybook style Cottage with a jerkinhead roof. It is a one and a half storey, stuccoed, wood-frame construction with concrete foundation. The entry porch is centred between its two cross gables and the roof over the entrance resembles a turret. The house sits on a prominent corner lot, stretching the length of 4th street from Regina Street to Sydney Street in the Queen's Park neighbourhood.

3.2 Heritage Value of Historic Place

Edgar House at 323 Regina Street has heritage value for its aesthetic and historic significance. Aesthetically, this house is an eye-catching, intact example of a Storybook style dwelling, with elements from both the French and English traditions. Its connection to the French Storybook style is seen in its various characteristic features, including: its L-shape and centred turret over its arched front entryway. Its connection to the English Storybook style is seen in elements such as its jerkinhead roof, as well as its low, sloping roof (its catslide) on its western corner. Shared elements of both Storybook styles include its rolled roofline giving it a false-thatched roof appearance, its stucco cladding, its asymmetrical design and its arched windows and doors. It was showcased in a 1928 newspaper article as a unique and attractive structure; a fact that still holds true today. Its uniqueness in the landscape contributes to this place's significance.

This house also has historic significance being among a rare stock of interwar period developments in the Queen's Park neighbourhood, being just shy of the decline that came with the Great Depression a year after its construction. It was built in 1928 with the help and input of various contractors and craftsman, named in the aforementioned article about the property. These individuals included the well-known and well-respected builder K.R. Matheson, as well as Hugh Gifford (for the plumbing and furnace), Archie Cowie (for its fireplaces and chimneys), V. Cooper and Sons (for the plastering and stucco work) and E. Hagen, (for the interior and exterior decorating). This house's namesake, Elmer Edgar, is also representative of the middle-class individuals working in New Westminster for the community, as he was the Manager of the local Tip Top Tailor's New Westminster branch. Tip Top Tailors is a Canadian company, founded in Toronto, that has been around since 1909.

3.3 Character Defining Elements

Key elements that define the heritage character of Edgar House at 323 Regina Street include:

- Its location in the Queen's Park neighbourhood.
- Its residential form, scale and massing as expressed by its one and a half storey height.
- Its jerkinhead roof and rolled shingles, imitating thatching, as well as its flared catslide on the western corner of its roof, connecting to the English Storybook style.
- Its French Storybook style elements as represented by its asymmetry and its L-shaped massing with a turret tucked in the 'L' forming a shelter over the front door.
- Its arched windows, doorways and doors.
- Its numerous wood windows featured on all sides of the house, in various sizes and configurations (some double-hung, some divided-light, some quarreled with diamond patterned panes, etc.)
- Its stuccoed exterior.

4.0 Research Findings

Neighbourhood: Queen's Park

Address & Postal Code: 323 Regina Street, V3L 1S8

Folio & PID: 08514000 & 013-593-285

Legal Description: Lot 12; Suburban Block 10 of Lot 4; New West District; Plan NWP2620

Zoning: Single Detached/RS-4

Builder & Date of completion: K.R. Matheson in 1928

Original Owner & Water Connection Connector and Year: Elmer A. Edgar & E.A. Edgar on July 14, 1928

The following tables are a consolidated summary of the residents of 323 Regina Street, as determined from the available city directories for New Westminster, as well as a list of the construction dates of the surrounding properties, illustrating the range of ages to this section of the street (visualized in Fig. 10).

Table 1: Consolidated list of the occupants of 323 Regina Street from the available city directories (Source: Vancouver Public Library, 1928 to 1955; and New Westminster Archives, 1970, 1979, 1985, 1991, 1992, 1998)

Year(s)	Name(s)	Occupation (if listed)
1928 – 1945	Elmer A. Edgar (Elverie B.)	Branch Manager, Tip Top Tailor
1946 – 1955	R. Gordon Quennell (Marion L.)	Retired
1970	Elliot E Nelles	Not listed
1979	Joyce M. Hall/Kath Hall	Not listed
1985 – 1998	R. T. Hall	Not listed

Table 2: Consolidated list of the construction dates for the properties surrounding 323 Regina Street, New Westminster, BC. (Source: BC Assessment)

Address	Year Built	Configuration
512 Third Street	1907	3 bedrooms, 2 baths
520 Third Street	1941	5 bedrooms, 3 baths
305 Regina Street	1910	3 bedrooms, 3 baths
308 Regina Street	1911	5 bedrooms, 3 baths
309 Regina Street	1936	2 bedrooms, 1 bath
310 Regina Street	1909	5 bedrooms, 2 baths
311 Regina Street	1939	2 bedrooms, 1 bath
313 Regina Street	1939	4 bedrooms, 2 baths
314 Regina Street	2000	4 bedrooms, 5 baths
316 Regina Street	1998	4 bedrooms, 3 baths
317 Regina Street	1936	4 bedrooms, 2 baths
319 Regina Street	1893	4 bedrooms, 3 baths
323 Regina Street	1928	4 bedrooms, 2 baths
514 Fourth Street	1926	4 bedrooms, 2 baths
515 Fourth Street	1940	3 bedrooms, 2 baths
516 Fourth Street	1911	4 bedrooms, 3 baths
518 Fourth Street	1973	3 bedrooms, 3 baths
520 Fourth Street	1912	5 bedrooms, 3 baths
526 Fourth Street	1913	5 bedrooms, 3 baths
528 Fourth Street	2012	3 bedrooms, 4 baths
402 Sixth Avenue	1915	4 bedrooms, 3 baths
322 Sixth Avenue	1921	6 bedrooms, 4 baths
318 Sixth Avenue	1912	4 bedrooms, 3 baths
316 Sixth Avenue	1924	3 bedrooms, 2 baths
310 Sixth Avenue	1908	4 bedrooms, 1 bath
306 Sixth Avenue	1911	2 bedrooms, 3 baths



Fig. 10: Map of the area surrounding 323 Regina Street, outlined in blue, with the construction years listed for the buildings in the immediate vicinity of the study site. Note the range of years. (Source: BC Assessment)

In summary, there are 26 houses along this section of Regina Street, 4th Street and Sixth Avenue. As a point of reference for understanding the surrounding neighbourhood and streetscape, their time periods breakdown as follows:

- 4% were built in the 1890s (1 out of 26)
- 12% were built in the 1900s (3 out of 26);
- 31% from the 1910s (8 out of 26);
- 15% from the 1920s (4 out of 26);
- 15% from the 1930s (4 out of 26);
- 8% from the 1940s (2 out of 26);
- None from the 1950s nor the 1960s;
- 4% from the 1970s (1 out of 26);
- None from the 1980s;
- 4% from the 1990s (1 out of 26); and
- 8% from the 21st century (2 out of 26).

4.1 Researcher's Note

In researching the captioned study site, Edgar House, it has been interesting and surprising to note that it is not included in the Queen's Park Heritage Conservation Area (HCA). On account of its heritage value – specifically, its aesthetic value as a somewhat rare and intact example of the whimsical Storybook style and its historical significance as an interwar pre-Great Depression development built by well-known tradesmen for a prominent Queen's Park family (in fact, already recognized in the HCA with their property at 415 Third Street (NWA 2004)) – it is unclear why this Edgar property at 323 Regina Street was omitted from the HCA. This seemed an important aspect to note amongst the site's research findings.

5.0 Archival Photographs

Unfortunately, no other historical photographs of the property were available beyond the 1928 newspaper article (Fig. 11) and the accompanying photograph of the 1989 heritage inventory description (Fig. 12). It is interesting to note the few changes to the property, such as the addition of a window box on the front window, which was apparently done shortly after the house was built in 1928 by a local ironworker. Other changes of note are the switch of the front entry staircase from being double-sided to single-sided and the addition of a chimney on the southeast corner, which has since been removed.



Fig. 11: Historical photograph of 323 Regina Street, 1928, extracted from the newspaper article on the property. (Source: The British Columbian, October 8, 1928, p. 7)



Fig. 12: Historical photograph of 323 Regina Street, 1989, taken from Volume 2 of the Heritage Resource Inventory. Note the largely similar look and condition of the property, with only minor changes, such as the addition of a window box on the front window, the change of the front entry staircase from being double-sided to being single-sided and the addition of another chimney, which has since been removed (please see the red arrows pinpointing these changed areas). (Source: Sleath 1989, p. 177)

6.0 Current Photographs



Fig. 13: Southern corner view of Edgar House at 323 Regina Street, 2020. (Source: Holisko)



Fig. 14: Eastern corner view of Edgar House at 323 Regina Street, 2020. (Source: Holisko)



Fig. 15: Northeastern side of Edgar House at 323 Regina Street, BC, 2020. (Source: Holisko)



Fig. 16: Northwestern side of Edgar House at 323 Regina Street, 2020. (Source: Holisko)

7.0 Conservation Objectives

Edgar House at 323 Regina Street will be preserved as part of a Heritage Revitalization Agreement in order to build a laneway house on their large lot and stratify their property. The proposed changes do not affect the Heritage Values nor the Character Defining Elements of this historic place.

A number of changes and some restoration work has already taken place to this historic place. For a comparison view of the work already completed, please refer to Figs. 17a and 17b below, from 2019 and 2020 respectively.



Figs. 17a and 17b: Comparative views of Edgar House at 323 Regina Street, 2019 (top) and 2020 (bottom), illustrating the various work done on site, listed in full on the following page. (Sources: Vallee (top) and Holisko)

For record purposes, work done is catalogued and summarized here, based on the information provided by the current owners:

- A similarly pitched jerkinhead roof was put over the deck on the northern side of the property, without the rolling eaves featured on the heritage building, presumably to follow Standard 11 of the Canadian *Standards and Guidelines*, to ensure its distinguishability as a new addition.
- The character-defining catslide on the western corner was repaired and restored, while being better revealed in moving the fence and installing a privacy gate.
- A small mudroom was added to the northern corner of the property, re-purposing one of the original windows that had to be removed from the kitchen.
- A deck and patio were added on the eastern corner of the property, along with a wrought iron fence, in a similar look to the window box ironwork that was added to the house shortly after it was built.
- An additional window box was also added to the south face of the property to match the one from the front.
- A set of windows from the south face of the house were re-purposed on site and replaced by wooden French doors, providing an egress point and access to the newly added south side deck and patio.
- One original window was badly water damaged and unsalvageable.
- The two small dormers along the northeastern, back side of the roof were combined into one longer one.
- Vinyl windows were installed in the two bathrooms and laundry room, along the northeastern, back side of the house with low visibility from the street. This is deemed an acceptable change on account of the minimal visual impact to the streetscape, since they are not visible from the street.
- The upper floor wood windows, facing Regina Street and Sydney Lane were replaced in-kind, with replica wood windows. The windows facing Fourth Street were not replaced and are still original.
- Areas of the stucco wall were also damaged and needed extensive patching, particularly around the front entrance and the side facing Fourth Street.
- The perimeter drain was replaced and at that time (as visible in the comparative photographs) a lot of landscaping was removed from the site, both from surrounding the house as well as from the corner portion of the hedge along Regina Street. This was to allow a clearer view of the house's front entrance, making it more accessible and visible, since the front entry largely faces Fourth Street, despite its address technically being Regina Street. The hedge was only partially removed to maintain some privacy for the new side patio on the eastern corner of the property.
- At this time, all of the drainage gutters and downspouts were replaced.
- The later-addition chimney located on the south corner of the house was removed.
- The later-addition blue awnings over the various windows were also removed.
- The house's original colour scheme (based on the 1928 newspaper article on the property) was restored.

Preservation, Restoration and Rehabilitation were and are the conservation objectives for the building. As defined by the *Standards and Guidelines for the Conservation of Historic Places in Canada* (2nd edition):

Preservation: The action or process of protecting, maintaining and/or stabilizing the existing materials, form and integrity of an historic place or of an individual component, while protecting its heritage value.

Restoration: The action or process of accurately revealing, recovering or representing the state of a historic place or of an individual component, as it appeared at a particular period in its history, while protecting its heritage value.

Rehabilitation: The action or process of making possible a continuing or compatible contemporary use of an historic place or of an individual component, through repair, alterations, and/or additions, while protecting its heritage value.

(Canada's Historic Places 2010, p. 255)

The conservation of Edgar House is focused on the preservation of the heritage house, including its various characteristic elements; restoration of its historical paint scheme; and rehabilitation of the front door and chimney. The following table summarizes the specific elements of Edgar House to be preserved, restored and rehabilitated (Table 3).

Table 3: Consolidated lists of the elements of Edgar House that are to be preserved, restored and rehabilitated. Note some have already been achieved

Preserved	Restored	Rehabilitated
Overall structure, including its form, scale and massing	Overall paint scheme	Front door
Rooflines		Chimney mortar
Stucco cladding		
All remaining original wood windows		

8.0 Building Description

Edgar House is a Storybook style Cottage, with elements from both the French and English traditions. It is a one and a half storey, stuccoed, wood-frame construction with concrete foundation. It is an L-shaped structure with a jerkinhead roof and rolled shingles, giving it a false-thatched look, as well as a flared catslide on its western corner roof. It has an elongated dormer on the northeast side of its roof (previously two dormers that have been combined). The entry porch is centred between its two cross gables and the roof over the arched entrance resembles a turret. It has numerous arched windows, doorways and doors as well as a range of wood windows on all sides of the house, in various sizes and configurations (some double-hung, some divided-light, some quarreled with diamond patterned panes, etc.). The site features a garage off of the north corner of the house in a similar look and style to the main property. The house sits on a prominent corner lot, stretching the length of 4th street from Regina Street to Sydney Street in the Queen's Park neighbourhood. It is one of the few 1920s houses remaining in the Queen's Park neighbourhood.

9.0 Condition Assessment

Overall, the exterior of Edgar House at 323 Regina Street appears to be in good to very good condition, based on the available exterior photographs. As outlined below there are just a few areas in need of minor attention.

9.1 Structure and Foundations

Overall, the condition of the walls and building envelope of Edgar House, from roof to foundation, appears to be good and having aged well. In particular, there are no major cracks visible in either the stuccoed walls or foundation. One small area of concern is the stone front steps that appear they could benefit from some minor cleaning and maintenance (Fig. 18) such as to remove moss/algae growth.



Fig. 18: Front stone steps and planter of Edgar House at 323 Regina Street, 2020, illustrating the minor maintenance concerns, such as moss growth and other plants growing between the stone slabs. (Source: Holisko)

9.2 Wood Elements

The visible, exterior wood elements, such as the doors, door frames, roof fascia and windows are, for the most part, in good condition. Any signs of deterioration are largely cosmetic, as illustrated and discussed further in the relevant sections below. Please note an internal inspection was not conducted to inspect the internal timber elements.

9.3 Roofing and Waterworks

The roof is in very good condition, overall (Figs. 19 and 20). It is difficult to determine the condition of the waterworks system from photographs, however, it is understood that these were recently replaced (with rounded aluminium ones to resemble the older more traditional style) and should therefore be in good working order. They should be checked regularly to ensure their continued efficient functioning.



Fig. 19: Front view of Edgar House at 323 Regina Street, 2020, illustrating the good condition of its roof. (Source: Holisko)



Fig. 20: Back view of Edgar House at 323 Regina Street, 2020, illustrating the good condition of its roof. (Source: Holisko)

9.4 Chimney

There is a chimney on the house, along its northwestern side (see Fig. 20 above), and it seems to be in largely good condition, with an intact chimney cap (Fig. 21a). It is worth noting that there are some signs of deterioration and loss of mortar, particularly in the areas that appear dark between the bricks (along the left side of Fig. 21b). The top of the chimney also appears that it could benefit from some cleaning and maintenance.



Figs. 21a and 21b: Fig. 21a (left) shows a detail shot of the Edgar House chimney, highlighting its largely good condition. Fig. 21b (right) shows a closer view of the chimney stack, showing some signs of deteriorating mortar and areas in need of cleaning (pinpointed by red arrows). (Sources: Holisko)

9.5 Windows and Doors

Some of the windows of the house have been replaced (or repurposed on site), although many are still original and, considering the age of the building, these intact windows and doors are in good to very good condition (as visible in Figs. 13 and 14 above and Fig. 22 below).



Fig. 22: The back deck of Edgar House at 323 Regina Street, 2020, illustrating the good condition of its original windows, with diamond patterned panes. (Source: Holisko)

Otherwise, the only other condition concern with regards to the windows and doors is with the front door, with its faded and splotchy staining (Fig. 23). It is hoped that this is simply a cosmetic concern that can be rectified by sanding and re-staining, although it should be inspected for any signs of rotting prior to any work being done on it.

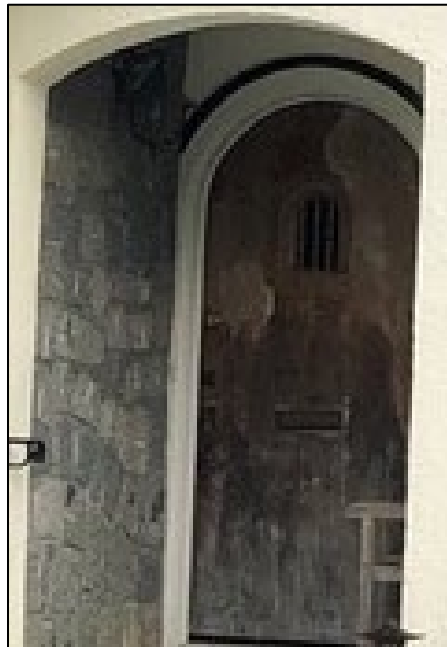


Fig. 23: Detail view of the front door of Edgar House at 323 Regina Street, 2020, illustrating its faded and splotchy staining. (Source: Holisko)

9.6 Cladding and Trimwork

As mentioned above, the stucco exterior appears to be in good condition, with no major issues identified, having been recently patched and restored. As for the trimwork, as discussed in the relevant sections above, these are also in very good shape.

9.7 Finishes

The finishes of the house are in good condition, having just recently been repainted to the historical colour scheme outlined in the 1928 newspaper article on the property and catalogued in section 10.7 below.

9.8 Landscaping

The landscaping on site is good, overall, with minimal landscaping growth near the structure and many plantings in pots, which helps to minimize the impact of roots on the building.

Despite these minor issues and concerns stated above, the overall condition of the property is good to very good. The owners should be commended for taking such good care of their property.

10.0 Recommended Conservation Procedures

10.1 Structure and Foundations – **Preservation**

- The main one and a half storey structure will be **preserved**.

10.2 Wood Elements – **Preservation**

- As addressed in greater detail in the relevant sections below, the wood elements will be **preserved**.

10.3 Roofing and Waterworks – **Preservation**

- The roofing and waterworks should be **preserved**, and regularly monitored and maintained to ensure their ongoing good condition.

10.4 Chimney – **Preservation** and **Rehabilitation**

- The chimney should be **preserved**, and **rehabilitated**, as needed. This should include regular monitoring and repointing by certified professionals, to avoid it needing to be rebuilt entirely down the road.
- Although certainly not recommended, if, overtime, it does degrade to the point of needing rebuilding, it should be dismantled to the roofline, the bricks should be cleaned and then re-used to rebuild the chimney with its original bricks, as much as possible.

10.5 Windows and Doors – **Preservation** and **Rehabilitation**

- The arched front door should be carefully **rehabilitated** (sanded down and re-stained) and **preserved**.
- All remaining original wood windows should be **preserved**.

- If there are concerns with regards to the performance of the original windows, an immediate measure to allow for better protection of them (while address heating and sound issues), is to install exterior wood storm windows on them. This would be the best conservation approach for their long-term preservation, if so desired, however, this is not a requirement.
- If this route is taken, the proposed storm windows should be traditional wood storm windows: Single pane, single light and of similar sash dimension to the window sash itself, to minimise the visual impact on the building and to allow the windows to continue to be visible on the exterior. They should be painted the same colour as the current. Dimensions should be the same as the window sash as per the proposed, historically appropriate colour scheme already used (and captured below). This is a reversible measure that would immediately benefit the building, providing greater protection to the house and improving its performance in relation to temperature control, energy efficiency and also from a noise perspective.

10.6 Cladding and Trimwork – **Preservation**

- The stucco should be **preserved**.
- The trims should be **preserved**, being monitored and maintained overtime, as needed.

10.7 Finishes – **Preservation**

- The current finish is based on the 1928 newspaper article on the house that describes its colour scheme as follows: “The walls are of cream California stucco with the arched windows and doors trimmed in black and white” (The British Columbian, October 8, 1928, p. 7). The selected colours were VC-1 Oxford Ivory for the body (from the Historical True Colours Palette; VHF 2012); Aura Low Lustre 634 for the white trim; and Regal Soft Gloss K403-80 for the black trim.
- This colour scheme should be **preserved** and maintained.
- For any eventual re-painting, follow Master’s Painters’ Institute, Repainting Manual procedures, including removing loose paint down to next sound layer, clean surface with mild TSP solution with gentlest means possible and rinse with clean water; do not use power-washing.

10.8 Landscaping

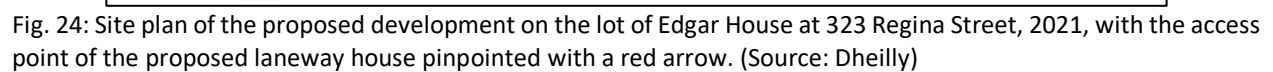
- Any additional landscaping being put in should have a minimum 2-ft clearance between the vegetation and the building face. This is preferable to ensure there is sufficient space from the structure and to remove any threat to the foundation or the building’s finishes over time.

11.0 Proposed Alterations and Future Changes

11.1 Proposed Alterations

The major proposed alterations to the property are:

- 1) Building a laneway house on the property (Figs. 24 and 25); and
- 2) Stratifying the property.



The proposed changes are considered a reasonable intervention given generally accepted conservation standards, rehabilitation needs and site conditions, in particular its large lot size. These proposed changes do not affect the Heritage Values and Character Defining Elements of the building.

Any future changes to the building's configuration, particularly any additions, should be carefully considered for minimal effect on the Heritage Values as embodied in the Character Defining Elements (CDEs) listed in the building's Statement of Significance (section 3.0 above).

Following completion of the outlined conservation work, the owner must maintain the building and land in good repair and in accordance with generally accepted maintenance standards. All work should follow the *Standards and Guidelines for the Conservation of Historic Places in Canada (2nd Edition)*. The Local Government determines the acceptable level or condition to which the heritage building is maintained through the *Heritage Maintenance Bylaw* (CCNW 2018). As with the Heritage Conservation Plan, the maintenance standards apply only to the exterior of the building.

It is important to establish a maintenance plan using the information below:

- Ensure site runoff drainage is directed away from the building.
- Maintain a minimum 2-ft clearance between vegetation and building face and a 12-inch-wide gravel strip against the foundation in planted areas, if possible.
- Do not permit vegetation (such as vines) to attach to the building.

b. Foundation

- Review exterior and interior foundations, where visible, for signs of undue settlement, deformation or cracking.
- If encountered, seek advice from a professional Engineer, immediately.
- Ensure perimeter drainage piping is functional.
- Arrange a professional drainage inspection every three to five years.

c. Wood Elements

- Maintaining integrity of the exterior wood elements is critical in preventing water ingress into the building. Annual inspection of all wood elements should be conducted.
- Closely inspect highly exposed wood elements for deterioration. Anticipate replacement in kind of these elements every 10 to 15 years.
- Any signs of deterioration should be identified and corrective repair/replacement action carried out. Signs to look for include:
 - Wood in contact with ground or plantings;
 - Excessive cupping, loose knots, cracks or splits;
 - Open wood-to-wood joints or loose/missing fasteners;
 - Attack from biological growth (such as moss or moulds) or infestations (such as carpenter ants);
 - Animal damage or accumulations (such as chewed holes, nesting, or bird/rodent droppings). These should be approached using Hazardous Materials procedures; and
 - Signs of water ingress (such as rot, staining or mould).
- Paint finishes should be inspected every three to five years and expect a full repainting every seven to ten years. Signs to look for include:
 - Bubbling, cracks, crazing, wrinkles, flaking, peeling or powdering; and
 - Excessive fading of colours, especially dark tones.
- Note all repainting should be as per the recommended historic colours in section 10.7 above.

d. Windows and Doors

- Replace cracked or broken glass as it occurs.
- Check satisfactory operation of windows and doors. Poor operation can be a sign of building settlement distorting the frame or sashes or doors may be warped.
- Check condition and operation of hardware for rust or breakage. Lubricate annually.
- Inspect weather stripping for excessive wear and integrity.

e. Roofing and Rainwater Works

- Inspect roof condition every five years, in particular looking for:
 - Loose, split or missing shingles, especially at edges, ridges and hips;
 - Excessive moss growth and/or accumulation of debris from adjacent trees; and
 - Flashings functioning properly to shed water down slope, especially at the chimneys.
- Remove roof debris and moss with gentle sweeping and low-pressure hose.
- Plan for roof replacement at around 18 to 22 years.
- Annually inspect and clean gutters and flush out downspouts. Ensure gutters positively slope to downspouts to ensure there are no leaks or water splashing onto the building.

- Ensure gutter hangers and rainwater system elements are intact and secure.
- Ensure downspouts are inserted into collection piping stub-outs at grade and/or directed away from the building onto concrete splash pads.

f. General Cleaning

- The building exterior should be regularly cleaned depending on build up of atmospheric soot, biological growth and/or dirt up-splash from the ground.
- Cleaning prevents build up of deleterious materials, which can lead to premature and avoidable maintenance problems.
- Windows, doors and rainwater works should be cleaned annually.
- When cleaning always use the gentlest means possible, such as soft bristle brush and low-pressure hose. Use mild cleaner if necessary, such as diluted TSP or Simple Green ©.
- Do not use high-pressure washing as it will lead to excessive damage to finishes, seals, caulking and wood elements and it will drive water in wall assemblies and lead to larger problems.

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APPENDIX 3

CONFIRMATION OF COMMITMENT BY REGISTERED PROFESSIONAL

Date: _____

City of New Westminster

511 Royal Avenue

New Westminster, BC

V3L 1H9

Attention: Director of Climate Action, Planning and Development

Re: Heritage Revitalization Agreement for 323 Regina Street

The undersigned hereby undertakes to be responsible for field reviews of the construction carried out at the captioned address for compliance with the requirements of Appendix 2 (Conservation Plan) of the Heritage Revitalization Agreement applicable to the property, which the undersigned acknowledges having received and reviewed, and undertakes to notify the City of New Westminster in writing as soon as possible if the undersigned's contract for field review is terminated at any time during construction. This letter is not being provided in connection with Part 2 of the British Columbia Building Code, but in connection only with the requirements of the Heritage Revitalization Agreement.

Registered Professional's Name

Address

Telephone No.

Signature or Seal

APPENDIX 4

CERTIFICATION OF REGISTERED PROFESSIONAL

Date: _____

City of New Westminster

511 Royal Avenue

New Westminster, BC

V3L 1H9

Attention: Director of Climate Action, Planning and Development

Re: Heritage Revitalization Agreement for 323 Regina Street

I hereby give assurance that I have fulfilled my obligations for field review as indicated in my letter to the City of New Westminster dated _____ in relation to the captioned property, and that the architectural components of the work comply in all material respects with the requirements of Appendix 2 (Conservation Plan) of the Heritage Revitalization Agreement referred to in that letter. This letter is not being provided in connection with Part 2 of the British Columbia Building Code, but in connection only with the requirements of the Heritage Revitalization Agreement.

Registered Professional's Name

Address

Telephone No.

Signature or Seal

APPENDIX 5
APPROVED PLANS

PROJECT DATA – 323 REGINA STREET

EXT’G LEGAL DESCRIPTION:	LOT 12 OF LOT 4, SB 10, NWD, PLAN 2620	
CIVIC ADDRESS:	323 REGINA STREET, NEW WESTMINSTER, B.C.	
CURRENT ZONING/ USE:	RS–4 RESIDENTIAL DISTRICT	
	PERMITTED	EXISTING
SITE AREA:	6000 SF MIN.	66.0x122.07=8,056.62 SF
MEAN BLDG. HEIGHT (DATUM: EL:329.47’)	25.0 FT	17.5 FT
MAXIMUM HEIGHT:	35.0 FT	23.0 FT
SITE COVERAGE:	35% (2,819.8 SF)	20.4% (1,643.4 SF)
SETBACKS OF BUILDING:		
FRONT:	19.0 FT	30.71 FT
REAR:	24.41 FT	44.0 FT
SIDE YARDS:	5.0 FT	W 13.7 FT/E 2.1 FT
FRONT PORCH ENCROACHMENT:	4.0 FT	0.0 FT
BUILDING AREAS:		
UPPER FLOOR AREA:	– SF	1,114.9 SF
MAIN FLOOR AREA:	– SF	1,643.4 SF
BSMT FLOOR AREA:	– SF	684.3 SF
TOTAL FLOOR AREA:	4028.31 SF	3,442.6 SF
FLOOR SPACE RATIO:	0.7	0.43
ATTACHED ACCESSORY:		
SIDE ENTRANCE PORCH:	– SF	33.0 SF
BACK PORCH/DECK:	– SF	219.4 SF
FRONT DECK:	– SF	72 SF
TOTAL FLOOR AREA:	805.6 SF (10%)	324.43 SF (4%)

PROJECT DATA – 571 FOURTH ST INFILL HOUSE

EXT’G LEGAL DESCRIPTION:	LOT 12 OF LOT 4, SB 10, NWD, PLAN 2620	
CIVIC ADDRESS:	571 FOURTH STREET, NEW WESTMINSTER, B.C.	
CURRENT ZONING/ USE:	RS–4 RESIDENTIAL DISTRICT	
ENERGY USAGE:	STEP CODE 3	
	PERMITTED/REQUIRED	PROPOSED
SITE AREA:	6000 SF MIN.	66.0x122.07=8,056.62 SF
MAXIMUM HEIGHT:	23.0 FT	22.5 FT
SITE COVERAGE:	10% (805.6 SF)	8% (642 SF)
SETBACKS OF BUILDING:		
LANE:	3.0 FT	3.0 FT
BETWEEN HOUSES:	16.0 FT	17.0 FT
SIDE YARDS:	6.0 FT	W 11.9 FT/E 23.5 FT
BUILDING AREAS:		
UPPER FLOOR AREA:	– SF	440.0 SF
MAIN FLOOR AREA:	– SF	610.0 SF
BSMT FLOOR AREA:	– SF	370 SF
TOTAL FLOOR AREA:	958 SF	1,420.0 SF
FLOOR SPACE RATIO:	0.12	0.18 *
ATTACHED ACCESSORY:		
FRONT PORCH:	32.0 SF	32.0 SF
CARPORT:	226 SF	222.8 SF
BIKE STORAGE:	32.0 SF	32.0 SF

OVERALL SITE PROJECT DATA

SITE AREA:	66.0 x 122.07 = 8056.62 SF
PROPOSED SITE COVERAGE:	2285.4 SF (28.4%)
PROPOSED TOTAL FSR:	0.60 (4862.6 SF)
PROPOSED EXCLUDING BSMT FSR:	0.47 (3808.3 SF)
PROPOSED PARKING:	2 SPACES PROVIDED



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PROJECT TITLE.
**HERITAGE RESTORATION
AGREEMENT,
323 REGINA ST.,
NEW WESTMINSTER, B.C.**

DRAWING TITLE.

SITE DATA

Do not scale from drawings. All dimensions are to be verified on site prior to commencement of work. Any discrepancies are to be reported to the Designer immediately. Any proposed changes must be confirmed with the Designer in advance. All work must comply with the National Building Code of Canada, The Province of British Columbia Building Code and all relevant municipal by-laws. This drawing and its contents remain the copyright of Nancy G Dheilly.
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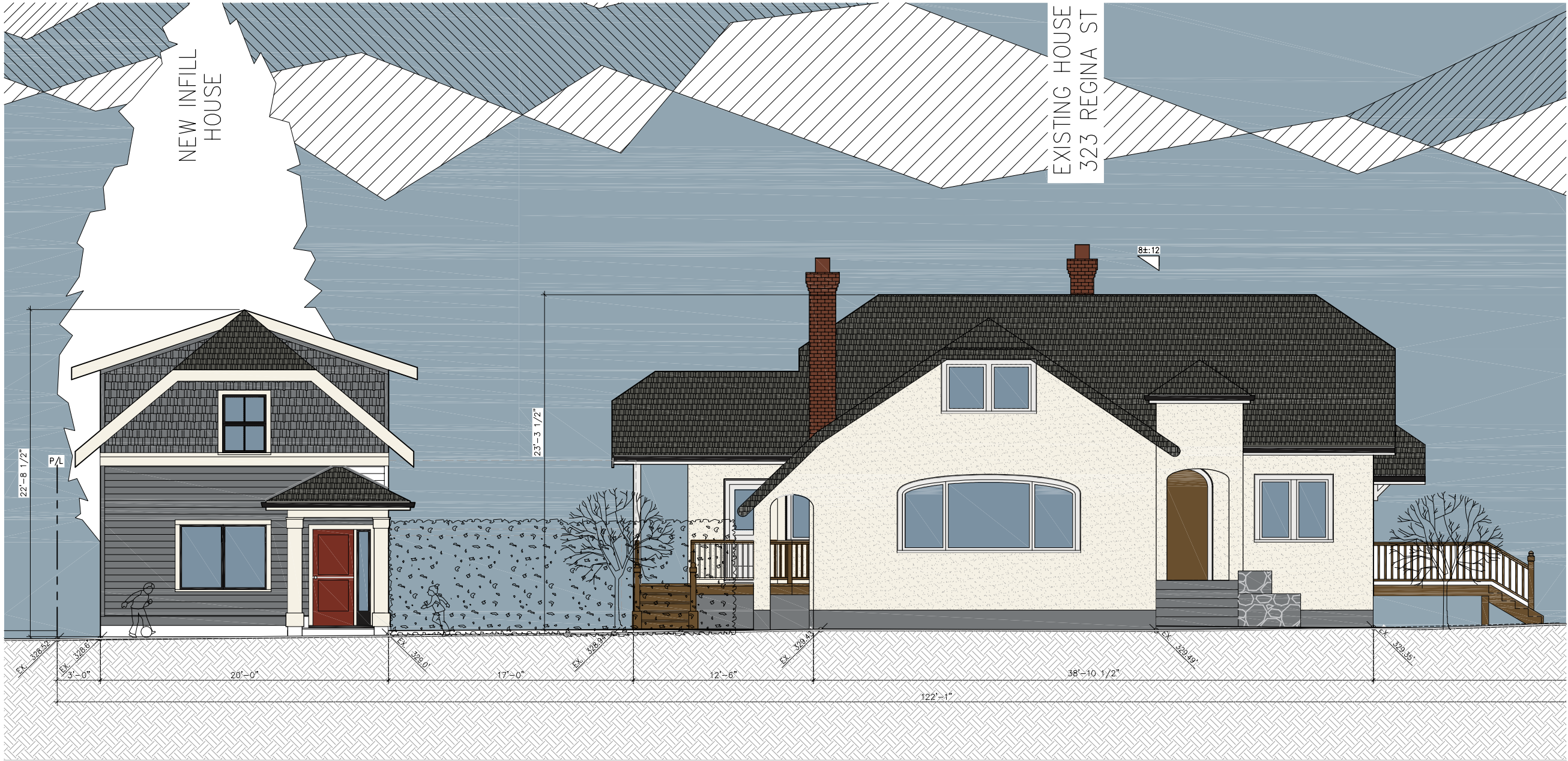
CAD FILE NAME.
19-254

DATE.	REVISED.
NOVEMBER 2020	AUG 6, 2021

SCALE.
NTS

DRAWING NO.

A-0.1



1 4TH STREET ELEVATION
— SCALE: 1/8" = 1'-0"



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PROJECT TITLE.
INFILL HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.
4TH STREET
ELEVATION

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19-254

DATE. REVISED.
NOVEMBER 2020 NOV 8, 2021

SCALE.
1/8"=1'0"

DRAWING NO.



MATERIALS KEY	
A	ASPHALT ROOF SHINGLES
B	IKO HARVARD SLATE
C	RESTORED WOOD FRAME WINDOWS GLOSS BLACK
D	RESTORED ROOF AND WINDOW TRIMS & PORCH DETAILS
E	AURA LOW LUSTRE 634-1 WHITE
F	RESTORED STUCCO VC-1 OXFORD IVORY
G	EXISTING GRANITE
H	GUTTER AND DOWNSPOUTS BLACK
I	NEW WOOD FRAME WINDOWS GLOSS BLACK
J	NEW ROOF AND WINDOW TRIMS & PORCH DETAILS
K	AURA LOW LUSTRE 634-1 WHITE
L	NATURAL CEDAR
M	NEW WHITE VINYL WINDOWS
N	SEE WINDOW SCHEDULE ON A-1.9



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PROJECT TITLE.
HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.
4TH ST (WEST)
ELEVATION

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SCALE.
3/16"=1'0"

DRAWING NO.

A-1.5

1 4TH ST (WEST) ELEVATION
— SCALE: 3/16" = 1'-0"



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PROJECT TITLE.

**HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.**

DRAWING TITLE.

**REGINA (SOUTH)
ELEVATION**

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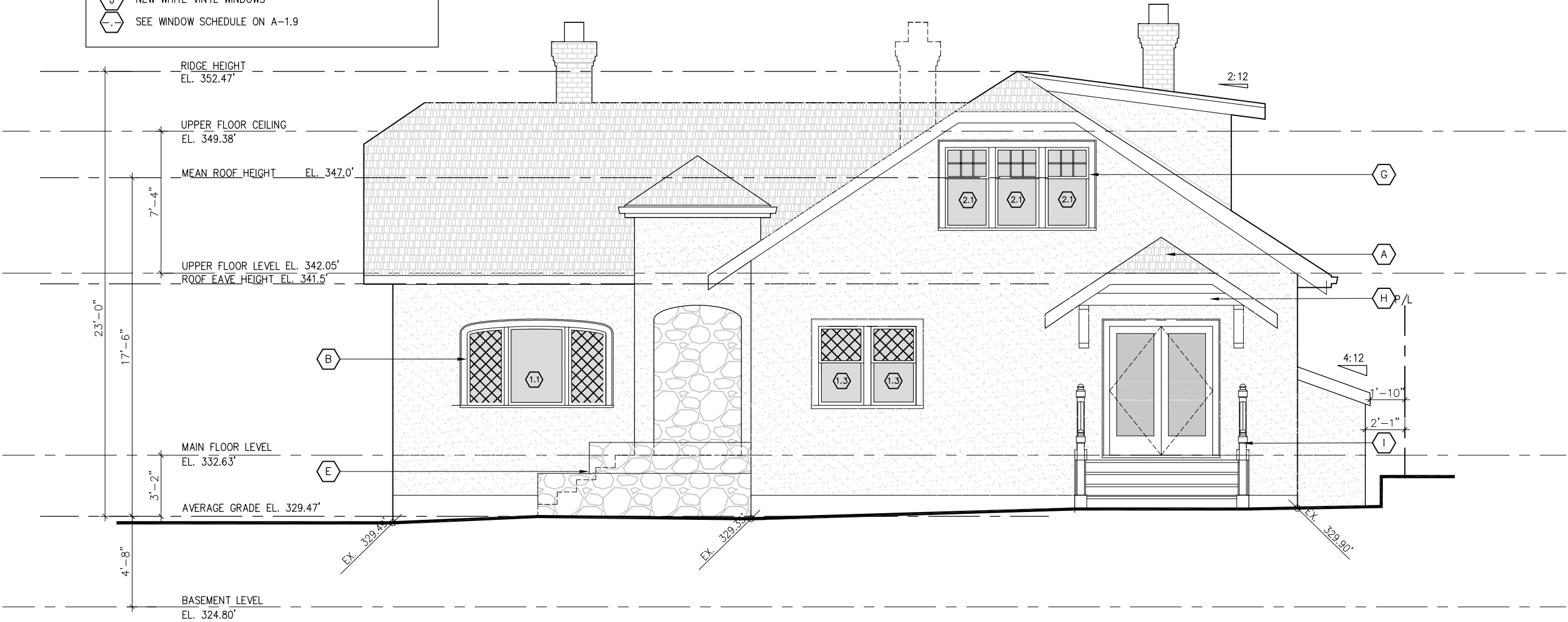
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DRAWING NO.

A-1.6

MATERIALS KEY

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IKO HARVARD SLATE
- B RESTORED WOOD FRAME WINDOWS GLOSS BLACK
- C RESTORED ROOF AND WINDOW TRIMS & PORCH DETAILS
AURA LOW LUSTRE 634-1 WHITE
- D RESTORED STUCCO VC-1 OXFORD IVORY
- E EXISTING GRANITE
- F GUTTER AND DOWNSPOUTS BLACK
- G NEW WOOD FRAME WINDOWS GLOSS BLACK
- H NEW ROOF AND WINDOW TRIMS & PORCH DETAILS
AURA LOW LUSTRE 634-1 WHITE
- I NATURAL CEDAR
- J NEW WHITE VINYL WINDOWS
- SEE WINDOW SCHEDULE ON A-1.9



1 REGINA (SOUTH) ELEVATION
— SCALE: 3/16" = 1'-0"



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PROJECT TITLE.
HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.
**NORTH
ELEVATION**

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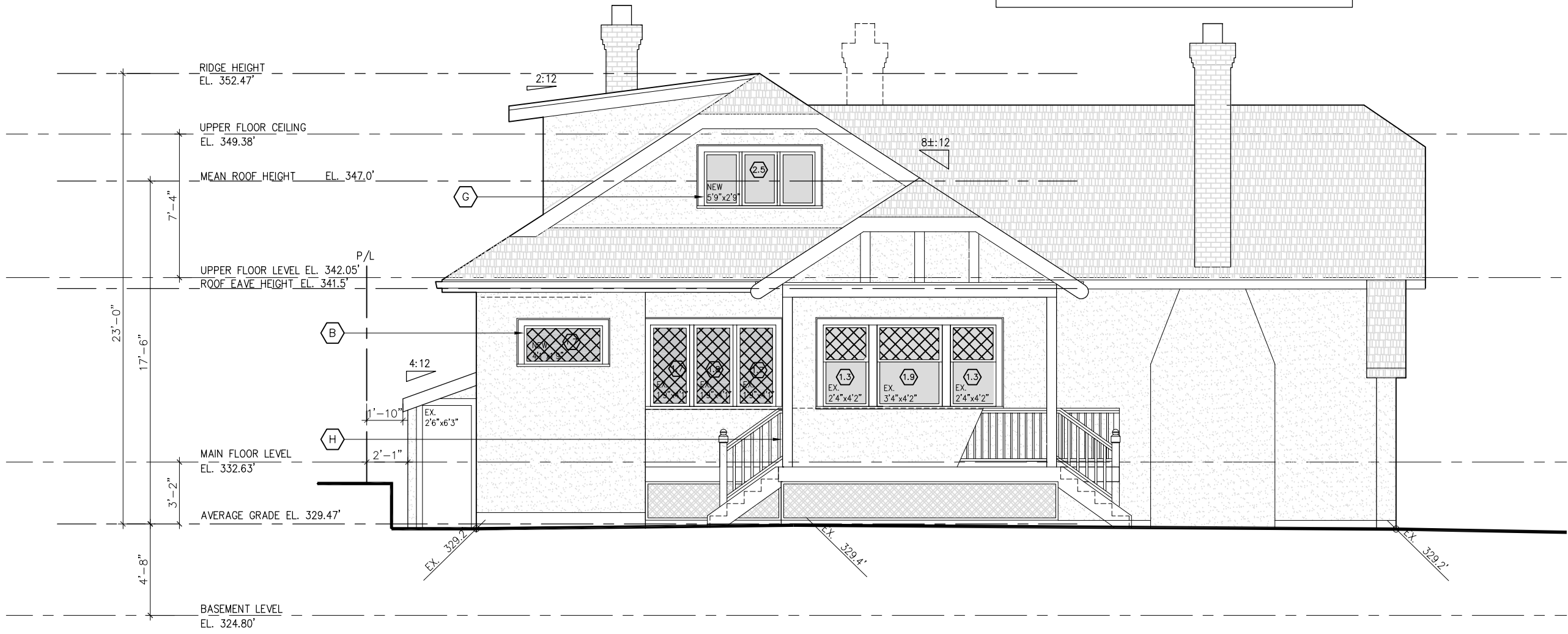
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3/16"=1'0"

DRAWING NO.

A-1.7

MATERIALS KEY

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IKO HARVARD SLATE
- B RESTORED WOOD FRAME WINDOWS GLOSS BLACK
- C RESTORED ROOF AND WINDOW TRIMS & PORCH DETAILS
AURA LOW LUSTRE 634-1 WHITE
- D RESTORED STUCCO VC-1 OXFORD IVORY
- E EXISTING GRANITE
- F GUTTER AND DOWNSPOUTS BLACK
- G NEW WOOD FRAME WINDOWS GLOSS BLACK
- H NEW ROOF AND WINDOW TRIMS & PORCH DETAILS
AURA LOW LUSTRE 634-1 WHITE
- I NATURAL CEDAR
- J NEW WHITE VINYL WINDOWS
- SEE WINDOW SCHEDULE ON A-1.9



1 NORTH ELEVATION
SCALE: 3/16" = 1'-0"



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PROJECT TITLE.
HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.
**EAST
ELEVATION**

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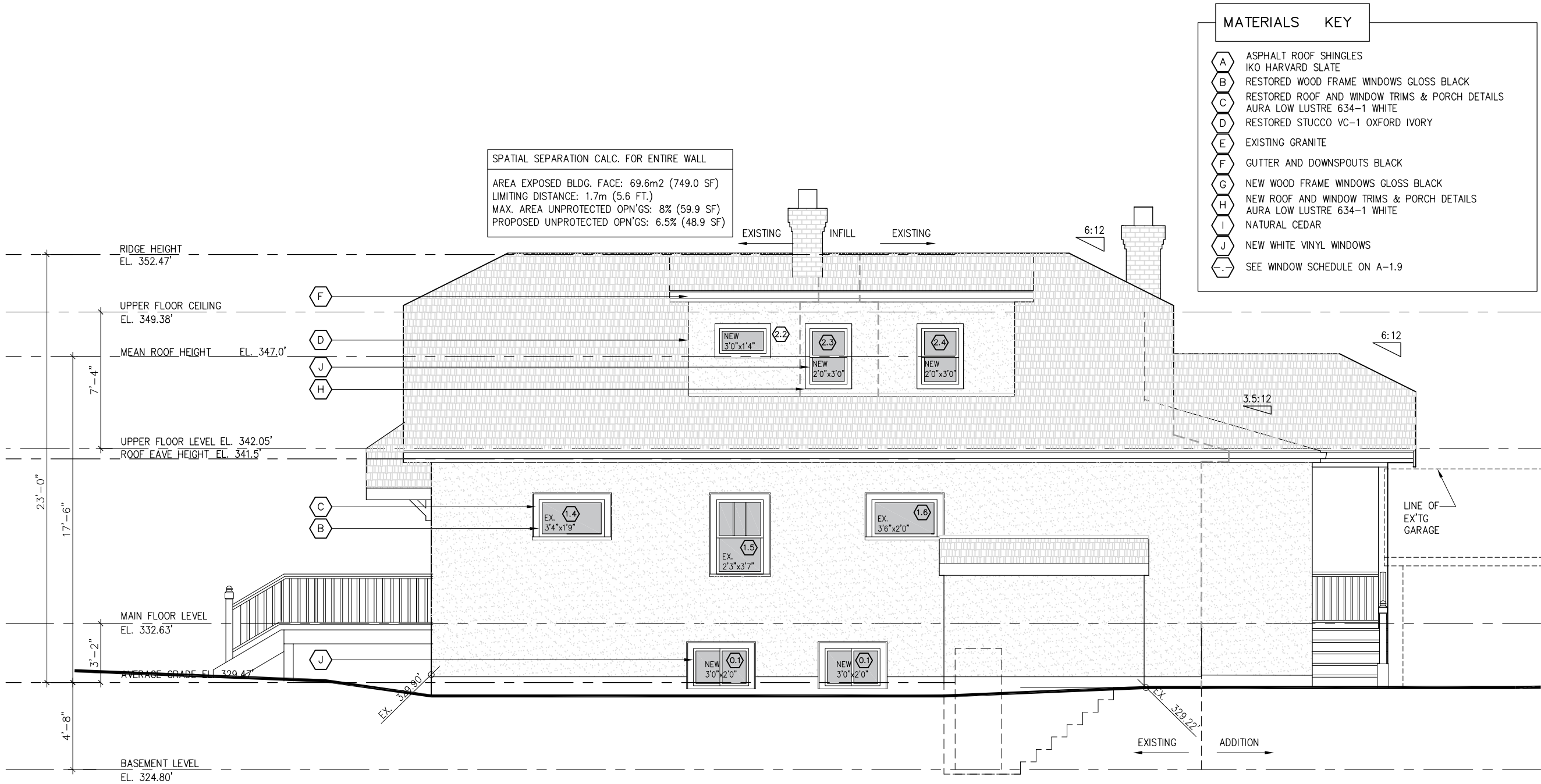
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SCALE.
3/16"=1'0"

DRAWING NO.

A-1.8



1 EAST ELEVATION
— SCALE: 3/16" = 1'-0"



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PROJECT TITLE.

HERITAGE HOUSE,
323 REGINA ST.,
NEW WESTMINSTER, B.C.

DRAWING TITLE.

WINDOW
SCHEDULE

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A-1.9

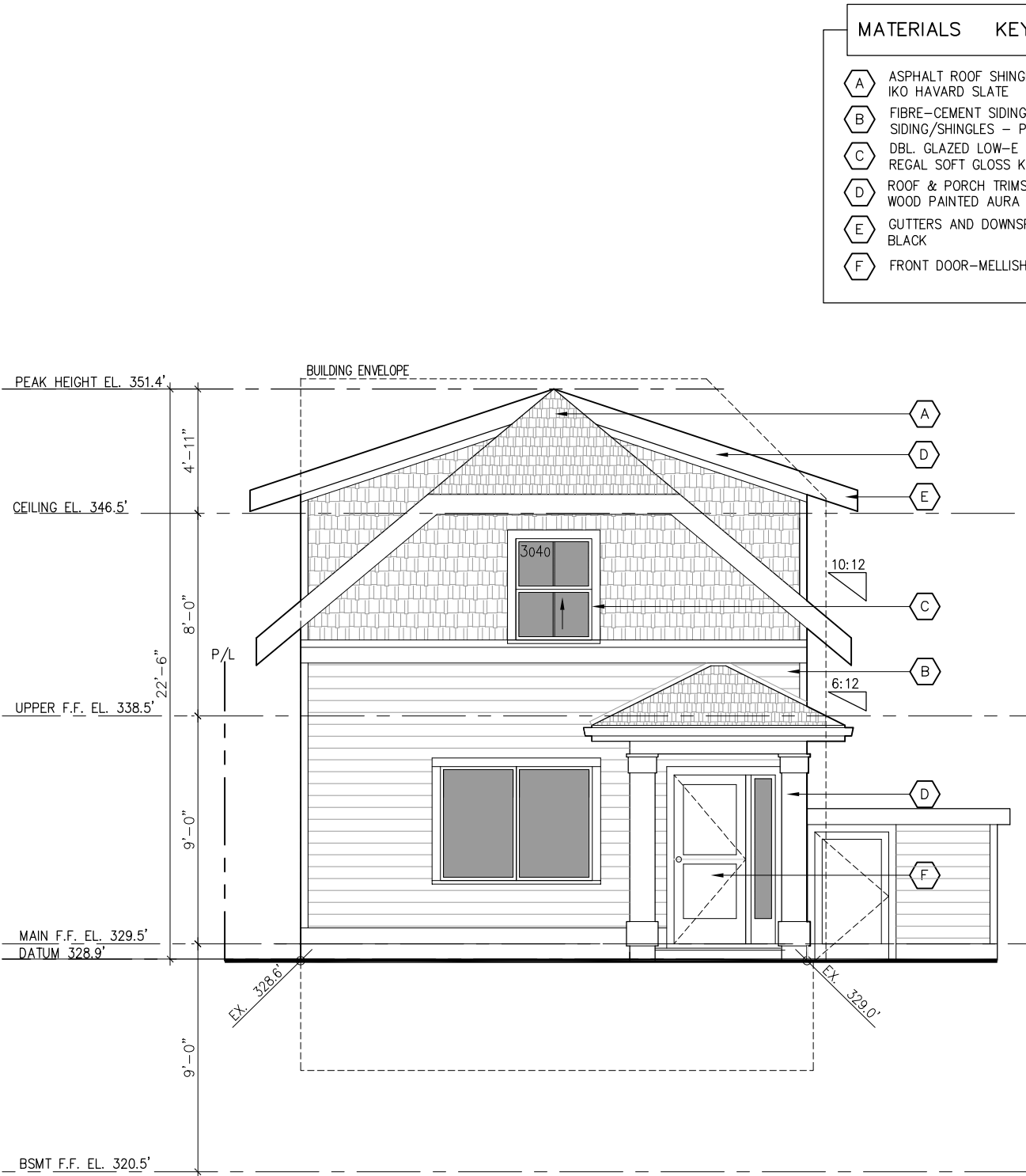
WINDOW SCHEDULE				
MARK	LOCATION	SIZE	DESCRIPTION	REMARKS
0.1	BASEMENT	3'-0"X2'-0"	SLIDING	NEW, VINYL
1.0	LIVING ROOM	10'-0"X4'-2"	FIXED	EX, WOODEN ARCHED WINDOW, W/LEADED GLASS SIDE PANELS AND CENTRAL PICTURE PANEL
1.1	LIVING ROOM	7'-3"X4'-2"	CASEMENT, FIXED, FIXED	EX, WOODEN ARCHED WINDOW, W/LEADED GLASS SIDE PANELS AND CENTRAL PICTURE PANEL
1.2	DEN	2'-4"X4'-2"	FIXED	EXISTING, WOODEN WITH LEADED GLASS
1.3	DEN, DINING ROOM	2'-4"X4'-2"	DOUBLE HUNG	EXISTING, WOODEN-LEADED GLASS UPPER PANEL (REUSE BDRM WINDOW FOR DEN)
1.4	BEDROOM	3'-4"X1'-9"	AWNING	EXISTING WOODEN WINDOW
1.5	BATHROOM	2'-3"X3'-7"	DOUBLE HUNG	EXISTING WOODEN, WITH OBSCURE GLASS IN LOWER PANEL, UPPER TRUE DIVIDED LITE.
1.6	KITCHEN	3'-6"X2'-0"	FIXED-STAINED GLASS	EXISTING WOODEN WINDOW WITH STAINED GLASS
1.7	MUDROOM, NOOK	1'-9"X4'-0"	FIXED-LEADED GLASS	EXISTING WOODEN WINDOW (REUSE NOOK WINDOW FOR MUDROOM)
1.8	NOOK	1'-9"X4'-0"	CASEMENT-LEADED GLASS	EXISTING WOODEN WINDOW
1.9	DINING ROOM	3'-4"X4'-2"	DOUBLE HUNG	EXISTING, WOODEN-LEADED GLASS UPPER PANEL
2.0	BEDROOM	2'-4"X3'-2"	DOUBLE HUNG	EXISTING, WOODEN-UPPER PANEL W/TRUE DIVIDED LITES
2.1	BEDROOM	2'-0"X4'-2"	DOUBLE HUNG	NEW, WOODEN-UPPER PANEL W/ TRUE DIVIDED LITES
2.2	BATH	3'-0"X1'-4"	AWNING	NEW, VINYL
2.3	BATH	2'-0"X3'-0"	SINGLE HUNG	NEW, VINYL, LOWER PANEL OBSCURE GLASS
2.4	LAUNDRY	2'-0"X3'-0"	SINGLE HUNG	NEW, VINYL
2.5	BEDROOM	5'-9"X2'-9"	CASEMENT, FIXED, CASEMENT	NEW, WOODEN

1

—

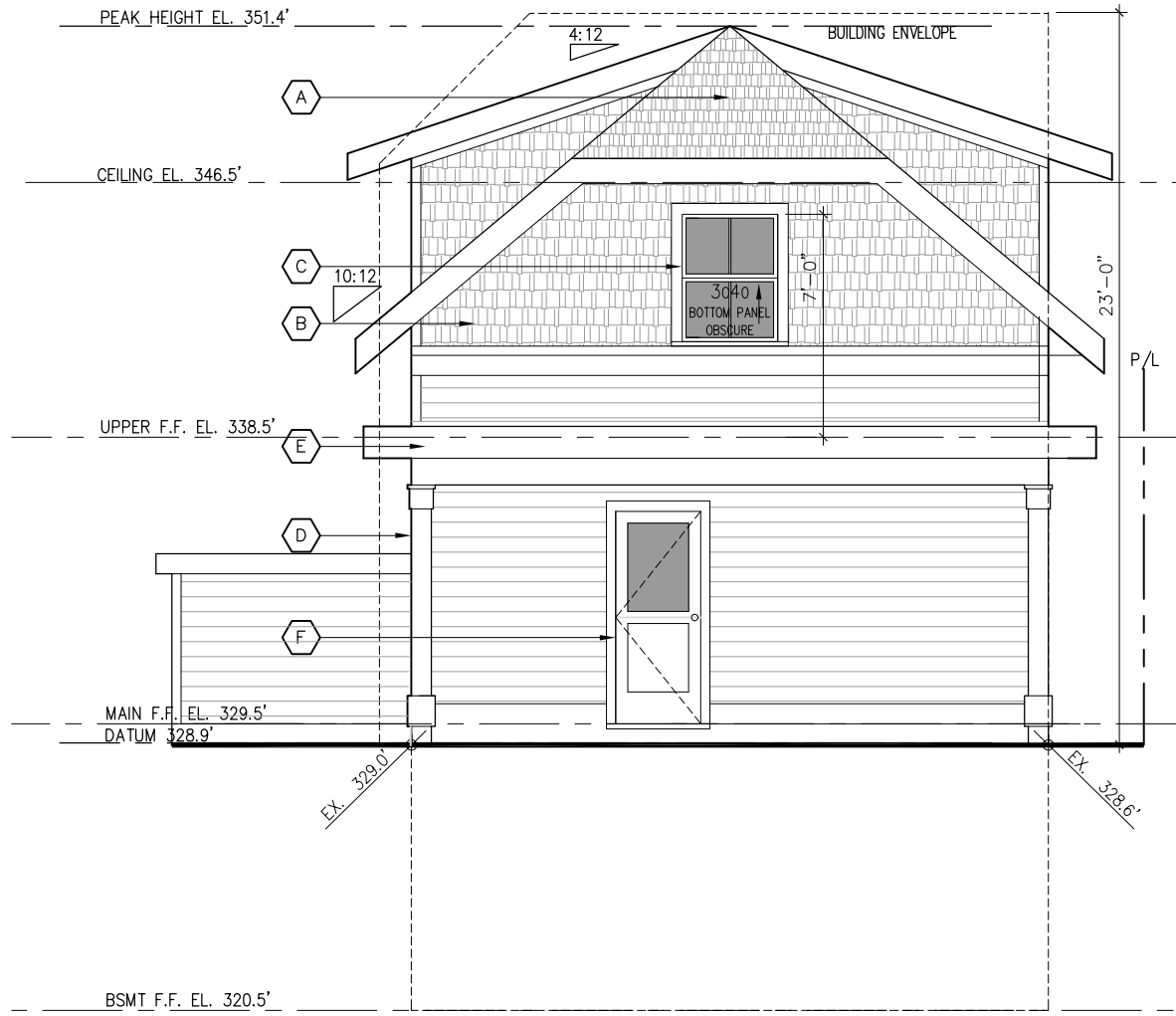
WINDOW SCHEDULE

SCALE: NTS



MATERIALS KEY

- A ASPHALT ROOF SHINGLES
IKO HAVARD SLATE
- B FIBRE-CEMENT SIDING W/5 1/2" EXPOSURE HARDI
SIDING/SHINGLES - POINT GREY (BM VC-24)
- C DBL. GLAZED LOW-E WOOD WINDOWS
REGAL SOFT GLOSS K404-80 BLACK
- D ROOF & PORCH TRIMS & GUARDRAILS
WOOD PAINTED AURA LOW LUSTRE 634-1 WHITE
- E GUTTERS AND DOWNSPOUTS
BLACK
- F FRONT DOOR-MELLISH RUST (BM VC-28)



1 WEST ELEVATION
SCALE: 3/16" = 1'-0"

2 EAST ELEVATION
SCALE: 3/16" = 1'-0"



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E-MAIL NDHEILLY@SHAW.CA

PROJECT TITLE.

INFILL HOUSE,
571 FOURTH ST,
NEW WESTMINSTER, B.C.

DRAWING TITLE.
**WEST AND EAST
ELEVATIONS**

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Note: For all structural information refer to structural drawings.

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NANCY G DHEILLY

CAD FILE NAME.
19-254

DATE. REVISED.
NOVEMBER 2020 NOV 8, 2021

SCALE.
3/16"=1'0"

DRAWING NO.

A-2.5



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PROJECT TITLE.
**INFILL HOUSE,
571 FOURTH ST,
NEW WESTMINSTER, B.C.**

DRAWING TITLE.
**NORTH
ELEVATION**

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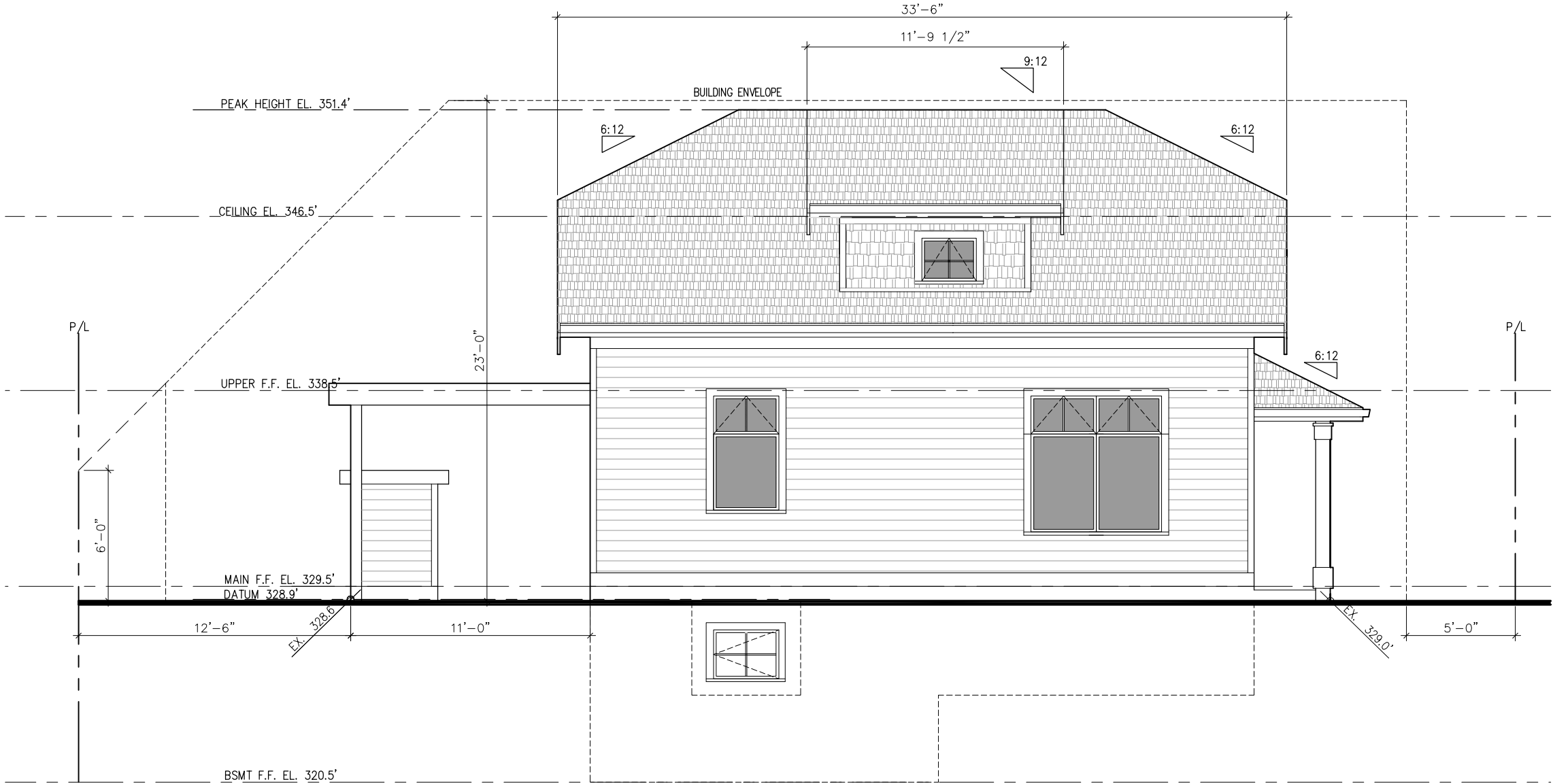
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19-254

DATE. REVISED.
NOVEMBER 2020 NOV 8, 2021

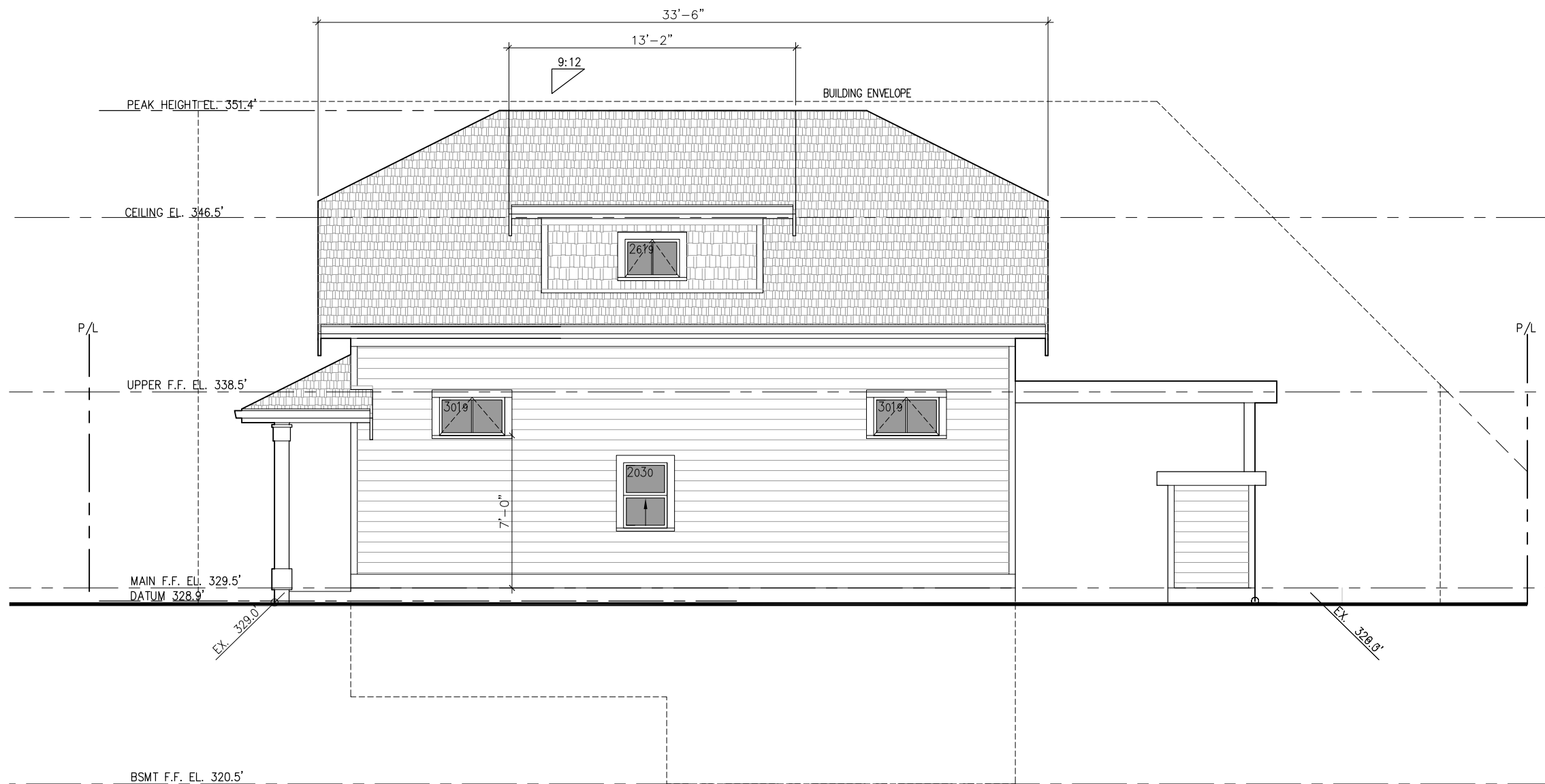
SCALE.
3/16"=1'0"

DRAWING NO.

A-2.6



1 NORTH ELEVATION
— SCALE: 3/16" = 1'-0"



1 SOUTH ELEVATION
— SCALE: 3/16" = 1'-0"



NANCY G DHEILLY
360 SHERBROOKE STREET
NEW WESTMINSTER, B.C., V3L 3M7
TEL. 604.526.2503
E-MAIL NDHEILLY@SHAW.CA

PROJECT TITLE.

INFILL HOUSE,
571 FOURTH ST,
NEW WESTMINSTER, B.C.

DRAWING TITLE.

SOUTH
ELEVATION

Do not scale from drawings. All dimensions are to be verified on site prior to commencement of work. Any discrepancies are to be reported to the Designer immediately. Any proposed changes must be confirmed with the Designer in advance. All work must comply with the National Building Code of Canada, The Province of British Columbia Building Code and all relevant municipal by-laws. This drawing and its contents remain the copyright of Nancy G Dheilly. Note: For all structural information refer to structural drawings.

DRAWN BY.
NANCY G DHEILLY

CAD FILE NAME.
19-254

DATE. REVISED.
NOVEMBER 2020 NOV 8, 2021

SCALE.
3/16"=1'0"

DRAWING NO.

A-2.7

APPENDIX 6

VARIATIONS TO ZONING BYLAW NO. 6680, 2001

	Single Detached Dwelling District (RS-4) Requirement/Allowance	Heritage Building (323 Regina Street)	Infill Building (471 Fourth Street)
Maximum Detached Accessory Dwelling Floor Space Ratio*	0.1	--	0.18
Minimum Left Side Setback (north)	1.5 metres (5 feet)	--	0.9 metres (3 feet)
Minimum Right Side Setback (east)	1.5 metres (5 feet)	0.6 metres (2.1 feet)	--

** Should Step Code 3, 4 or 5 of the Energy Step Code be met, the maximum space ratio can be increased as outlined in Section 310.11.1 of Zoning Bylaw No. 6680, 2001*

THE CORPORATION OF THE CITY OF NEW WESTMINSTER

BYLAW NO. 8305, 2022

A bylaw of the Corporation of the City of New Westminster to designate the principal building located at 323 Regina Street as protected heritage property.

WHEREAS the *Local Government Act*, RSBC 2015, c.1 provides Council with authority, by bylaw, to designate real property, in whole or in part, as protected heritage property, on terms and conditions it considers appropriate;

AND WHEREAS the registered owner of the land located at 323 Regina Street has entered into a Heritage Revitalization Agreement authorized by Bylaw No. 8304, 2022 (the "Heritage Revitalization Agreement"), which has requested that Council designate the principal building on the land as protected heritage property, and has released the City from any obligation to compensate the registered owner for the effect of such designation;

AND WHEREAS Council considers that the principal building located at 323 Regina Street has significant heritage value and character and is a prominent and valued heritage property in the City;

AND WHEREAS Council considers that designation of the principal building located at 323 Regina Street as protected heritage property under the provisions of the *Local Government Act* is necessary and desirable for its conservation;

NOW THEREFORE City Council of the Corporation of the City of New Westminster enacts as follows:

TITLE

1. This Bylaw may be cited for all purposes as "Heritage Designation Bylaw (323 Regina Street) No. 8305, 2022."

INTERPRETATION

2. In this Bylaw, the terms "heritage value", "heritage character" and "alter" have the corresponding meanings given to them in the *Local Government Act*.

DESIGNATION

3. The principal building located on that parcel of land having a civic address of 323 Regina Street, New Westminster, British Columbia, legally described as PID: 013-593-285; LOT 12 OF LOT 4 SUBURBAN BLOCK 10 PLAN 2620 and labelled "Heritage House" in Schedule A (the "Building"), is hereby designated in its entirety as protected heritage property under section 611 of the *Local Government Act* of British Columbia.

PROHIBITION

4. Except as expressly permitted by Section 5 or as authorized by a heritage alteration permit issued by the City, no person shall undertake any of the following actions, nor cause or permit any of the following actions to be undertaken in relation to the Building:
 - (a) alter the exterior of the Building;
 - (b) make a structural change to the Building including, without limitation, demolition of the Building or any structural change resulting in demolition of the Building;
 - (c) move the Building; or
 - (d) alter, excavate or build on that portion of land upon which the Building is located.

EXEMPTIONS

5. Despite Section 4, the following actions may be undertaken in relation to the Building without first obtaining a heritage alteration permit from the City:
 - (a) non-structural renovations or alterations to the interior of the Building that do not alter the exterior appearance of the Building; and
 - (b) normal repairs and maintenance that do not alter the exterior appearance of the Building.
6. For the purpose of section 5, “normal repairs” means the repair or replacement of non-structural elements, components or finishing materials of the Building with elements, components or finishing materials that are equivalent to those being replaced in terms of heritage character, material composition, colour, dimensions and quality.

MAINTENANCE

7. The Building shall be maintained in good repair in accordance with the City of New Westminster Heritage Property Maintenance Standards Bylaw No. 7971, 2018, as amended or replaced from time to time.

HERITAGE ALTERATION PERMITS

8. Where a heritage alteration permit is required under this Bylaw for a proposed action in relation to the Building, application shall be made to the City of New Westminster Development Services Department, Planning Division in the manner and on the form prescribed, and the applicant shall pay the fee imposed by the City for such permit, if any.

9. City Council, or its authorized delegate, is hereby authorized to:

- (a) issue a heritage alteration permit for situations in which the proposed action would be consistent with the heritage protection provided for the Building under this Bylaw and the Heritage Revitalization Agreement;
- (b) withhold the issue of a heritage alteration permit for an action which would not be consistent with the heritage protection provided for the Building under this Bylaw or the Heritage Revitalization Agreement;
- (c) establish and impose terms, requirements and conditions on the issue of a heritage alteration permit that are considered to be consistent with the purpose of the heritage protection of the Building provided under this Bylaw and the Heritage Revitalization Agreement; and
- (d) determine whether the terms, requirements and conditions of a heritage alteration permit have been met.

RECONSIDERATION BY COUNCIL

10. An applicant or owner whose application for a heritage alteration permit for alteration of the Building has been considered by an authorized delegate may apply for a reconsideration of the matter by Council, and such reconsideration shall be without charge to the applicant or owner.

GIVEN FIRST READING this _____ day of _____ 2022.

GIVEN SECOND READING this _____ day of _____ 2022.

PUBLIC HEARING held this _____ day of _____ 2022.

GIVEN THIRD READING this _____ day of _____ 2022.

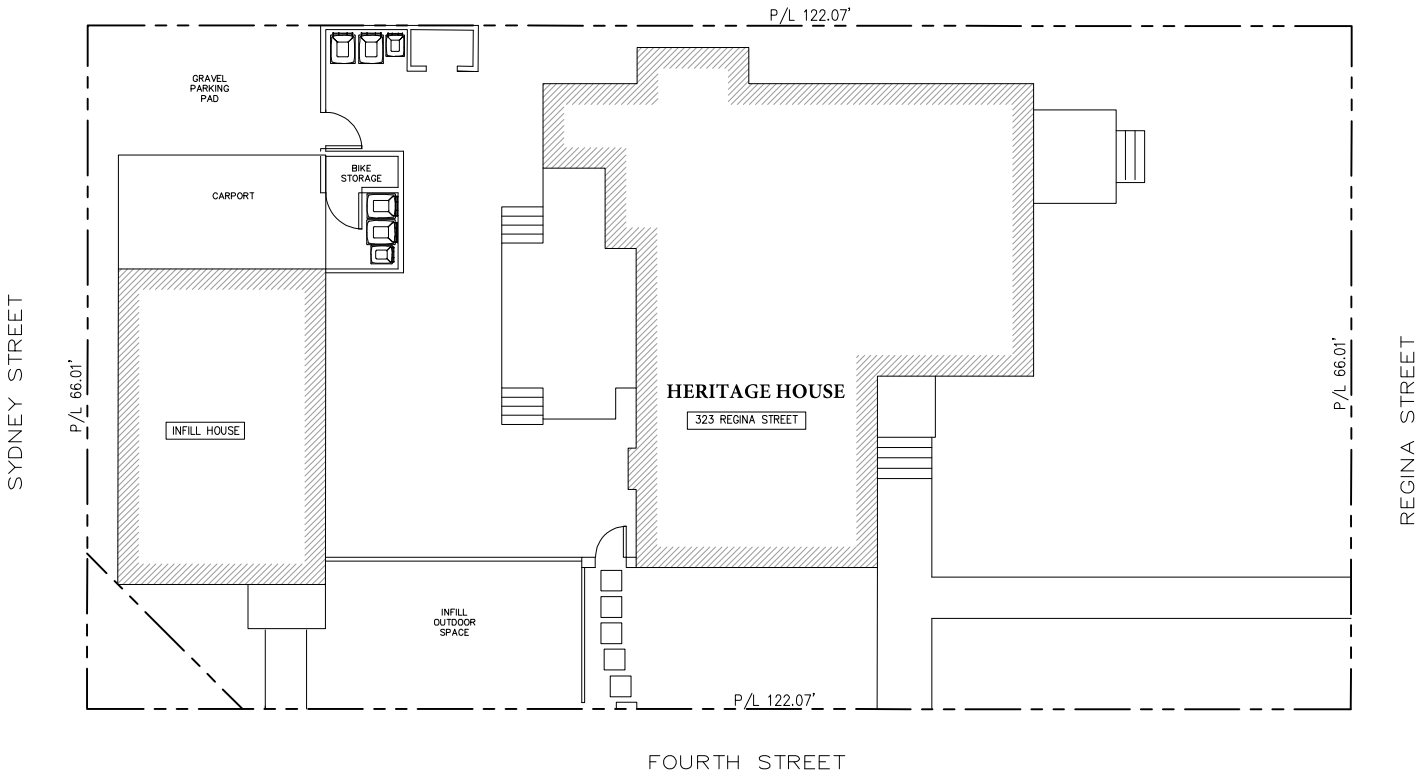
ADOPTED and the Seal of the Corporation of the City of New Westminster affixed this
_____ day of _____ 2022.

MAYOR JONATHAN X. COTE

JACQUE KILLAWEE, CITY CLERK

SCHEDULE A

SKETCH



1 SITE PLAN
— SCALE: 3/32"=1'-0"



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PROJECT TITLE.
**HERITAGE RESTORATION
AGREEMENT,
323 REGINA ST.,
NEW WESTMINSTER, B.C.**

DRAWING TITLE.
**SIMPLE SITE
PLAN**

Do not scale from drawings. All dimensions are to be verified on site prior to commencement of work. Any discrepancies are to be reported to the Designer immediately. Any proposed changes must be confirmed with the Designer in advance. All work must comply with the National Building Code of Canada, The Province of British Columbia Building Code and all relevant municipal by-laws. This drawing and its contents remain the copyright of Nancy G Dheilly.
Note: For all structural information refer to structural drawings.

DRAWN BY.
NANCY G DHEILLY

CAD FILE NAME.
19-254

DATE. REVISED.
DECEMBER 2021 DEC 7, 2021

SCALE.
3/32"=1'-0"

DRAWING NO.

A-0.3

CORPORATION OF THE CITY OF NEW WESTMINSTER

BYLAW NO. 8308, 2022

A Bylaw of the City of New Westminster representing the Five-Year
Financial Plan for the years 2022 – 2026, inclusive

WHEREAS pursuant to Section 165 of the Community Charter, Council must establish a Five-Year Financial Plan for the period 2022 – 2026 inclusive;

NOW THEREFORE, the Council of the Corporation of the City of New Westminster ENACTS
AS FOLLOWS:

(1) This bylaw may be cited for all purposes as the “Five-Year Financial Plan (2022 – 2026),
Bylaw No. 8308, 2022”.

(2) Council does hereby adopt the Five-Year Financial Plan, for the years 2022 - 2026
inclusive, for each year of the plan, as set out in Schedule A.

(3) Schedules B and C provide supplementary information to the bylaw.

GIVEN FIRST READING this day of , 2022.

GIVEN SECOND READING this day of , 2022.

GIVEN THIRD READING this day of , 2022.

ADOPTED this day of , 2022.

Mayor Jonathan X. Cote

Jacque Killawee, City Clerk

CORPORATION OF THE CITY OF NEW WESTMINSTER

CONSOLIDATED FINANCIAL PLAN

Schedule 'A' to Bylaw No. 8308, 2022

	2022	Budget Projections			
	Budget	2023	2024	2025	2026
REVENUE					
Municipal Taxation (see below)	\$ 97,877,861	\$ 103,095,703	\$ 109,463,995	\$ 115,272,273	\$ 120,451,743
Utility Rates	100,119,172	105,126,857	110,290,843	115,769,312	121,585,481
Sale of Services	14,373,935	15,771,060	15,871,060	15,571,060	15,571,060
Grants from Other Governments (1)	8,920,339	5,893,400	2,871,600	2,871,600	2,871,600
Contributions (2)	24,086,630	24,174,930	18,839,530	17,170,930	10,983,930
Other Revenue	16,915,915	18,081,073	18,221,457	18,082,689	17,797,265
Total Revenues	262,293,852	272,143,023	275,558,485	284,737,864	289,261,079
EXPENSES					
General Services					
Police Services	34,408,503	34,729,375	35,717,010	36,385,994	36,975,831
Parks and Recreation	19,416,533	22,296,796	26,355,706	26,703,057	26,982,221
Fire and Rescue	17,868,972	18,554,183	19,290,982	19,670,640	20,219,813
Development Services	7,358,604	7,014,666	7,155,875	7,313,040	7,449,313
Engineering	29,776,519	30,592,468	31,214,919	31,519,297	32,086,538
General Government	29,850,878	30,366,173	30,452,417	30,563,408	31,384,291
Library	4,905,686	4,996,028	5,068,783	5,119,432	5,180,001
	143,585,695	148,549,689	155,255,692	157,274,868	160,278,008
Utilities Services					
Electrical Utility	40,883,766	41,706,514	42,581,978	45,037,184	45,867,089
Water Utility	9,358,198	9,967,152	10,999,891	12,093,204	13,346,871
Sewer Utility	15,330,615	17,646,962	20,145,656	23,121,261	28,848,140
Solid Waste Utility	3,994,991	4,112,023	4,210,458	4,295,997	4,424,510
	69,567,570	73,432,651	77,937,983	84,547,646	92,486,610
Fiscal Expenses					
Interest and Bank Charges	2,849,958	5,177,088	6,157,719	6,222,996	6,445,961
Total Expenses	216,003,223	227,159,428	239,351,394	248,045,510	259,210,579
INCREASE IN TOTAL EQUITY	46,290,629	44,983,595	36,207,091	36,692,354	30,050,500
Reconciliation to Financial Equity					
Amortization of Tangible Capital Assets	26,286,000	27,438,000	29,959,000	31,329,000	31,803,000
Capital Expenses (Schedule B)	(170,699,310)	(109,731,100)	(66,230,250)	(58,198,700)	(48,236,400)
Debt Retirement	(5,151,403)	(8,214,754)	(9,686,716)	(9,657,090)	(10,214,494)
Proceeds on Debt Issuance	72,184,585	30,772,800	8,000,000	7,500,000	500,000
CHANGE IN FINANCIAL EQUITY (Reserves)	(31,089,499)	(14,751,459)	(1,750,875)	7,665,564	3,902,606
Financial Equity, beginning of year	154,633,733	123,544,234	108,792,775	107,041,900	114,707,464
FINANCIAL EQUITY (Reserves), end of year	\$ 123,544,234	\$ 108,792,775	\$ 107,041,900	\$ 114,707,464	\$ 118,610,070

Notes:

(1) Includes capital grants noted on Schedule B.

(2) Includes capital contributions and DCCs noted on Schedule B.

Municipal Taxation					
Property Taxes	\$ 96,205,561	\$ 101,389,353	\$ 107,721,045	\$ 113,491,623	\$ 118,632,293
Parcel Taxes	19,200	17,750	17,750	17,750	17,750
Grant-in-Lieu of Taxes	1,184,200	1,219,700	1,256,300	1,294,000	1,332,800
Utilities 1%-in-Lieu of Taxes	468,900	468,900	468,900	468,900	468,900
	\$ 97,877,861	\$ 103,095,703	\$ 109,463,995	\$ 115,272,273	\$ 120,451,743

CORPORATION OF THE CITY OF NEW WESTMINSTER

CONSOLIDATED FINANCIAL PLAN

Schedule 'A' to Bylaw No. 8308, 2022

(continued)

Proportion of Revenues By Funding Source:

The following table shows the proportion of total revenue proposed to be raised from each funding source. Property taxes form the second largest portion of revenues. They provide a stable and consistent source of revenues to pay for many services, such as police and fire protection, that are difficult or undesirable to fund on a user-pay basis.

Utilities' rates are the City's largest component of planned revenues. These revenues pay for services including electricity, water, sewer and solid waste and are charged on a user-pay basis. This basis attempts to fairly apportion utility service costs to those that make use of these services.

Other revenue sources, including sale of services, government grants and contributions make up the remainder of total revenues. These revenues fluctuate due to economic conditions and City initiatives.

Revenue Source	% Total Revenue
Taxation	37%
Utility Rates	38%
Sale of Services	5%
Gov't Grants	3%
Contributions	9%
Other Revenue	6%
	100%

Distribution of Property Taxes Between Property Classes:

The following table provides the distribution of property tax revenue between property classes. The City's primary goal is to set tax rates that are sufficient, after maximizing non-tax revenues, to provide for service delivery; City assets; and maintain tax stability. This is accomplished by maintaining the historical relationship between the property classes and applying the same annual tax rate increase across all Classes. A secondary goal is to set tax rates that are competitive within the region; consequently, the City may, from time to time, adjust the property tax distribution between the Classes as deemed necessary.

Class No	Property Class	% Tax Burden
1	Residential	63%
2	Utilities	<1%
4	Major Industry	2%
5	Light Industry	3%
6	Business	31%
8	Recreation/Non-Profit	<1%
9	Farm	<1%
		100%

Use of Permissive Exemptions:

The City's Annual Municipal Report contains a list of permissive exemptions granted for the year and the amount of tax revenue foregone. Permissive tax exemption is granted to not-for-profit institutions including religious institutions, some recreational facilities, service organizations and cultural institutions that form a valuable part of our community.

Since the mid-90's the City has generally ceased granting new permissive exemptions from property taxes in order to preserve the tax revenue base. Organizations granted exemption prior to implementation of this practice continue to be considered for exemption provided they make an annual submission showing the use of the property subject to exemption has not been altered. All other applications for permissive exemption from property taxes are reviewed on a case-by-case basis.

CORPORATION OF THE CITY OF NEW WESTMINSTER

CONSOLIDATED CAPITAL PROGRAM

Schedule 'B' to Bylaw No. 8308, 2022

Note: This Schedule has been provided as an addendum to Schedule A. The figures in this Schedule are included in the consolidated figures in Schedule A.

	2022 Budget	Budget Projections			
		2023	2024	2025	2026
CAPITAL EXPENSES					
Land	\$ 500,000	\$ 4,733,800	\$ -	\$ -	\$ 2,000,000
Buildings	62,291,200	30,451,100	8,908,200	8,388,000	5,070,000
Vehicles/Equipment	7,503,500	5,530,800	6,250,650	4,883,500	4,753,500
Other Projects	8,519,295	5,164,800	3,601,500	3,709,300	3,135,000
Park Improvements	4,374,100	5,061,200	4,744,000	1,651,000	2,084,000
Engineering Structures	23,561,600	14,140,000	10,400,000	9,250,000	15,225,000
Water Infrastructure	6,011,500	6,838,900	5,338,900	5,838,900	5,838,900
Sewer Infrastructure	15,028,730	10,752,500	7,987,000	7,780,000	7,480,000
Electrical Distribution System	42,909,385	27,058,000	19,000,000	16,698,000	2,650,000
TOTAL	\$ 170,699,310	\$ 109,731,100	\$ 66,230,250	\$ 58,198,700	\$ 48,236,400
FUNDING SOURCES					
Reserve Funds	\$ 76,163,925	\$ 59,102,500	\$ 46,331,650	\$ 40,468,700	\$ 43,693,400
Development Cost Charges	3,734,150	2,660,000	1,321,600	375,000	263,000
Long Term Debt	72,184,585	30,772,800	8,000,000	7,500,000	500,000
Grants from Other Governments	5,660,800	3,021,800	-	-	-
Contributions	12,955,850	14,174,000	10,577,000	9,855,000	3,780,000
TOTAL	\$ 170,699,310	\$ 109,731,100	\$ 66,230,250	\$ 58,198,700	\$ 48,236,400

City of New Westminster - Development Cost Charge Funding Envelope Plan for the 2009 DCC Bylaw 7311

NOTES:

1. This DCC Funding Envelope Plan is based on the capital projects set out in the 2009 Development Cost Charge Review which forms the basis for the City's DCC Bylaw. The City's DCC Bylaw was amended in 2015 to reflect new rates based on an updated capital project plan.

2. City contributions will be from reserves while other contributions are from provincial / federal government grants.

3. The mainland waterfront parkland acquisition / development (\$16M) was initially funded with debt with the intention that the principal on the debt would be repaid over time using Parks DCCs.

	<u>Values</u>	<u>TTL</u>	<u>2009 - 2021</u>	<u>2022</u>	<u>2023</u>	<u>2024</u>	<u>2025</u>	<u>2026</u>	<u>2027 +</u>
Total DCCs	87,284,408	\$ 87,284,408	\$ 38,993,527	\$ 3,734,150	\$ 2,660,000	\$ 1,321,600	\$ 375,000	\$ 263,000	\$ 39,937,131
Total City & Other Contributions	34,985,065	34,985,065	11,932,581	2,349,257	3,406,586	2,395,886	1,339,286	939,286	12,622,183
	122,269,473	\$ 122,269,473	\$ 50,926,108	\$ 6,083,407	\$ 6,066,586	\$ 3,717,486	\$ 1,714,286	\$ 1,202,286	\$ 52,559,314

CORPORATION OF THE CITY OF NEW WESTMINSTER
CONSOLIDATED STATEMENT OF RESERVES AND DEVELOPMENT COST CHARGES
Schedule 'C' to Bylaw No. 8308, 2022

Note: This Schedule has been provided as an addendum to Schedule A. The reserve figures in this Schedule are included in the consolidated figures in Schedule A. Development Cost Charges are provided for information, but are deferred charges rather than reserves.

FINANCIAL EQUITY (RESERVES)	2022 Budget	Budget Projections			
		2023	2024	2025	2026
Revenues:					
Contributions	\$ 6,817,903	\$ 7,135,376	\$ 6,750,365	\$ 6,765,777	\$ 6,318,907
	6,817,903	7,135,376	6,750,365	6,765,777	6,318,907
Transfers (to) from:					
Operating Budget	38,256,523	37,215,665	37,830,410	41,368,487	41,277,099
Capital Budget	(76,163,925)	(59,102,500)	(46,331,650)	(40,468,700)	(43,693,400)
	(37,907,402)	(21,886,835)	(8,501,240)	899,787	(2,416,301)
Change in Financial Equity (Reserves)	(31,089,499)	(14,751,459)	(1,750,875)	7,665,564	3,902,606
Financial Equity, Beginning of Year	154,633,733	123,544,234	108,792,775	107,041,900	114,707,464
Financial Equity, End of Year	123,544,234	108,792,775	107,041,900	114,707,464	118,610,070
CHANGE IN RESERVES					
Non-Statutory Reserves	\$ (29,574,974)	\$ (10,346,144)	\$ (1,912,754)	\$ 7,500,447	\$ 5,734,188
Statutory Reserves					
Cemetery	52,025	53,066	54,127	55,210	56,314
Construction of Municipal Works	(1,628,826)	(4,521,902)	42,960	43,819	(1,955,305)
Parking Cash In Lieu	24,803	25,299	25,805	26,321	26,847
Park Land Acquisition	1,293	1,319	1,346	1,373	1,400
Tax Sale Land	36,180	36,903	37,641	38,394	39,162
Change in Reserves	\$ (31,089,499)	\$ (14,751,459)	\$ (1,750,875)	\$ 7,665,564	\$ 3,902,606
Statutory DCC Reserves					
Drainage DCC	\$ 61,370	\$ 123,998	\$ 126,478	\$ 129,007	\$ 131,588
Parkland DCC	1,143,242	(247,292)	788,161	1,383,325	1,392,291
Sewer DCC	(540,329)	378,414	386,483	449,212	458,196
Transportation DCC	(1,233,594)	(8,365)	271,468	570,897	694,315
Water DCC	(48,425)	180,406	184,015	187,695	191,448
Change in DCCs	\$ (617,736)	\$ 427,161	\$ 1,756,605	\$ 2,720,136	\$ 2,867,838
RESERVE BALANCES					
Non-Statutory Reserves	\$ 112,812,498	\$ 102,466,354	\$ 100,553,600	\$ 108,054,047	\$ 113,788,235
Statutory Reserves					
Cemetery	885,796	938,862	992,989	1,048,199	1,104,513
Construction of Municipal Works	6,669,881	2,147,979	2,190,939	2,234,758	279,453
Parking Cash In Lieu	1,264,932	1,290,231	1,316,036	1,342,357	1,369,204
Park Land Acquisition	65,961	67,280	68,626	69,999	71,399
Tax Sale Land	1,845,166	1,882,069	1,919,710	1,958,104	1,997,266
Total Reserves	\$ 123,544,234	\$ 108,792,775	\$ 107,041,900	\$ 114,707,464	\$ 118,610,070
Statutory DCC Reserves					
Drainage DCC	\$ 449,878	\$ 573,876	\$ 700,354	\$ 829,361	\$ 960,949
Parkland DCC	14,360,371	14,113,079	14,901,240	16,284,565	17,676,856
Sewer DCC	1,695,705	2,074,119	2,460,602	2,909,814	3,368,010
Transportation DCC	(468,272)	(476,637)	(205,169)	365,728	1,060,043
Water DCC	2,020,303	2,200,709	2,384,724	2,572,419	2,763,867
Total DCC Reserves	\$ 18,057,985	\$ 18,485,146	\$ 20,241,751	\$ 22,961,887	\$ 25,829,725

CORPORATION OF THE CITY OF NEW WESTMINSTER

BYLAW NO. 8307, 2021

A Bylaw to authorize the expenditure of moneys from the
Development Cost Charge Reserve Funds
for 2021 debt retirement related to DCC capital expenditures and for
2021 capital expenditures related to drainage infrastructure, water
infrastructure, and transportation infrastructure DCC capital projects

WHEREAS the Council has established development cost charge reserve funds for Queensborough drainage, water, transportation, and parkland development and Mainland transportation and parkland development and;

WHEREAS the expenditure of funds from the reserve funds for the projects identified in this bylaw are anticipated in the City's current financial plan;

THE COUNCIL of the Corporation of the City of New Westminster, in open meeting assembled, ENACTS AS FOLLOWS:

1. This bylaw may be cited for all purposes as the "DEVELOPMENT COST CHARGE RESERVE FUNDS EXPENDITURE BYLAW NO. 8307, 2021".
2. The Council ratifies, confirms and authorizes the expenditures up to the amount included in Schedule A from the Development Cost Charge Reserve Funds set out therein.

GIVEN THREE READINGS this 13th day of December , 2021.

ADOPTED and the Seal of the Corporation of the City of New Westminster affixed this day of , 2021.

Mayor Jonathan X. Cote

Jacque Killawee, City Clerk

Schedule A to Bylaw No. 8307, 2021

BYLAW # 8307, 2021			
Project Description	Estimated Project Cost	City / Other Funded Cost	Expenditure from DCC
<u>Queensborough Drainage DCC Projects</u>			
Boundary Street Pump Station (QD10)	2,000,000	1,505,000	495,000
QB. Drainage DCC Balance at Dec 31, 2020	413,212		
Actual 2021 QB. Drainage DCC Contributions to-date	82,281		
2021 QB. Drainage DCC Projects	(495,000)		
Est. QB. Drainage DCC Balance After Projects	493		
<u>Queensborough Water DCC Projects</u>			
Blackley Street DCC Watermain (QW1)	185,000	3,700	181,300
Duncan St. Watermain (QW2)	79,900	1,600	78,300
QB. Water DCC Balance at Dec 31, 2020	842,823		
Est. 2021 QB. Water DCC Contributions	-		
2021 QB. Water DCC Projects	(259,600)		
Est. QB. Water DCC Balance After Projects	583,223		
<u>Queensborough Transportation DCC Projects</u>			
Queensborough Transportation Howes Street (QT3)	50,100	500	49,600
Boyd/Duncan Intersection & Signal (QT9)	420,000	4,200	415,800
QB. Transportation DCC Balance at Dec 31, 2020	(2,208,667)		
Actual 2021 QB. Transportation DCC Contributions to-date	399,488		
2021 QB. Transportation DCC Projects	(465,400)		
Est. QB. Transportation DCC Balance After Projects	(2,274,579)		
<u>Mainland Transportation DCC Projects</u>			
Pedestrian Crossing Improvements (T9, T25)	398,700	288,200	110,500
McBride Boulevard Safety & Transit (T23)	33,100	11,100	22,000
Upgrades to Traffic Signal System (T24)	25,000	18,100	6,900
Neighbourhood Traffic Calming Program (T25)	300,000	216,800	83,200
Road Safety Improvements (T31)	21,900	15,800	6,100
Sixth St Great Street (T34)	30,000	21,700	8,300
ML. Transportation DCC Balance at Dec 31, 2020	3,308,887		
Actual 2021 ML. Transportation DCC Contributions to-date	31,418		
2021 ML. Transportation DCC Projects	(237,000)		
Est. ML. Transportation DCC Balance After Projects	3,103,305		
<u>Queensborough Parkland DCCs</u>			
2021 Debt Principal Repayment for Waterfront Park Development	187,500	1,900	185,600
QB. Parkland DCC Balance at Dec 31, 2020	3,120,688		
Actual 2021 QB. Parkland DCCs Contributions to-date	690,063		
2021 City Wide Parkland DCC Projects	(185,600)		
Est. QB. Parkland DCC Balance after Expenditure	3,625,151		
<u>Mainland Parkland DCCs</u>			
2021 Debt Principal Repayment for Waterfront Park Development	363,900	3,600	360,300
ML. Parkland DCC Balance at Dec 31, 2020	9,791,707		
Actuals 2021 Mainland Parkland DCCs Contributions to-date	60,750		
2021 City Wide Parkland DCC Projects	(360,300)		
Est. ML. Parkland DCC Balance after Expenditure	9,492,157		
Grand Total	4,095,100	2,092,200	2,002,900